

STATE OF MAINE STATE HIGHWAY COMMISSION

PLANS FAIRFIELD

SOMERSET COUNTY

MAINE FEDERAL AID INTERSTATE #95

PROJECT NO. I-95-6 (25) 126

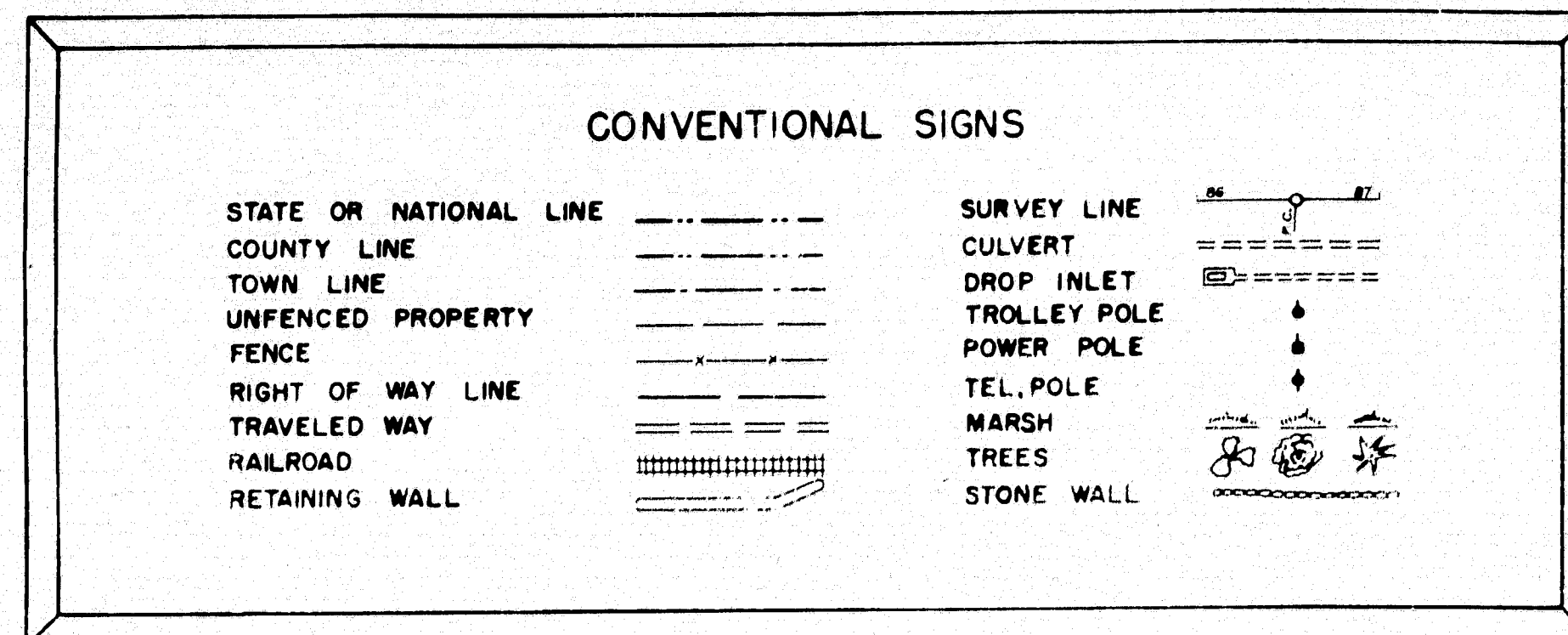
TOTAL LENGTH 0.000 MILES

SCALES
 PLAN 1 IN. = 50 FT.
 PROFILE { HOR. 1 IN. = 50 FT.
 VER. 1 IN. = 5 FT.
 CROSS SECTIONS 1 IN. = 10 FT.

AS BUILT
1959

SHEET NO.

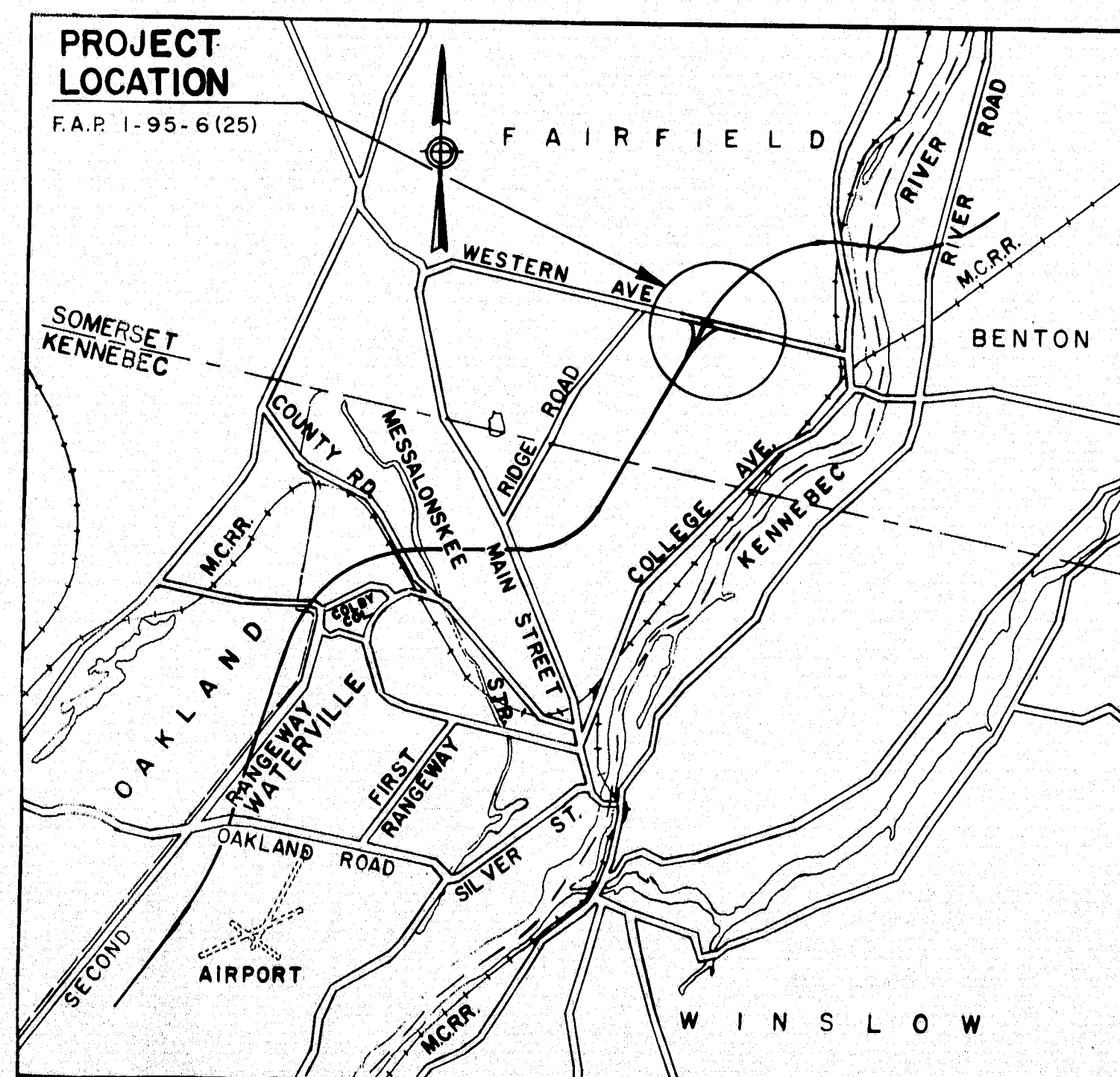
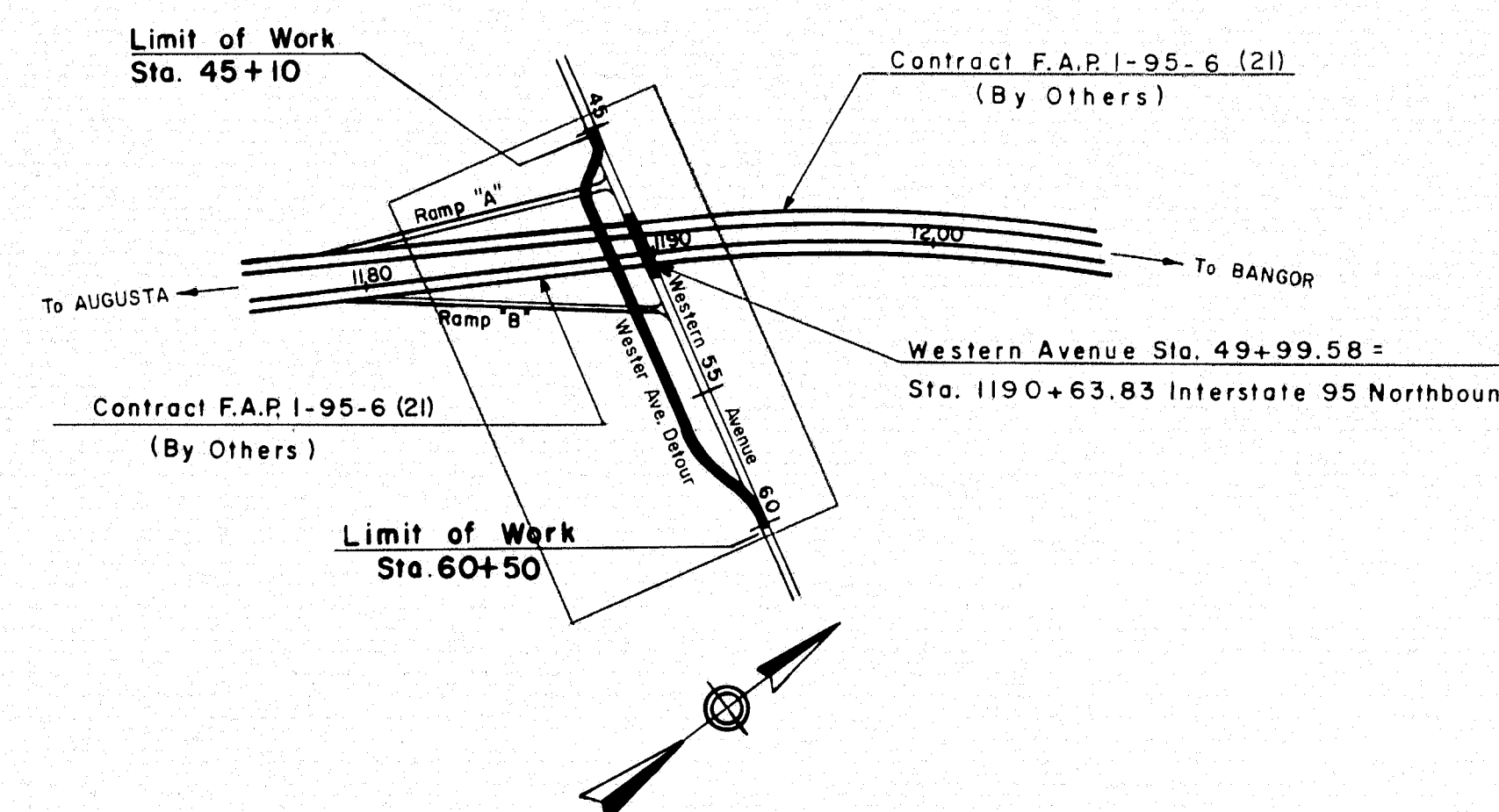
1	Title Sheet
2-3	Typical Sections
4	Detail Sheet
5-10	Standard Details
11	General Plan - Western Avenue Detour
12	Profile Western Avenue Detour
13-20	Structural Plan Western Avenue Bridge
21-27	Cross Sections



Note: The work under this contract includes the construction of the Western Avenue bridge without pavement but including the approach fills, the construction of the Western Avenue Detour and structural plate pipe culvert at Station 56.

All work contemplated under this Contract to be Governed by and in conformity with the Standard Specifications, Highways and Bridges, Revision of Jan. 1956, except as modified on these Plans And by the Special Provisions.

WESTERN AVENUE BRIDGE OVER INTERSTATE #95



A PORTION OF KENNEBEC AND SOMERSET COUNTIES

APPROX. SCALE 1 IN. = 1 MILE

LAYOUT PLAN

Scale 1" = 600'

INTERSTATE #95

A.D.T. (1960)	=	8370
A.D.T. (1980)	=	10660
D.H.V. (1980)	=	1600
D. (1980)	=	65%
T. (1980)	=	15%
V. (M.P.H.)	=	60

WESTERN AVENUE

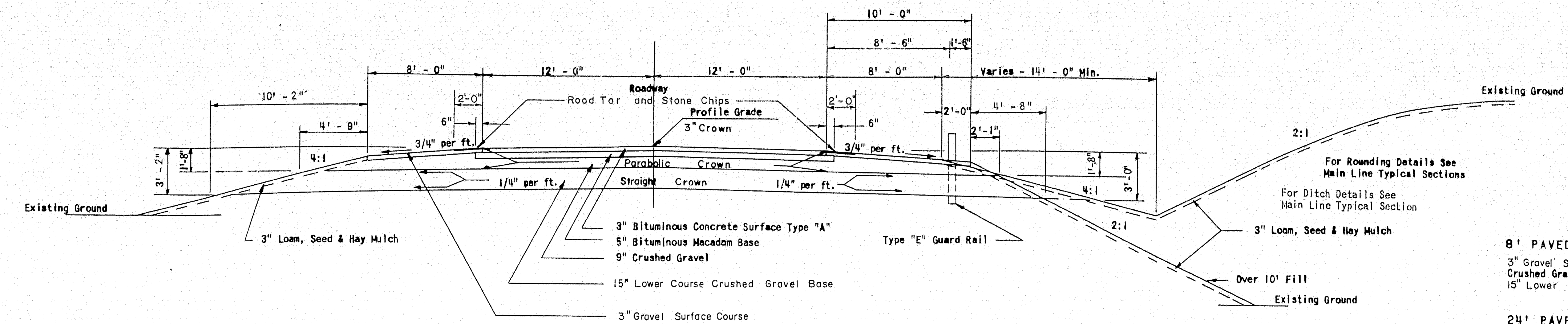
A.D.T. (1960)	=	2410
A.D.T. (1980)	=	3160
D.H.V. (1980)	=	474
D. (1980)	=	65%
T. (1980)	=	15%
V. (M.P.H.)	=	50

APPROVED:
MAINE STATE HIGHWAY COMMISSION

David H. Stevens
CHAIRMAN
R. Leon Williams
CHIEF ENGINEER

CONSULTING ENGINEERS
THE CLARKSON ENGINEERING COMPANY, INC.
BOSTON MASS.
8/27/59
CONSULTING ENGINEER DATE

DEPARTMENT OF COMMERCE
BUREAU OF PUBLIC ROADS
REGION 1
APPROVED:
DIVISION ENGINEER DATE

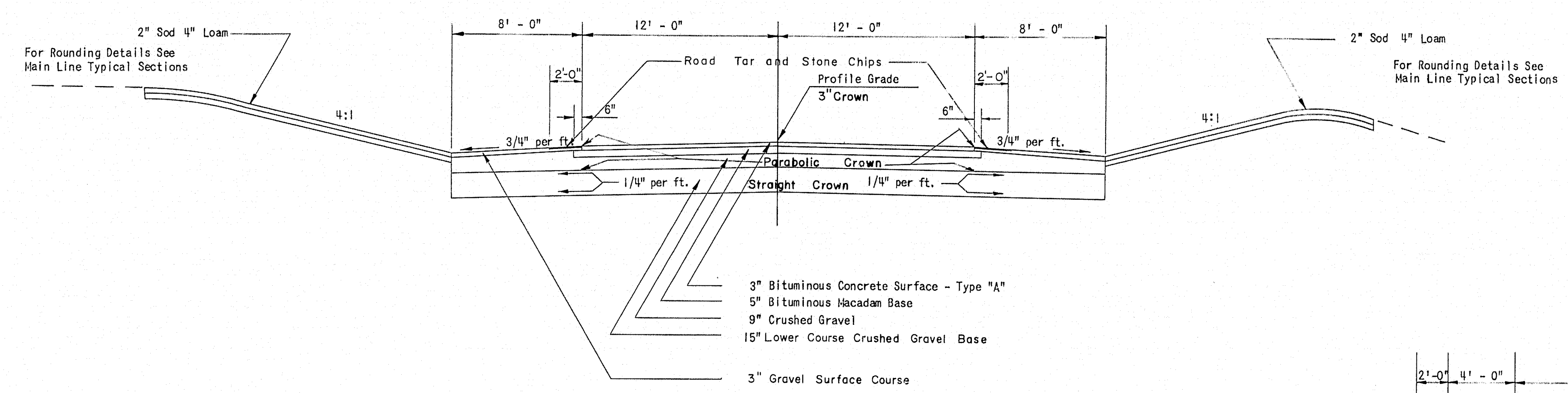


WESTERN AVENUE
TYPICAL SECTION - NORMAL

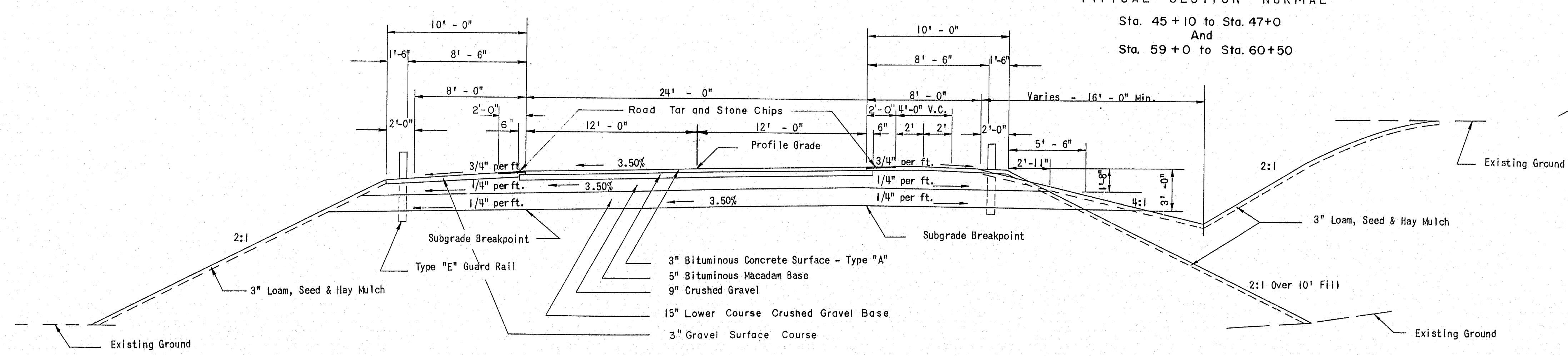
8' PAVED SHOULDER	C.Y./100 L.F.
3" Gravel Surface Course	7.41
Crushed Gravel Base (to 4:1 Slope)	34.46
15" Lower Course Crushed Gravel Base	67.34

24' PAVEMENT	C.Y./100 L.F.
3" Bituminous Concrete Surface	22.22
5" Bituminous Macadam Base (25' Wide)	38.56
9" Crushed Gravel Base	66.67
15" Lower Course Crushed Gravel Base	111.11

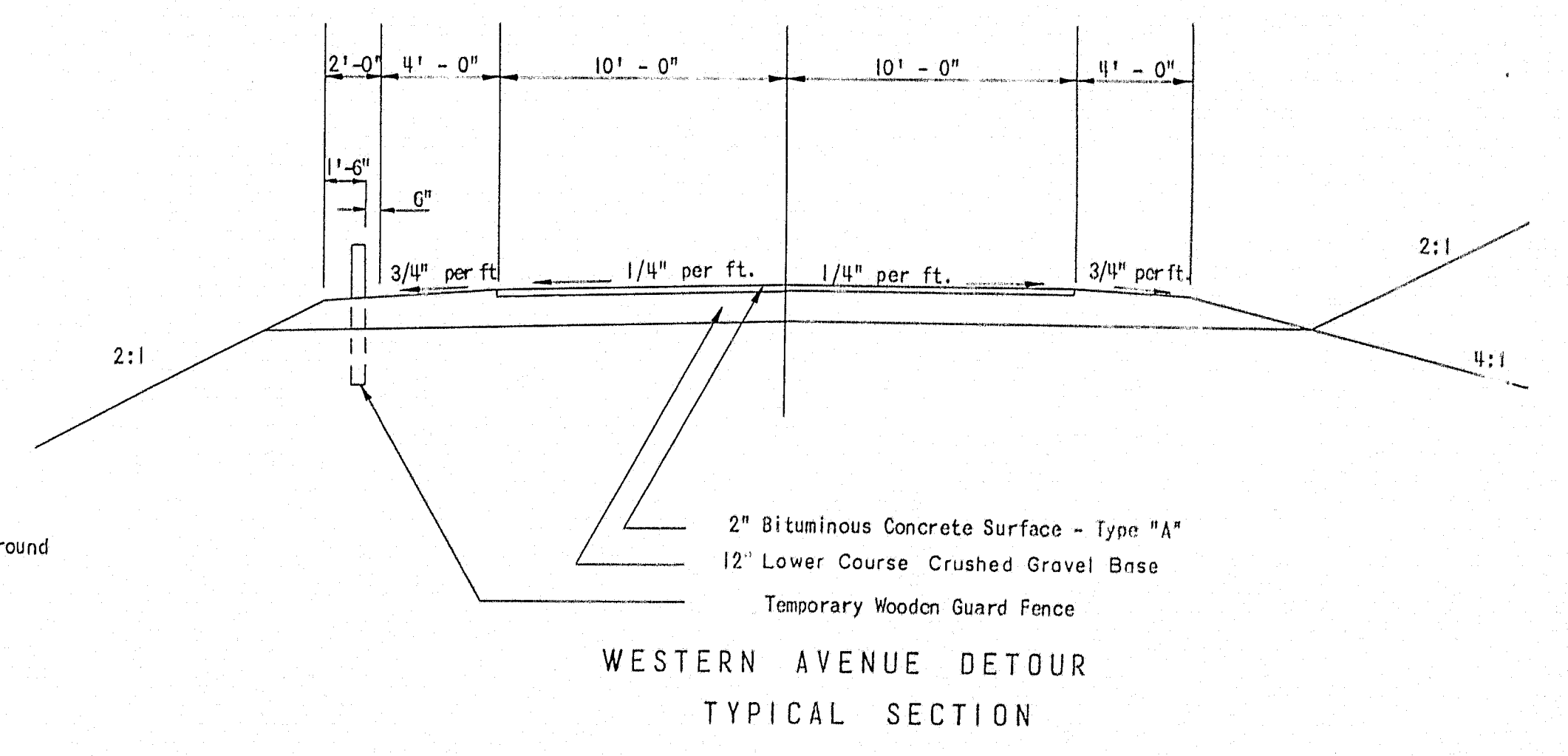
10' PAVED SHOULDER	C.Y./100 L.F.
3" Gravel Surface Course	9.26
Crushed Gravel Base (to 2:1 Slope)	36.74
15" Lower Course Crushed Gravel Base	59.56



WESTERN AVENUE
TYPICAL SECTION - NORMAL
Sta. 45+10 to Sta. 47+0
And
Sta. 59+0 to Sta. 60+50



WESTERN AVENUE
TYPICAL SECTION - SUPERELEVATED



WESTERN AVENUE DETOUR
TYPICAL SECTION

MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

TYPICAL SECTIONS

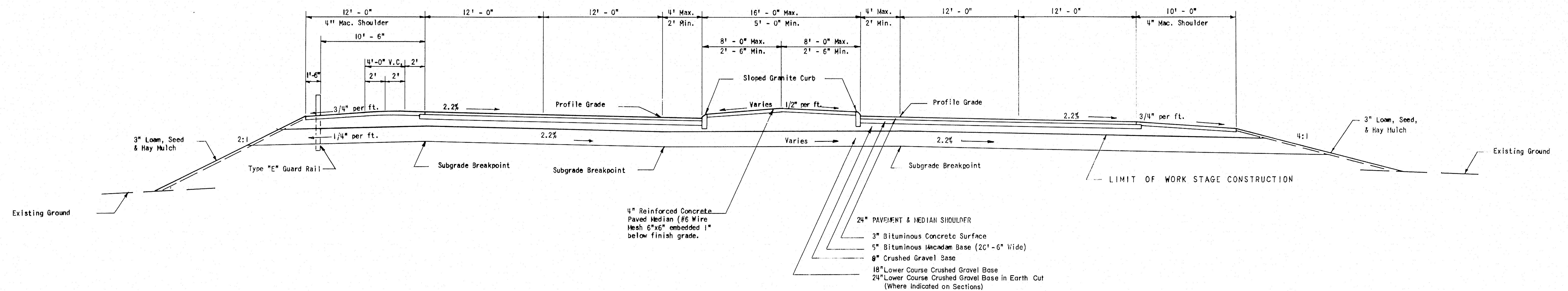
THE CLARKESON ENGINEERING CO., INC.
CONSULTING ENGINEERS
BOSTON MASSACHUSETTS

S. P. R. REG. NO.	STATE	PROJECT NO	SHEET NO.	TOTAL SHEETS
1	MAINE	[95-6(25)	3	27

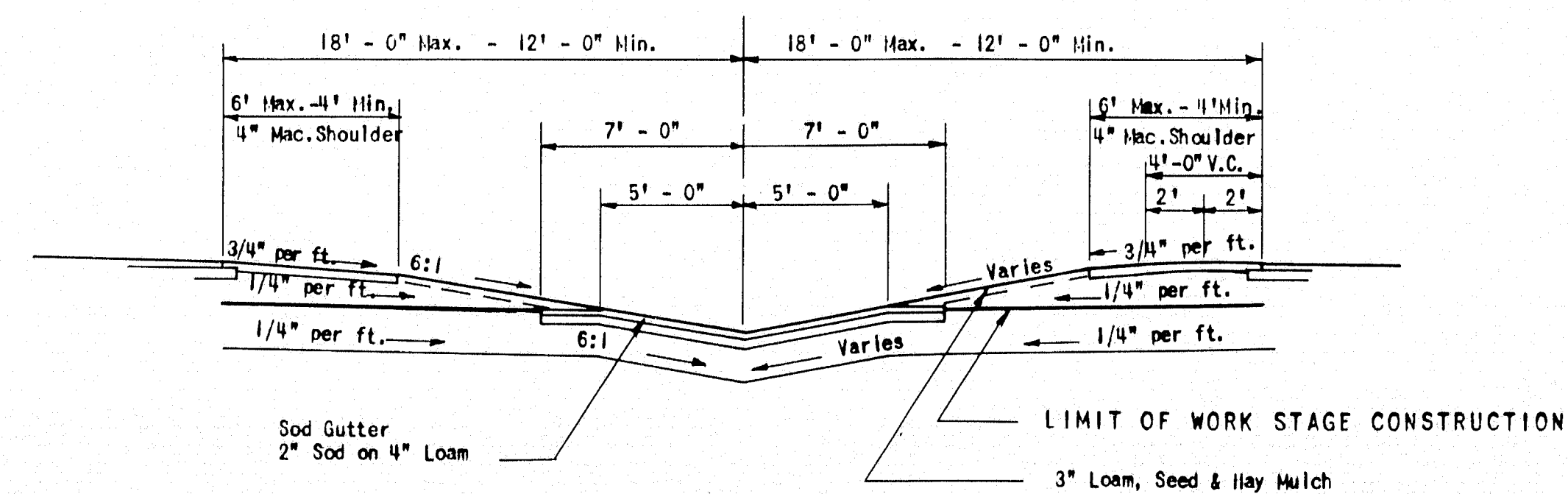
FAIRFIELD INTERSTATE

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TYPICAL SECTION - SUPERELEVATED



TYPICAL SECTION - SUPERELEVATED
WITH RAISED MEDIAN

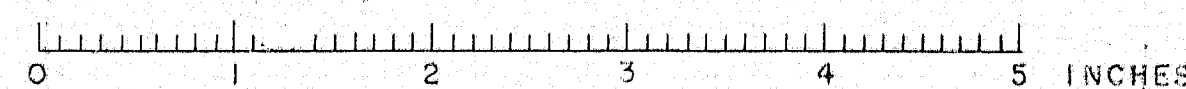


TYPICAL SECTION - SUPERELEVATED
36' TO 24' DEPRESSED MEDIAN
STA. 1236+50 NB TO 1243+00 NB

MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

TYPICAL SECTIONS

THE CLARKESON ENGINEERING CO., INC.
CONSULTING ENGINEERS
BOSTON MASSACHUSETTS



FINAL ESTIMATED QUANTITIES			
ITEM NO.	DESCRIPTION	QUANTITY	UNIT
201-5	CLEARING	0.524	ACRES
203-9	EARTH EXCAVATION	3,250.33/4	C.Y.
204-10	STRUCTURAL EARTH EXCAVATION - DRAINAGE	1,859.450	C.Y.
204-11	STRUCTURAL ROCK EXCAVATION - DRAINAGE	56.22	C.Y.
204-12	STRUCTURAL EARTH EXCAVATION - ABUTS. & RETAINING WALLS	189.72.8	C.Y.
204-14	STRUCTURAL EARTH EXCAVATION - PIERS	255.247/	C.Y.
204-16	STRUCTURAL EARTH EXCAVATION - CHANNEL	700.431.6	C.Y.
204-17	STRUCTURAL ROCK EXCAVATION - CHANNEL	50.46nc	C.Y.
205-8	COMMON BORROW	30,748	C.Y.
205-9	GRANULAR BORROW	8,000	C.Y.
302-7	GRAVEL BASE COURSE - IN PLACE MEASUREMENT	1,430.229	C.Y.
302-13	LOWER COURSE CRUSHED GRAVEL BASE	2,120.928	C.Y.
309-5	STRIPPING PITS	1,100	C.Y.
310-6	SPRINKLING	8.16nc	UNITS
311-6	CALCIUM CHLORIDE	10.75	TON
404-28	BITUMINOUS CONCRETE SURFACE COURSE, TYPE "A"	385.88	TON
602-11	15" ASPHALT COATED CORRUGATED METAL PIPE	870	L.F.
701-33	PORTLAND CEMENT CONCRETE, ABUTMENTS & RETAINING WALLS	142.81	C.Y.
701-35	PORTLAND CEMENT CONCRETE, PIERS	228.51	C.Y.
701-40	PORTLAND CEMENT CONCRETE, ROADWAY & SIDEWALK SLABS ON STEEL BRIDGES	316.16	C.Y.
701-47	PORTLAND CEMENT	105.55	BBLS.
702-103	STRUCTURAL STEEL, FABRICATED & DELIVERED	276.431	LBS.
702-104	STRUCTURAL STEEL, ERECTION	276.431	LBS.
704-13	96" STRUCTURAL PLATE PIPE	208.0	L.F.
705-13	REINFORCING STEEL, DELIVERED	122.967	LBS.
705-14	REINFORCING STEEL, PLACING	122.967	LBS.
708-16	STEEL H-BEAM PILES - 42 LBS./FOOT	1,945.0	L.F.
709-5	BRIDGE DRAINAGE	1	L.S.
710-5	SHEAR CONNECTORS, DELIVERED & PLACED	1	L.S.
804-6	FRENCH DRAINS	75.59.5	C.Y.
807-15	ALUMINUM RAILING	560.2	L.F.
905-23	GUARD RAIL - TYPE "A"	320.310.0	L.F.
905-31	ANCHORAGES FOR TYPE "A" GUARD RAIL	2	EACH
905-35	GUARD POST - TYPE "A"	2	None
905-44	TEMPORARY WOODEN GUARD FENCE	476.448.0	L.F.
907-10	HAND LAID RIPRAP	145.93.9	C.Y.
908-9	LOAM BORROW	100.49.0	C.Y.
910-10	SEEDING - PARKWAY MIXTURE	13.2	UNITS
912-6	HAY MULCH	2.195	TONS
914-6	PROJECT MARKERS	216nc	EACH
917-6	TRAFFIC OFFICERS	200.22	HW. HRS.
926-7	REMOVAL OR RAZING BUILDINGS NO. 1	1	EACH
926-8	REMOVAL OR RAZING BUILDINGS NO. 2	1	EACH
926-9	REMOVAL OR RAZING BUILDINGS NO. 3	1	EACH
926-10	REMOVAL OR RAZING BUILDINGS NO. 4	1	EACH
926-11	REMOVAL OR RAZING BUILDINGS NO. 5	1	EACH
930-13	PORTABLE BARRICADE WITH FLASHING LIGHTS	2	EACH
933-6	SLOPE PAVING FOR BRIDGES	457.9	S.Y.
308-5	GRAVEL FOR FOUNDATIONS	600.315	C.Y.
909-7	OVERHAUL - PLACE HEAS	32.27	Y.M.
909-7	SODDING	38.0	S.Y.

CLEARING		
STATION TO STATION	SIDE	REMARKS
Western Avenue		
47 + 10 TO 47 + 65	LT.	
48 + 20 TO 48 + 00	LT.	
Western Avenue Detour		
56 + 80 TO 58 + 05	LT. & RT.	
57 + 92 TO 58 + 36	RT.	

GUARD RAIL - TYPE "A"		
STATION TO STATION	LENGTH	REMARKS
Western Avenue Detour		
55 + 74 TO 58 + 70 RT.	304'	
55 + 60 To 58 + 70 RT.	310'	

TEMPORARY WOODEN GUARD FENCE		
STATION TO STATION	LENGTH	REMARKS
Western Avenue Detour		
55 + 74 TO 58 + 70 LT.	466'	
48 + 76 TO 48 + 24 RT.	48'	
55 + 60 To 59 + 36 LT.	376'	
48 + 58 To 49 + 30 RT.	72'	

PORTABLE BARRICADES		
STATION	NO.	REMARKS
Western Avenue		
46 + 25	1	With Flashing Lights
59 + 00	1	" " "

ROADWAY CULVERTS				
STATION	SIZE	ACC. CURV.	STRUCT. PLATE PIPE	REMARKS
Western Avenue				
56 + 18	96"		208'	7 Gage - Beveled Ends
Western Avenue Detour				
46 + 35	15"	22'		Driveway Culvert
56 + 00	15"	62'		" "

GUARD POSTS - TYPE "A"		
STATION	SIDE	REMARKS
Western Avenue		
47 + 10 TO 47 + 65	LT.	
48 + 20 TO 48 + 00	LT.	
Western Avenue Detour		
56 + 80 TO 58 + 05	LT. & RT.	
57 + 92 TO 58 + 36	RT.	

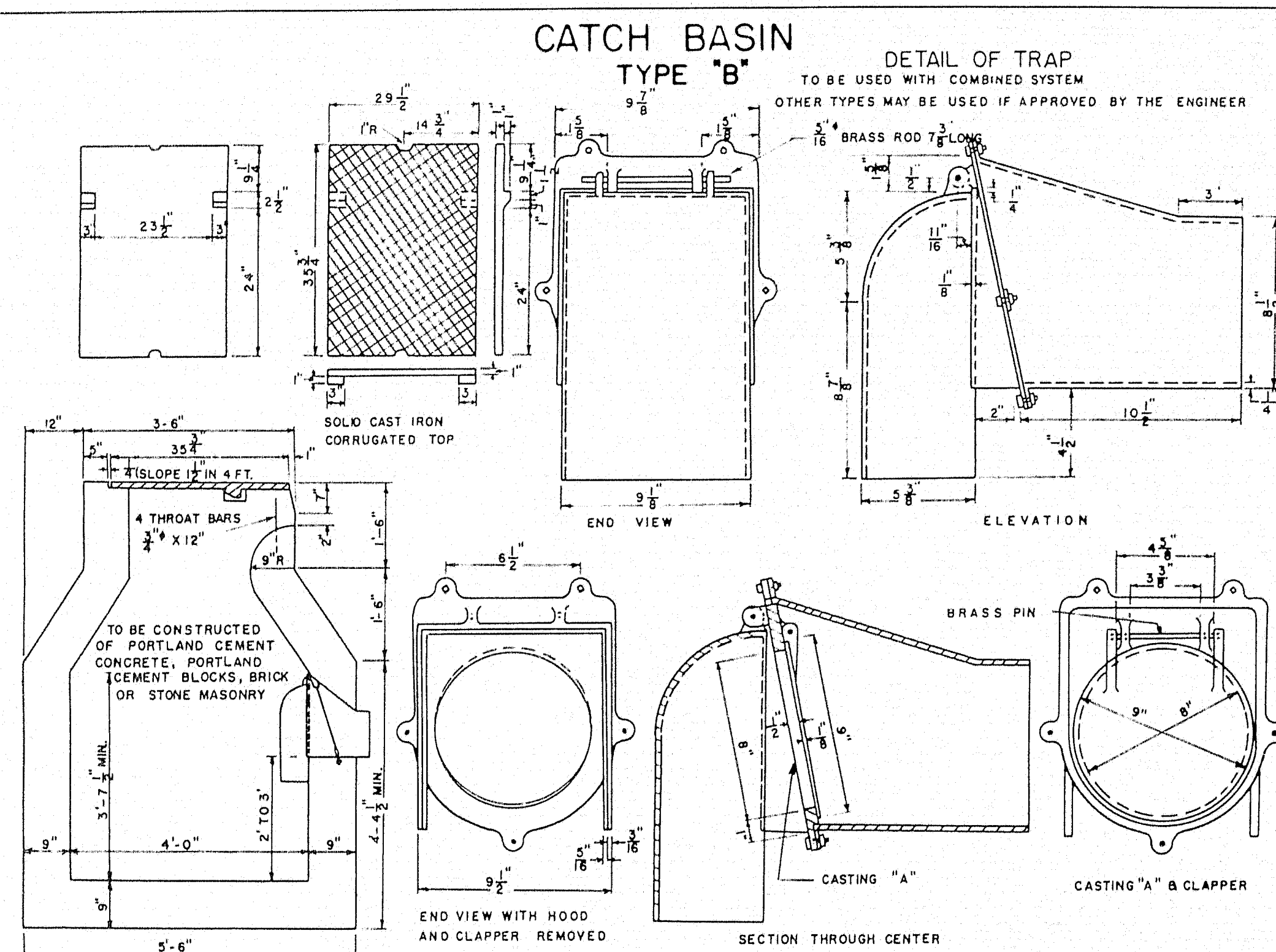
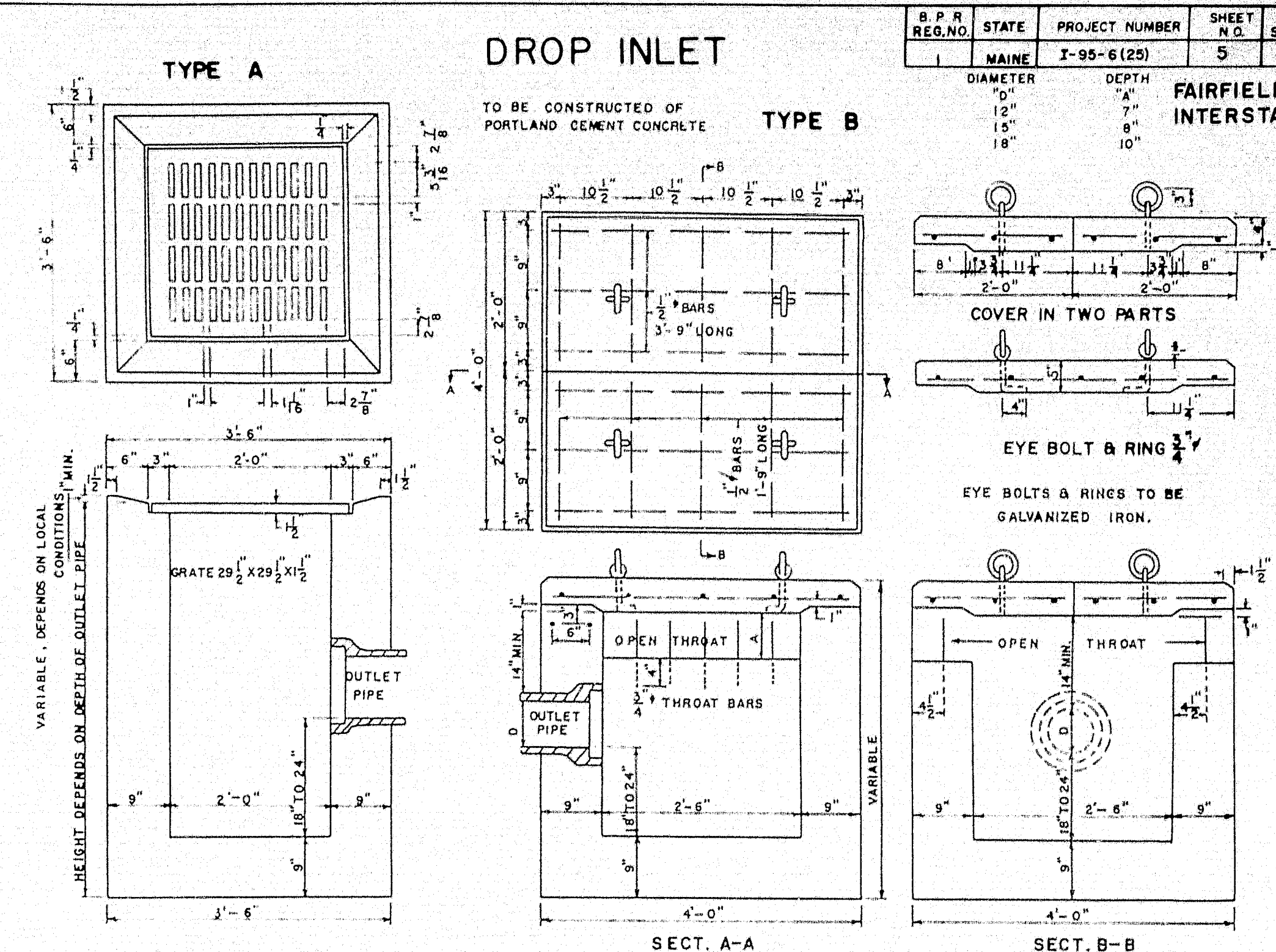
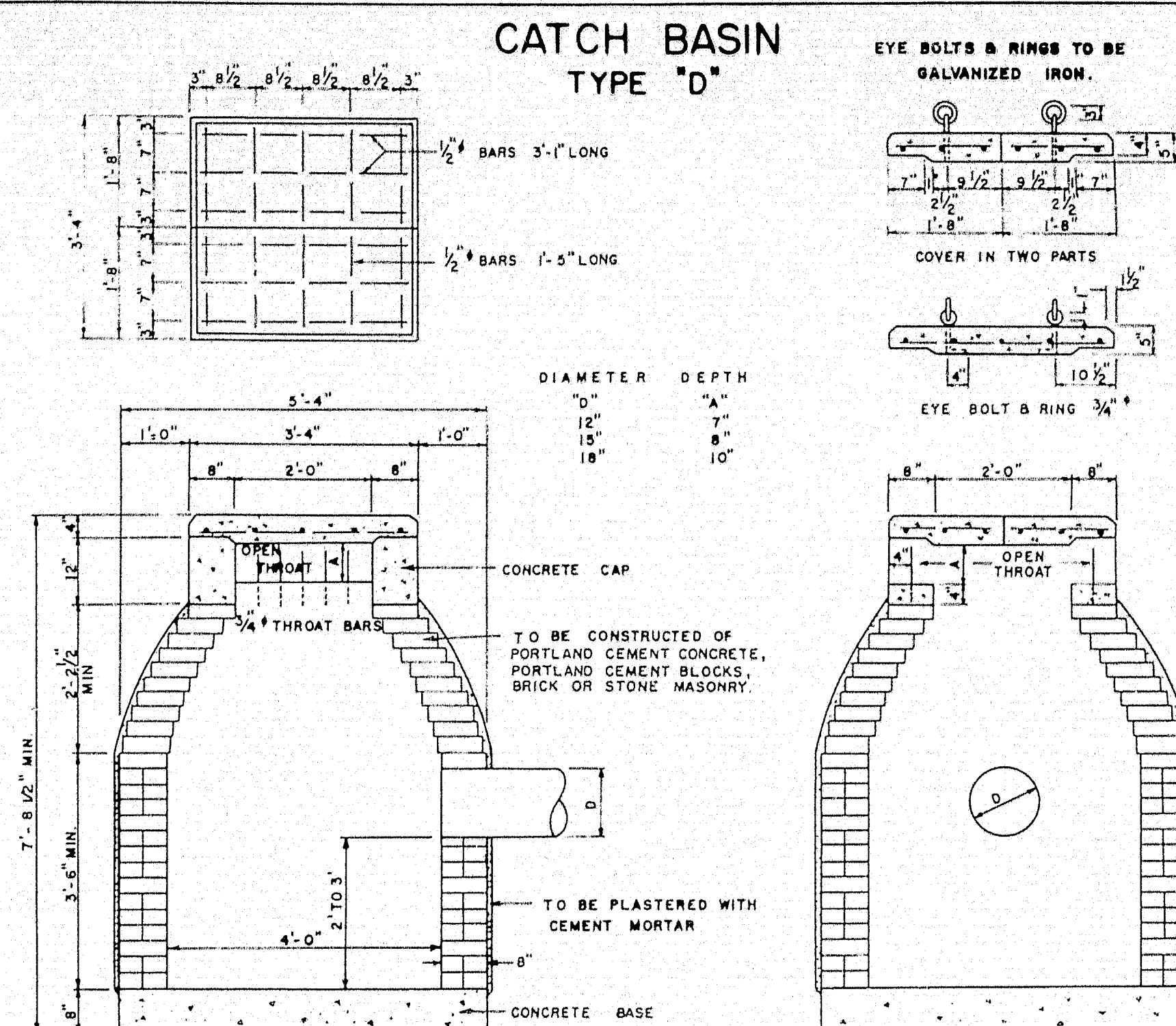
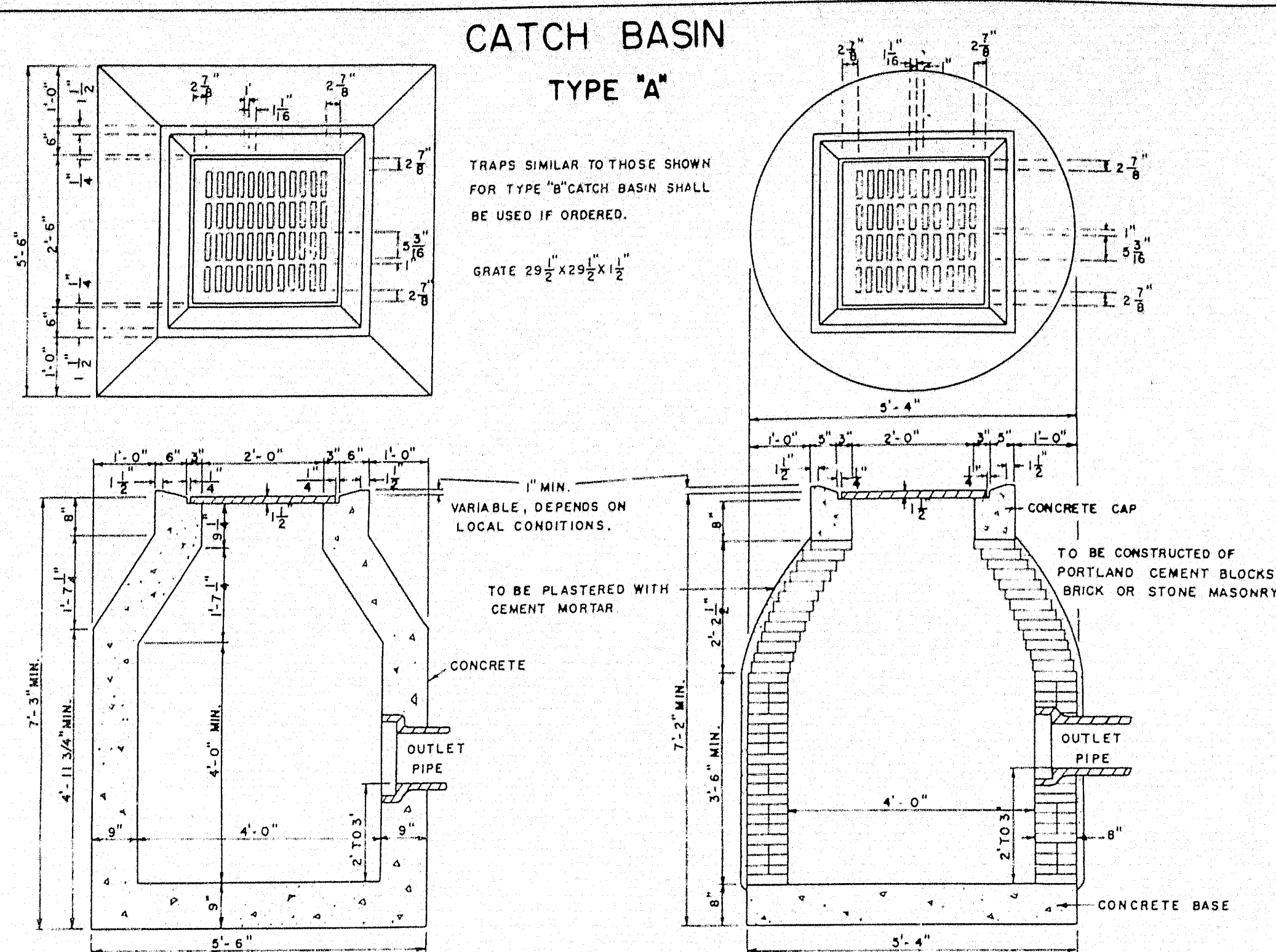
ANCHORAGES FOR TYPE "A" GUARD RAIL		
STATION	SIDE	REMARKS
Western Avenue Detour		
55 + 74 TO 58 + 70 RT.	RT.	
55 + 60 To 58 + 70 RT.	RT.	

SUMMARY OF CLASSIFIED EXCAVATION AND BORROW			
TOTAL EXCAVATION FROM CROSS-SECTIONS	3184.9	C.Y.	+ 65.1 = 3250 C.Y.
ESTIMATED EARTH SHRINKAGE FACTOR (-15%)	477.7	C.Y.	
AVAILABLE FILL FROM CROSS-SECTIONS	2707.2	C.Y.	
TOTAL EMBANKMENT FROM CROSS-SECTIONS	39,908.4	C.Y.	
20% GRANULAR BORROW	7,981.7	C.Y.	+ 18.3 = 8000 C.Y.
EMBANKMENT SUB-TOTAL	31,926.7	C.Y.	
EMBANKMENT SUB-TOTAL	31,926.7	C.Y.	
AVAILABLE FILL FROM CROSS-SECTIONS	2,707.2	C.Y.	
BORROW REQUIRED	29,219.5	C.Y. x 115%	= 33,700 C.Y.

B. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-6 (25)	5	27

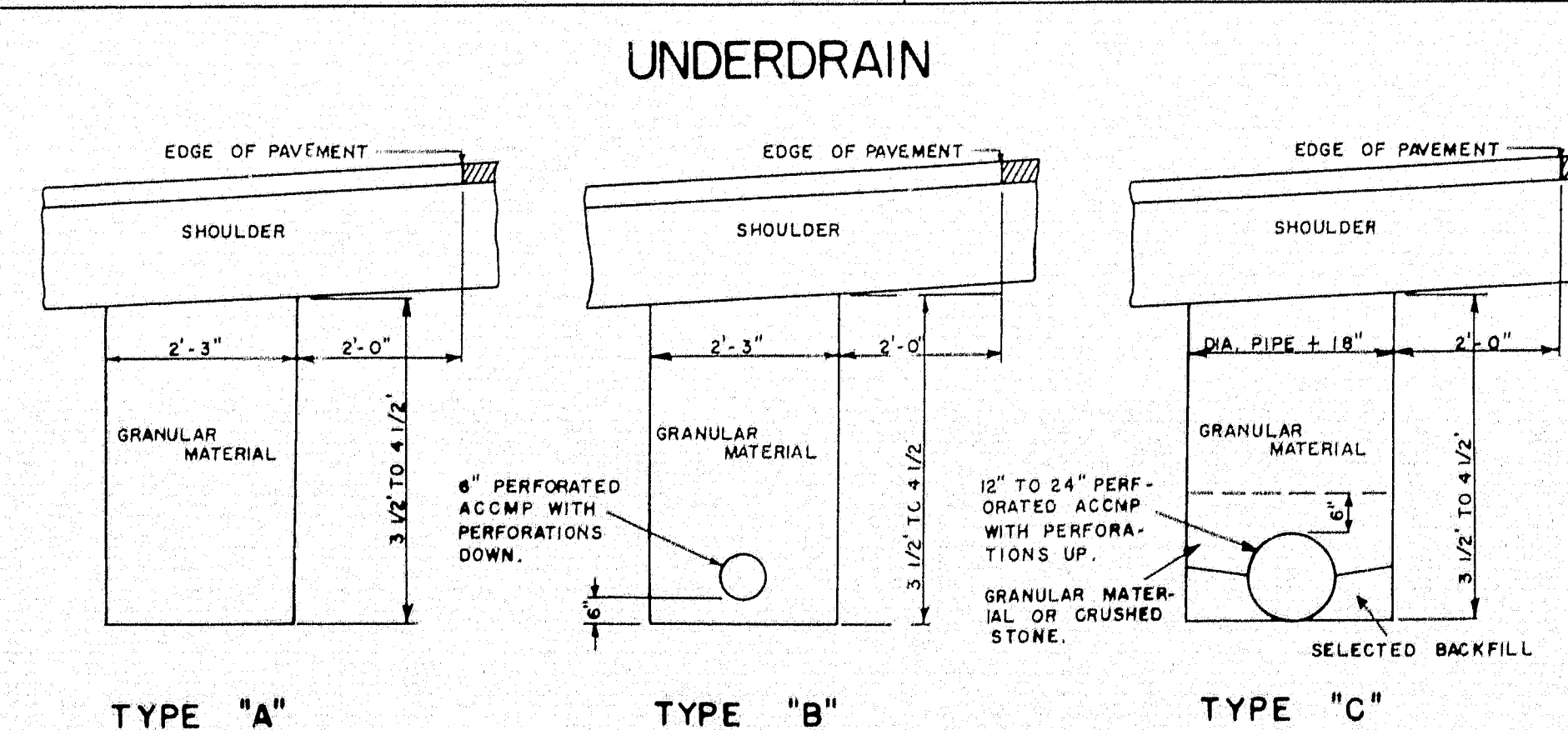
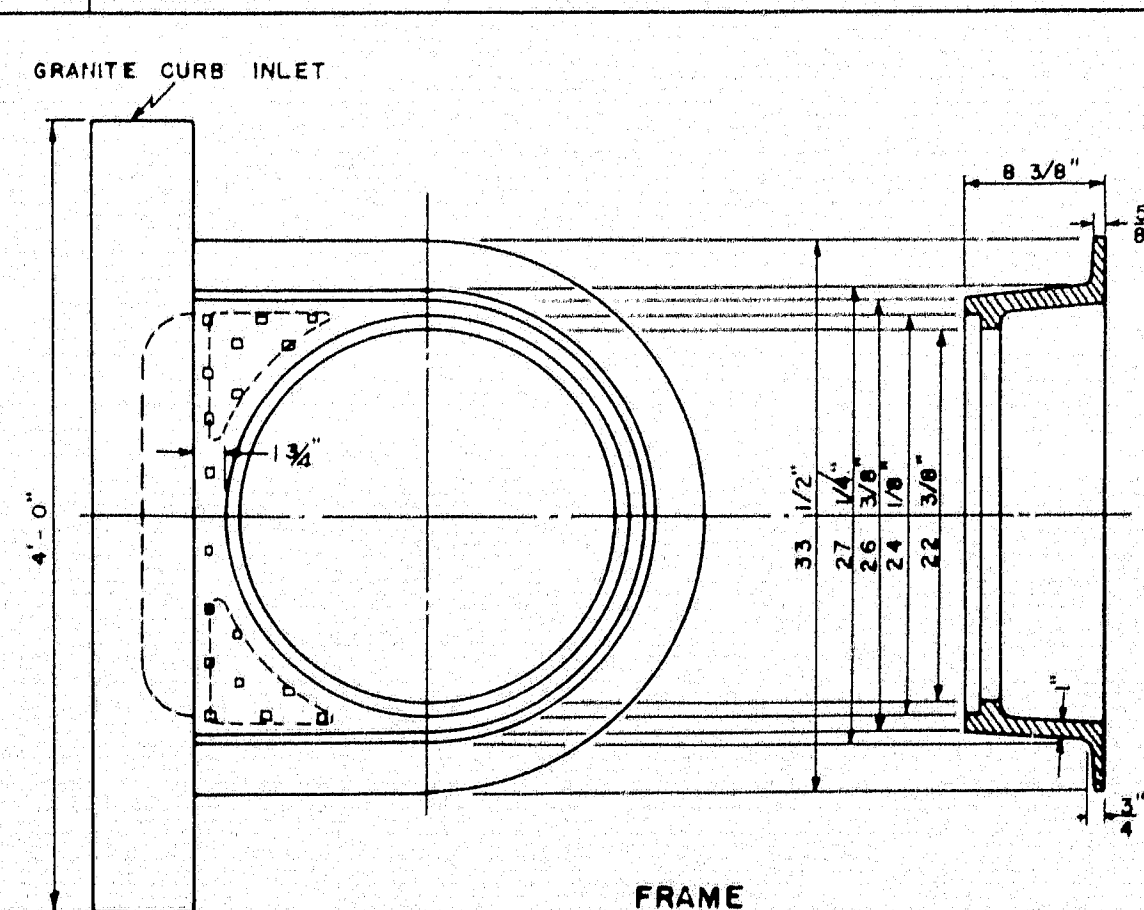
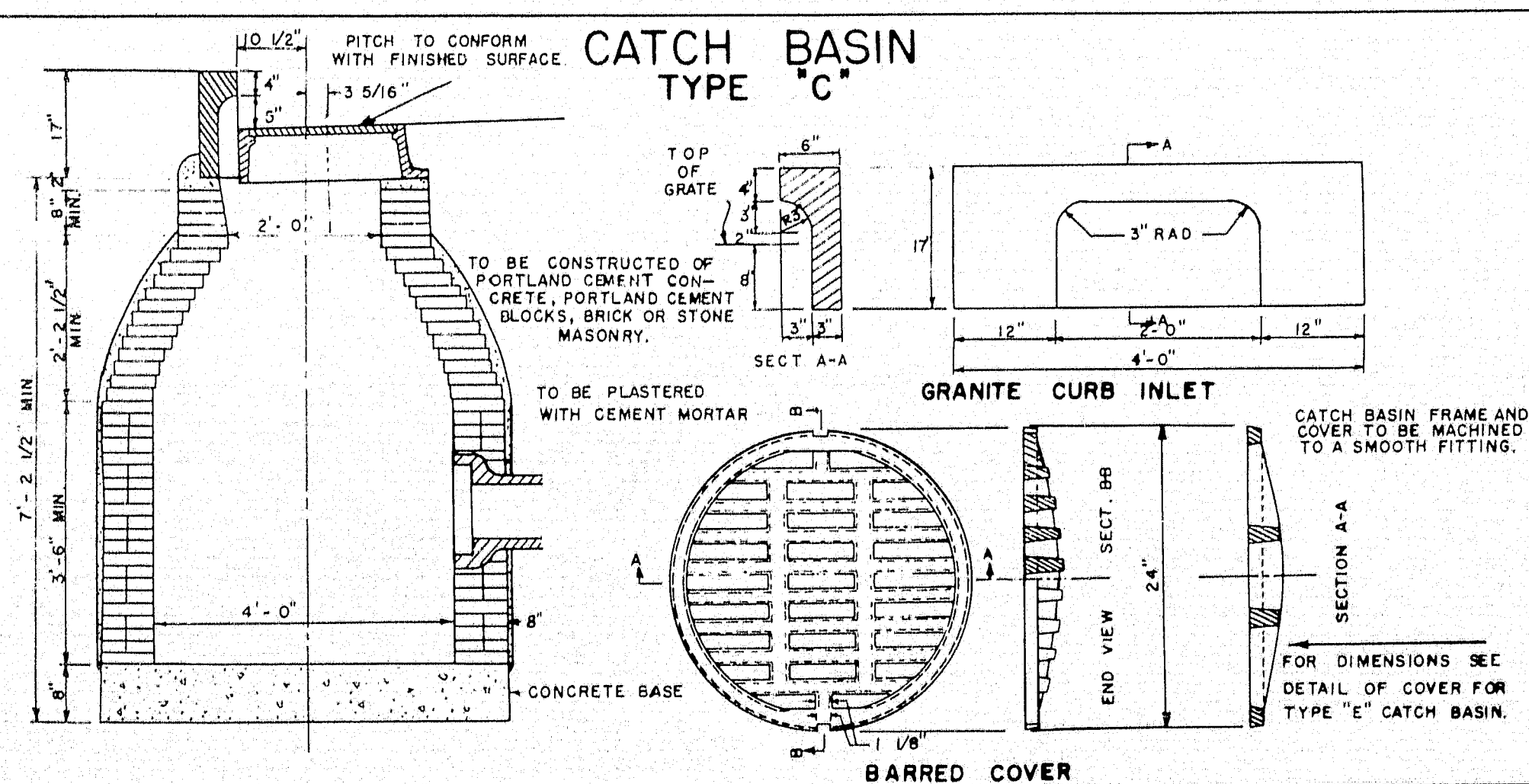
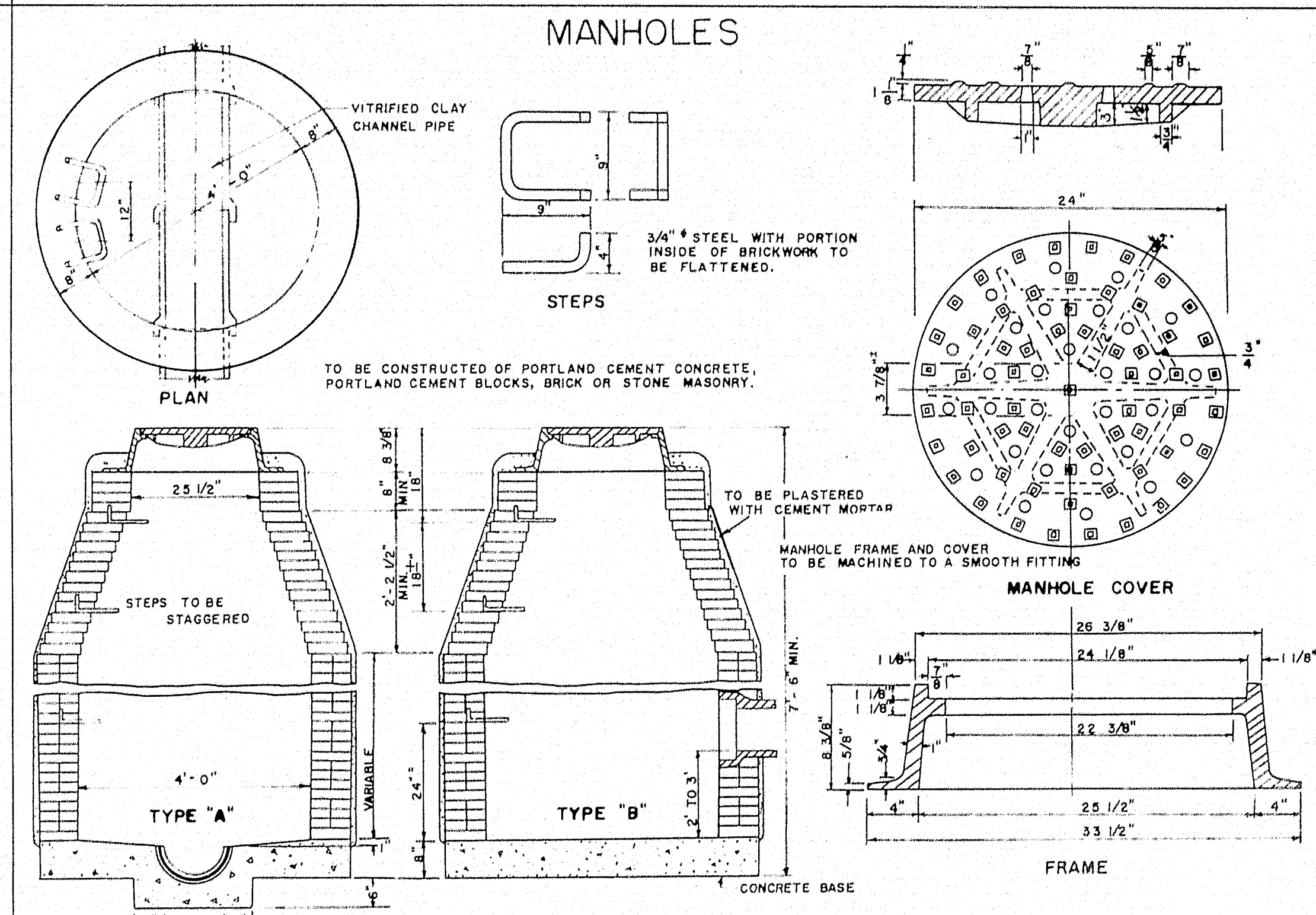
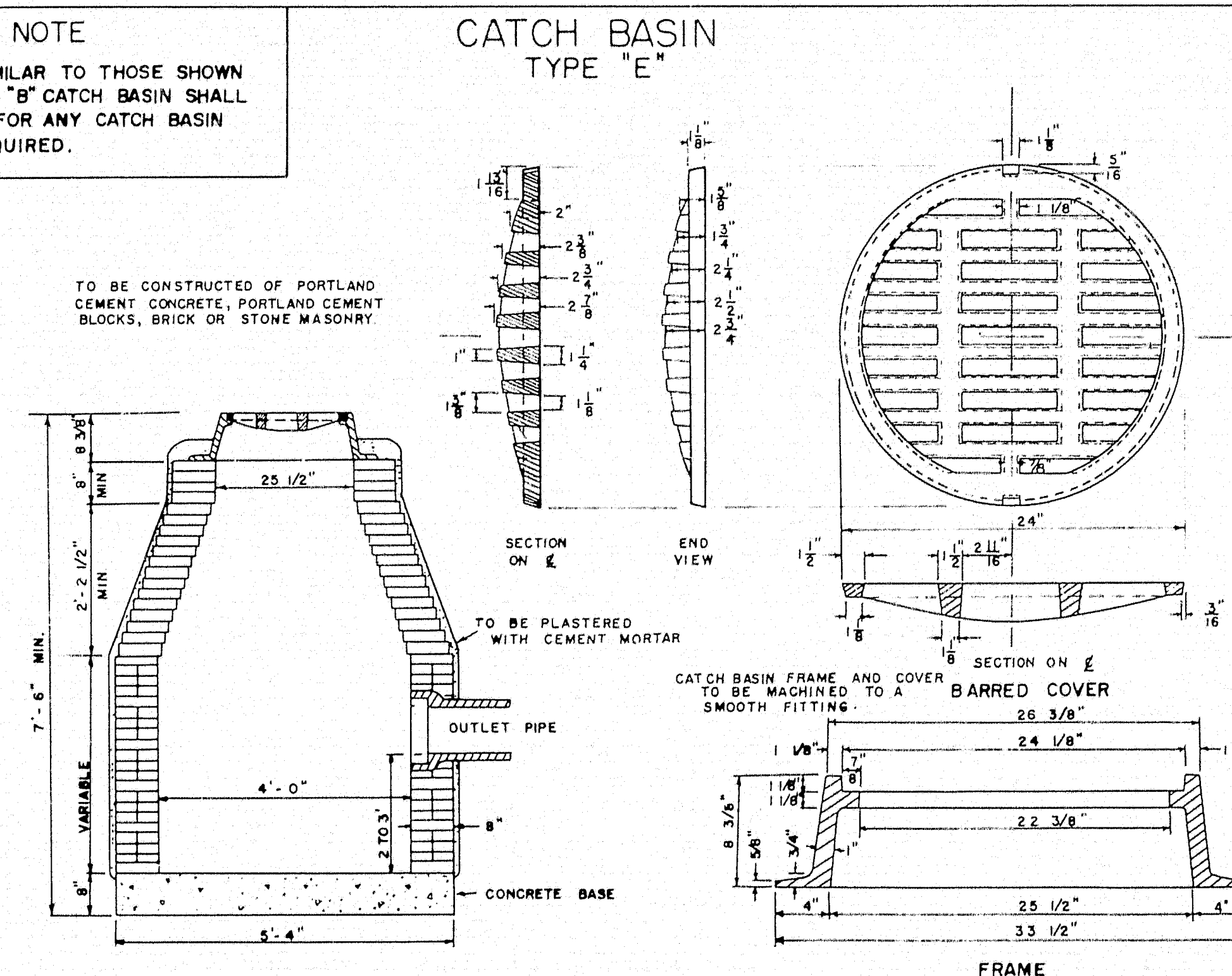
DIAMETER DEPTH

6" "A"	FAIRFIELD INTERSTATE
12" 7"	
15" 8"	
18" 10"	



NOTE

TRAPS SIMILAR TO THOSE SHOWN
FOR TYPE "B" CATCH BASIN SHALL
BE USED FOR ANY CATCH BASIN
WHEN REQUIRED.

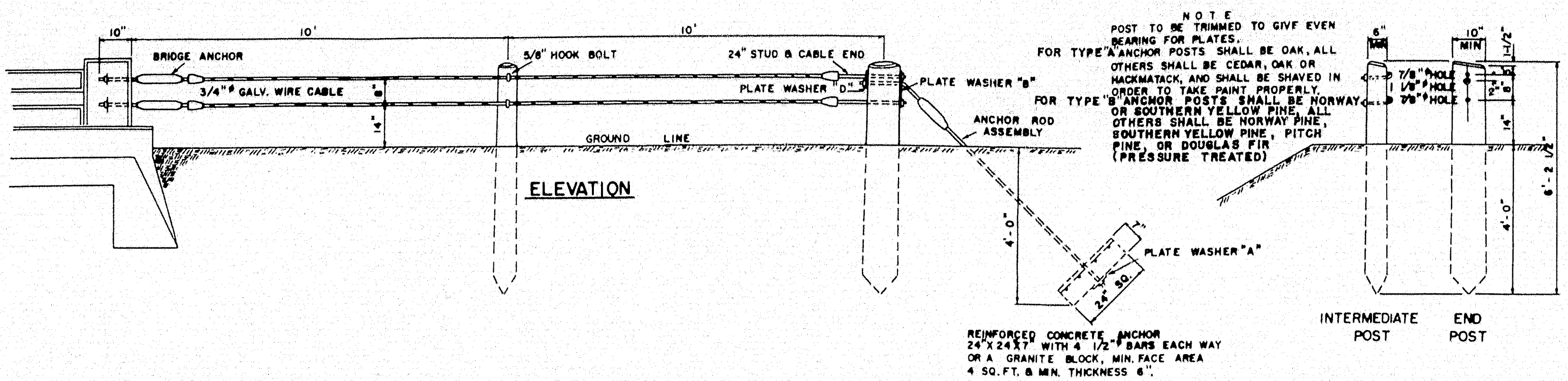


NOTE:
ALL CATCH BASINS AND MANHOLES CONSTRUCTED OF
PORTLAND CEMENT BLOCKS, BRICK OR STONE MASONRY
SHALL BE CYLINDRICAL IN SHAPE.

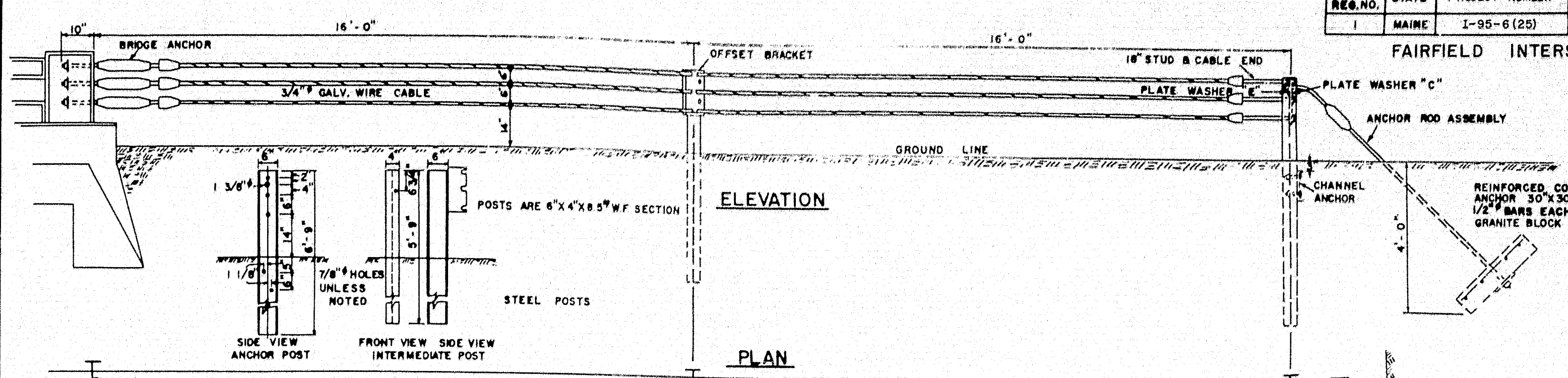
MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

STANDARD DETAILS

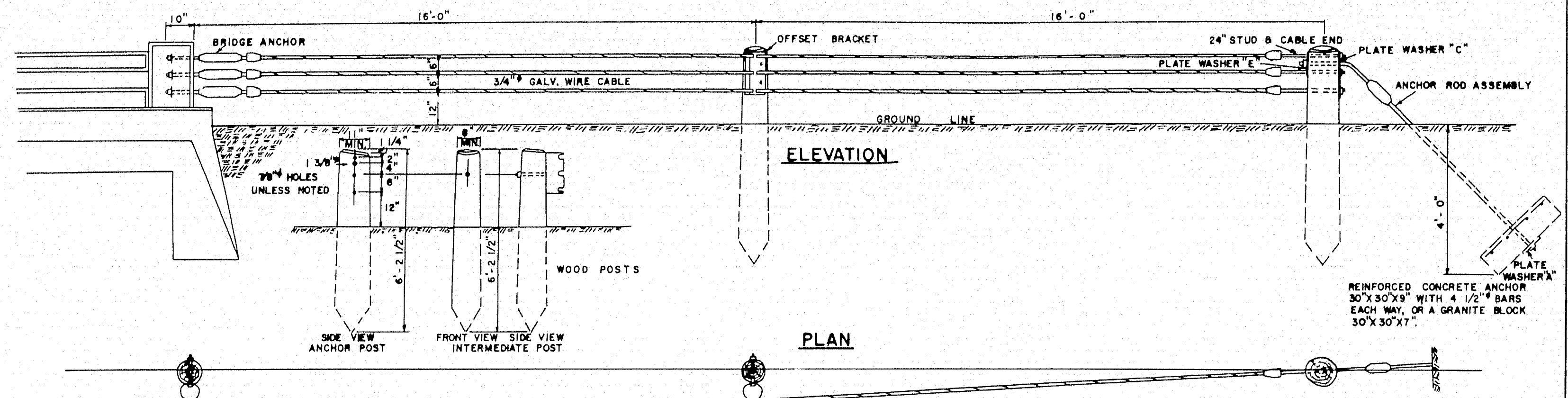
CATCH BASINS, MANHOLES, DROP INLETS AND UNDERDRAINS



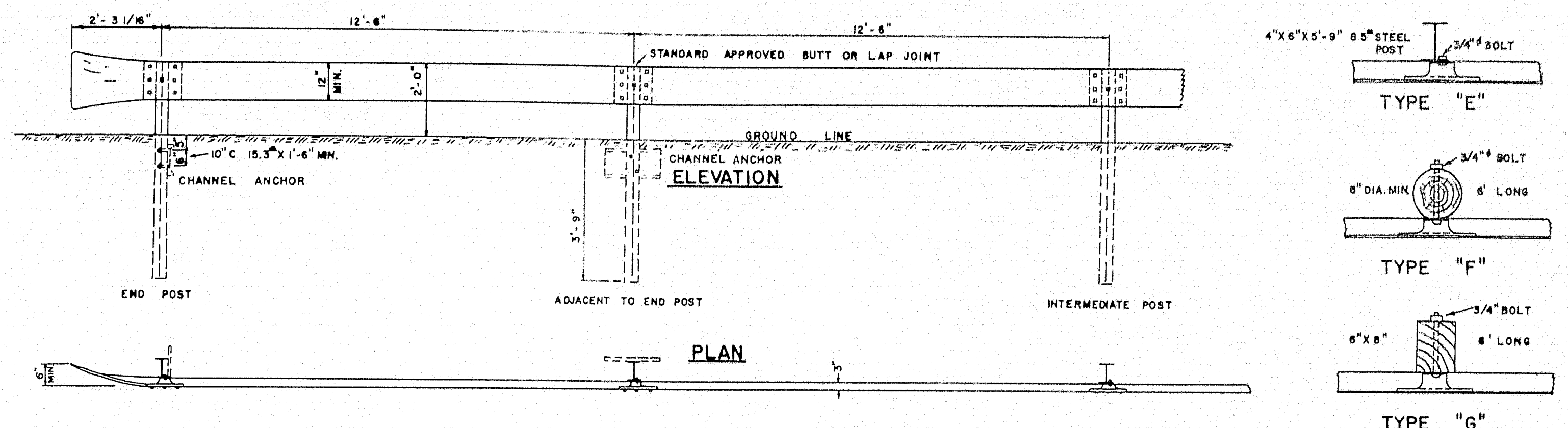
GUARD RAIL TYPE "A" & "B"



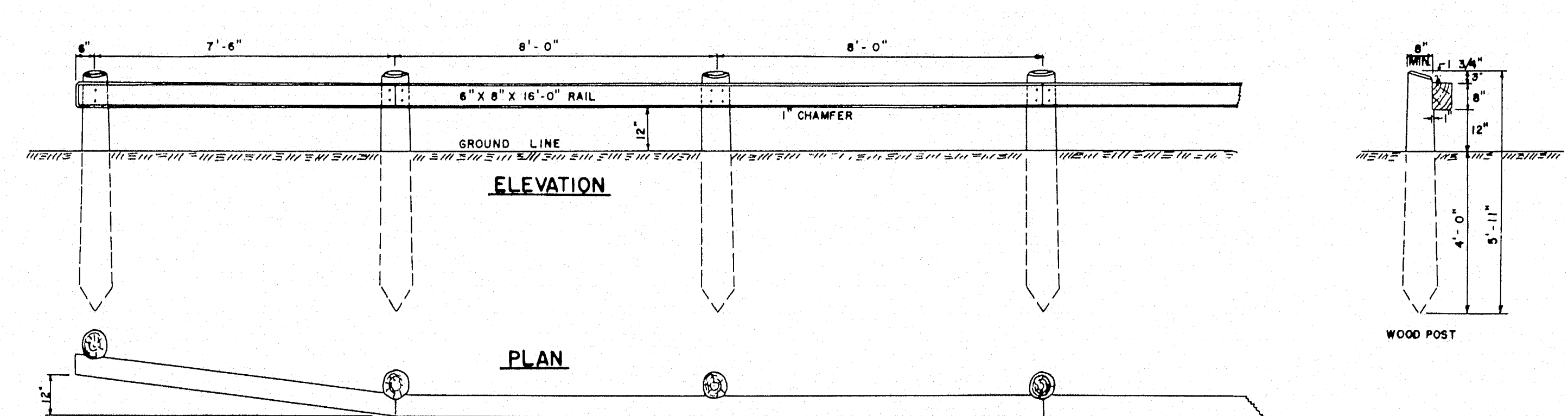
GUARD RAIL TYPE "C"



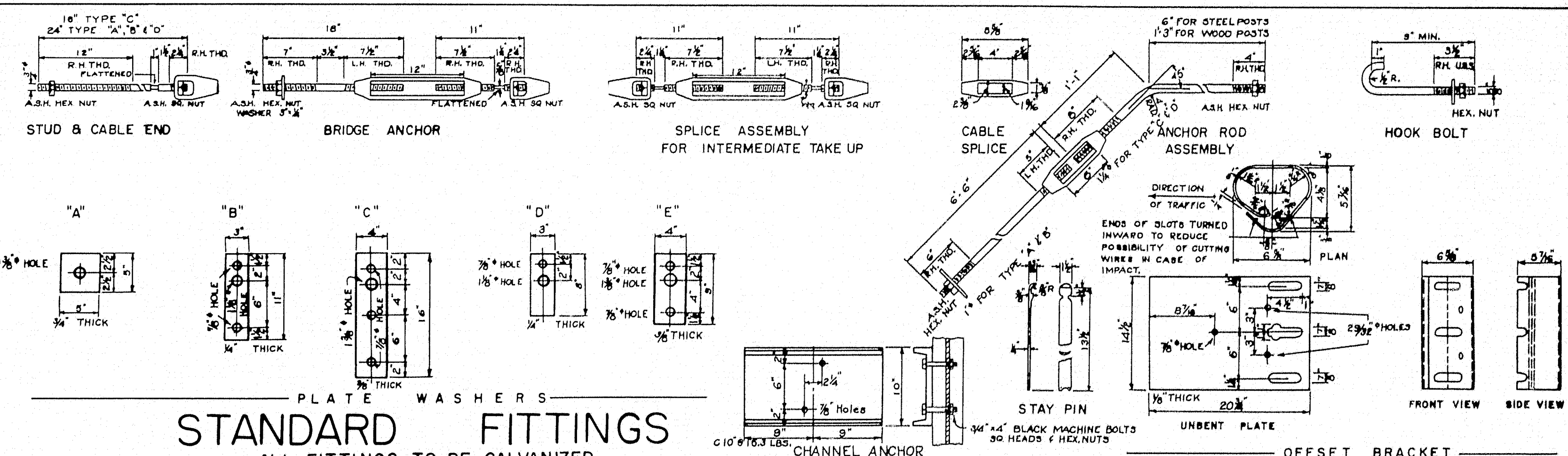
GUARD RAIL TYPE "D"



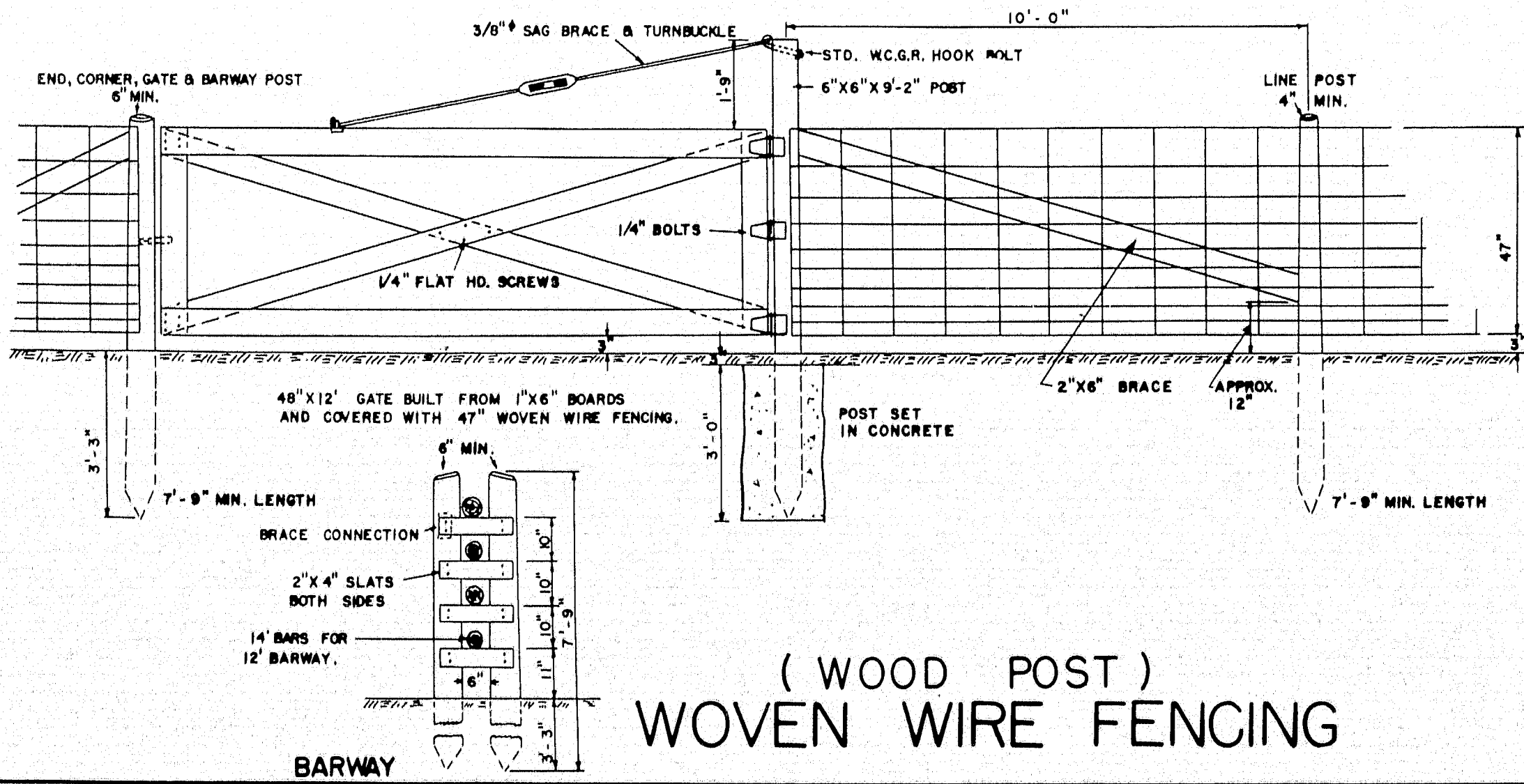
GUARD RAIL TYPE "E", "F" & "G"



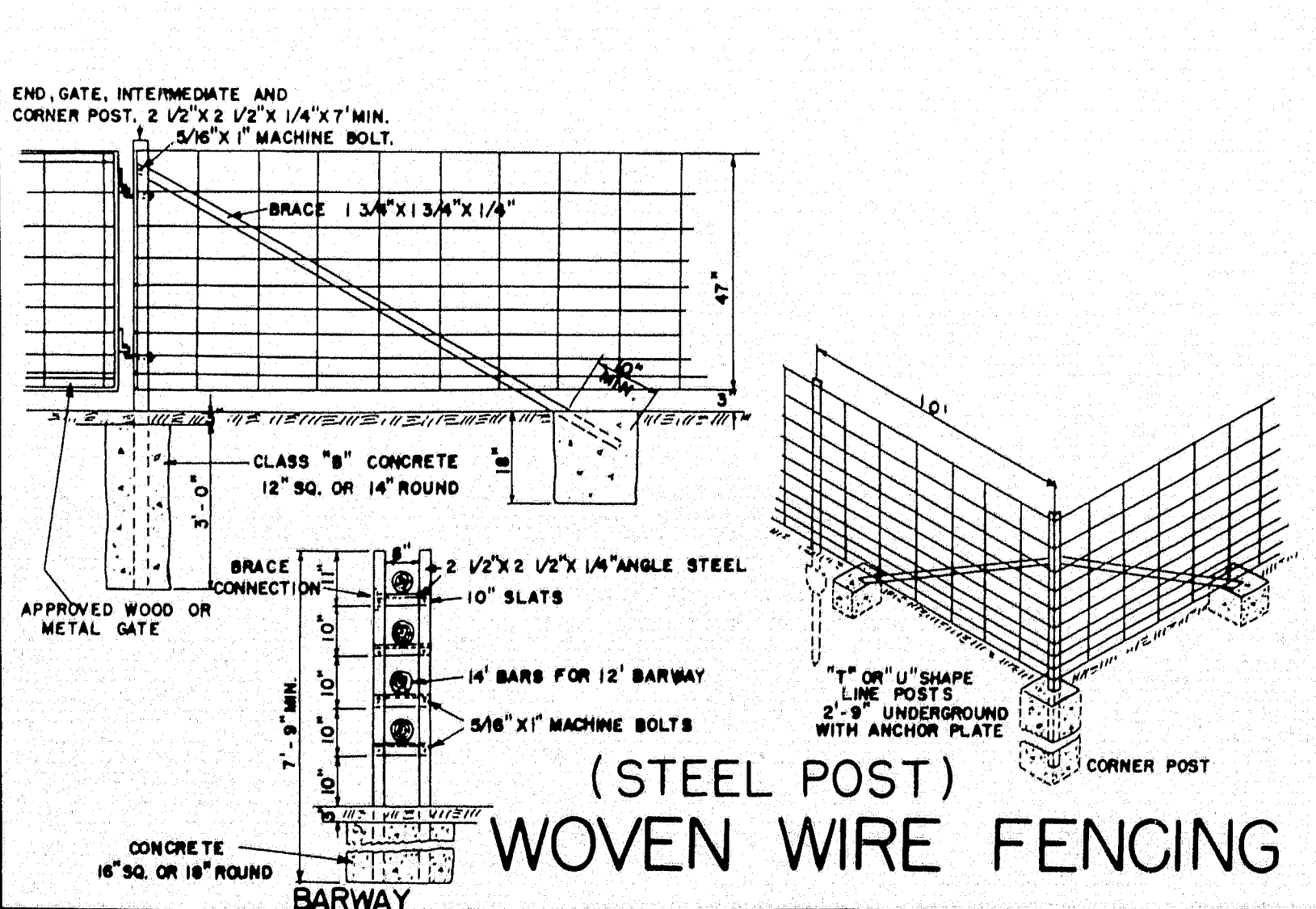
GUARD RAIL TYPE "H"



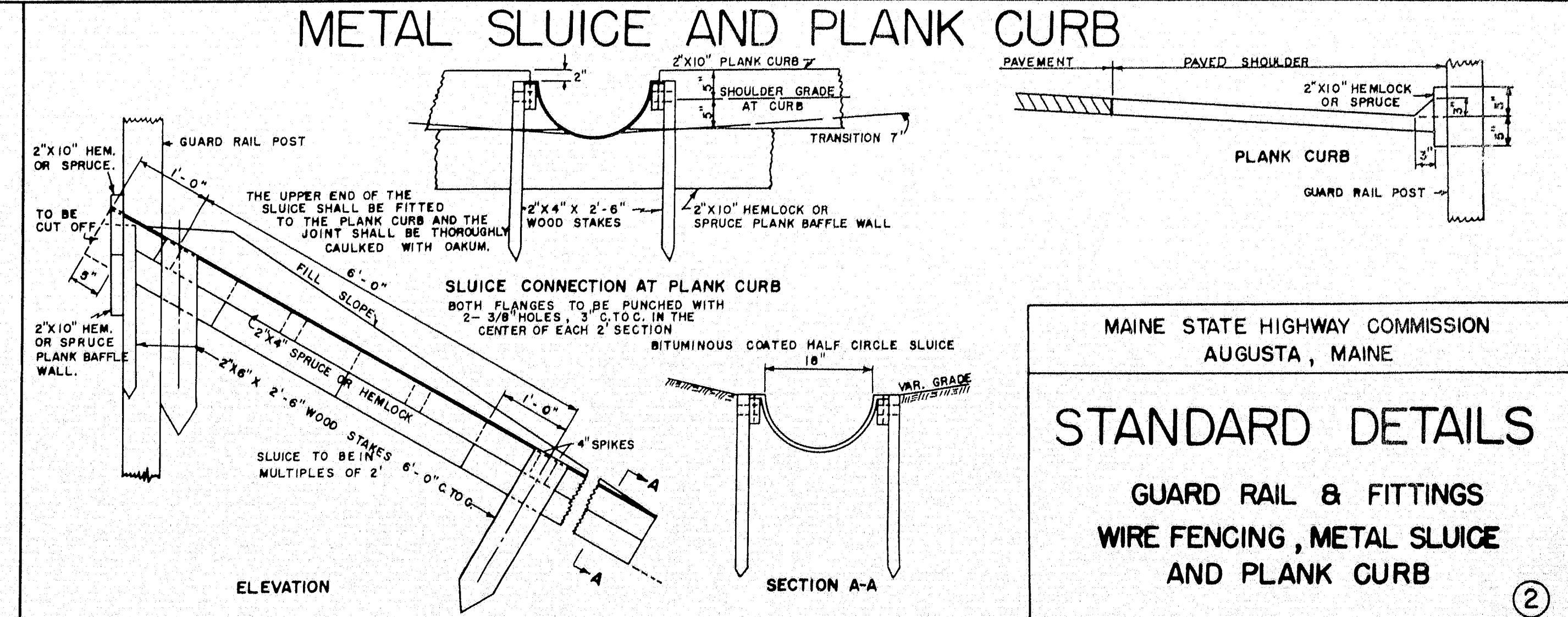
STANDARD FITTINGS
ALL FITTINGS TO BE GALVANIZED



(WOOD POST)
WOVEN WIRE FENCING



(STEEL POST)
WOVEN WIRE FENCING



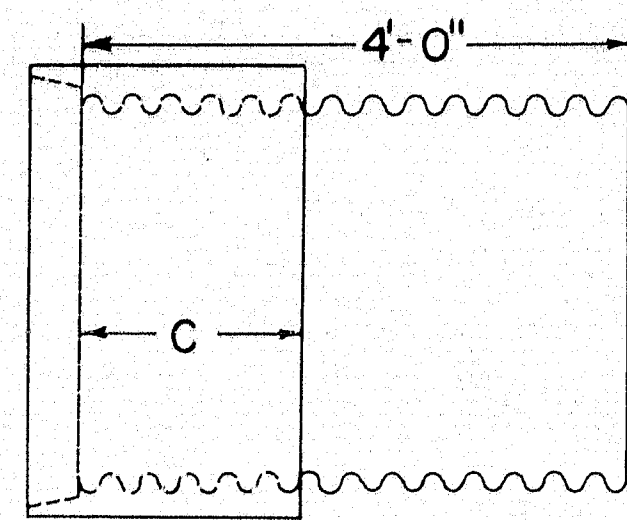
METAL SLUICE AND PLANK CURB

MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

STANDARD DETAILS

GUARD RAIL & FITTINGS
WIRE FENCING, METAL SLUICE
AND PLANK CURB

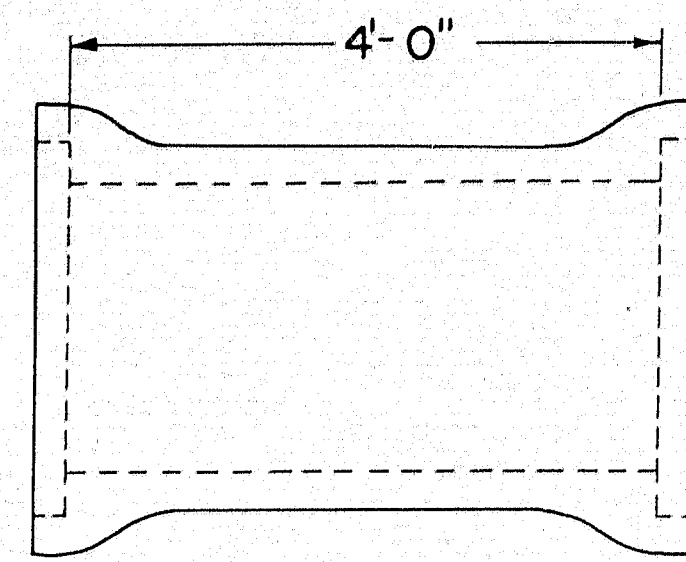
PIPE CONNECTIONS



GROOVE END COMBINATION
For 30" to 72" inclusive, diameter connection
between concrete and metal pipe

"C" = 17" min. for sizes 30" to 48" incl.
"C" = 23" min. for sizes over 48"

Asphalt coated corrugated metal pipe
shall conform to the latest
standard specifications

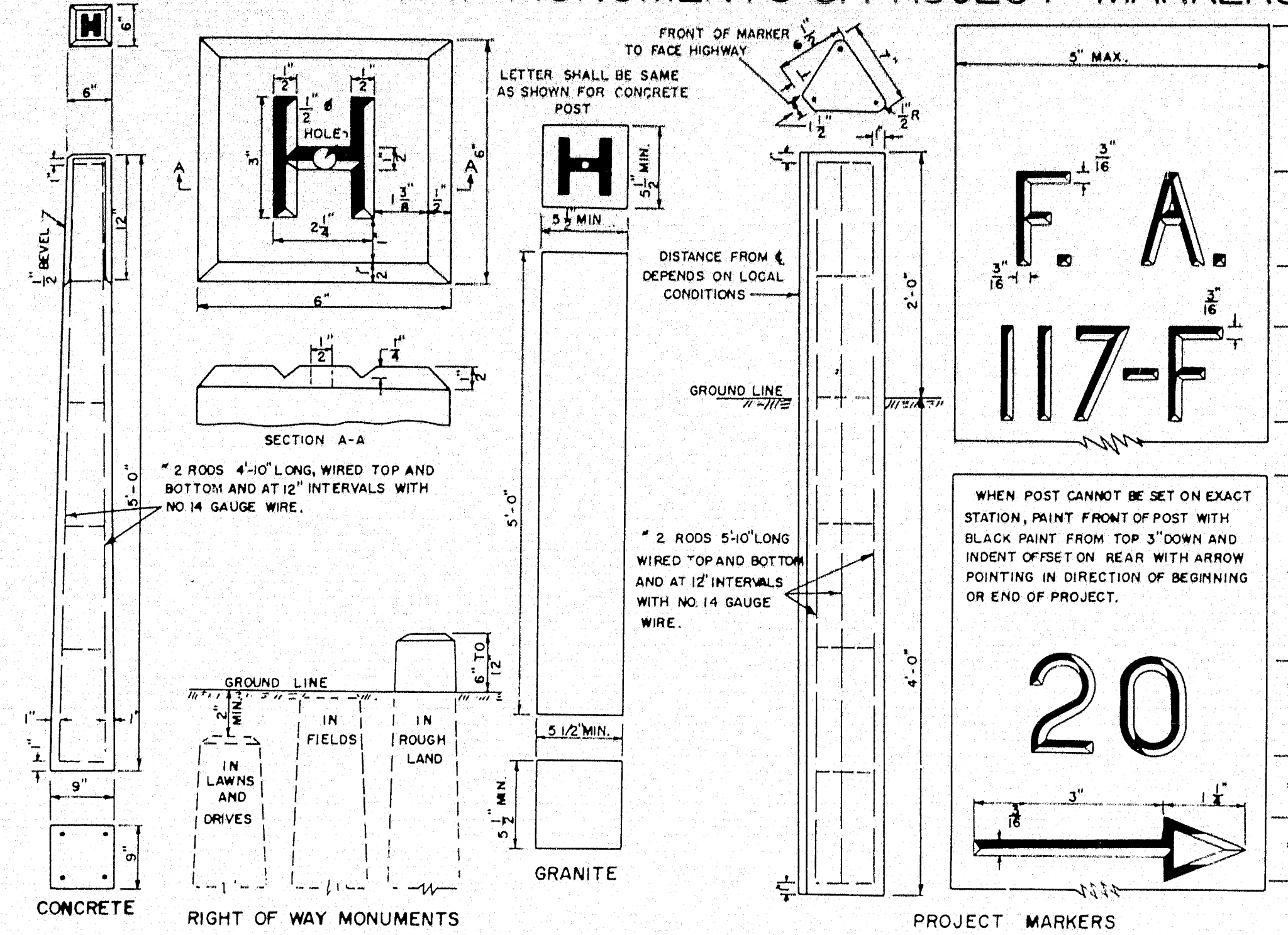


REINFORCED CONCRETE PIPE CONNECTOR
DOUBLE BELL

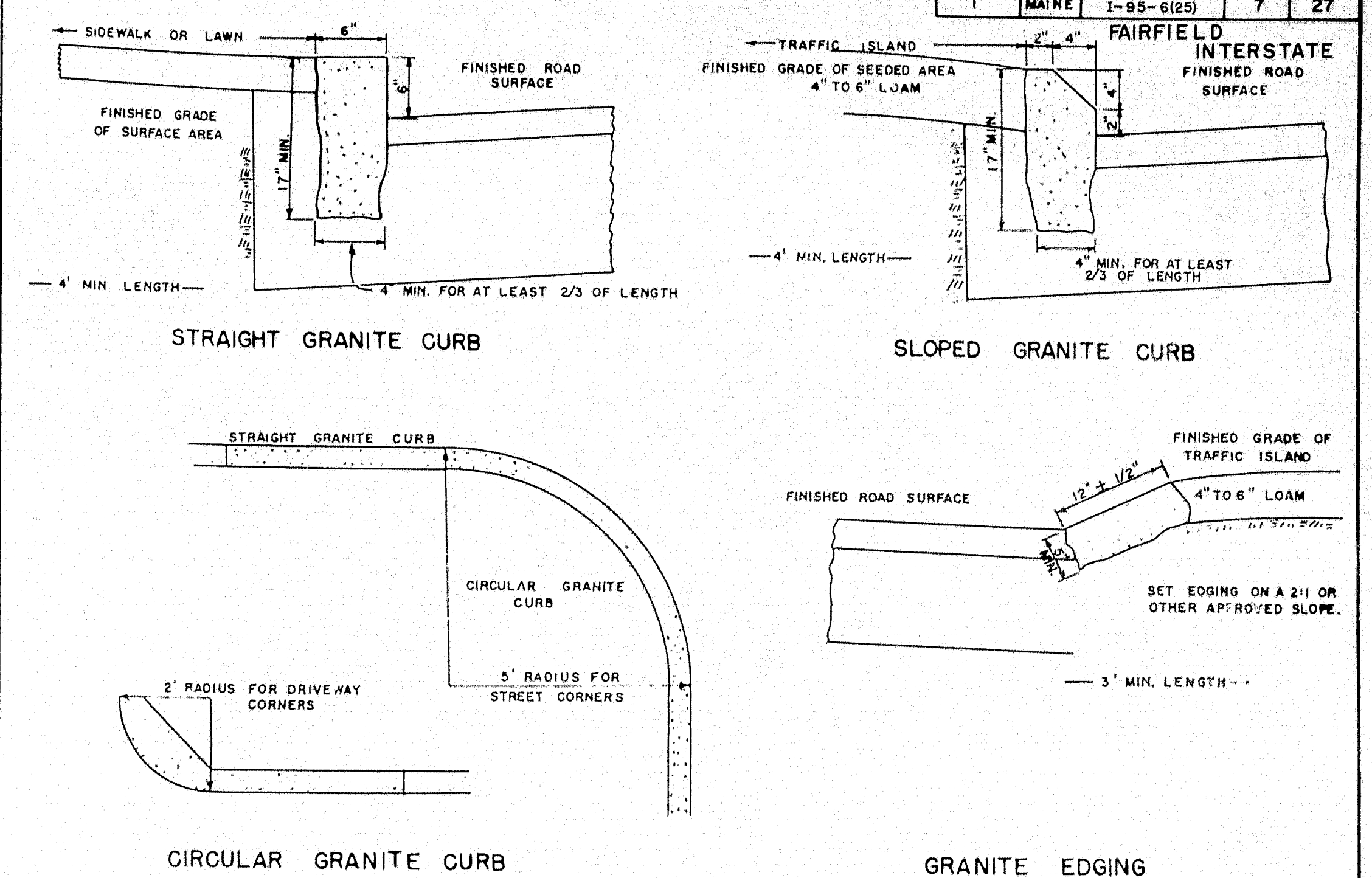
For 12" to 24" inclusive, diameter connection
between concrete and metal pipe

Reinforced concrete pipe shall
conform to the latest standard
specifications

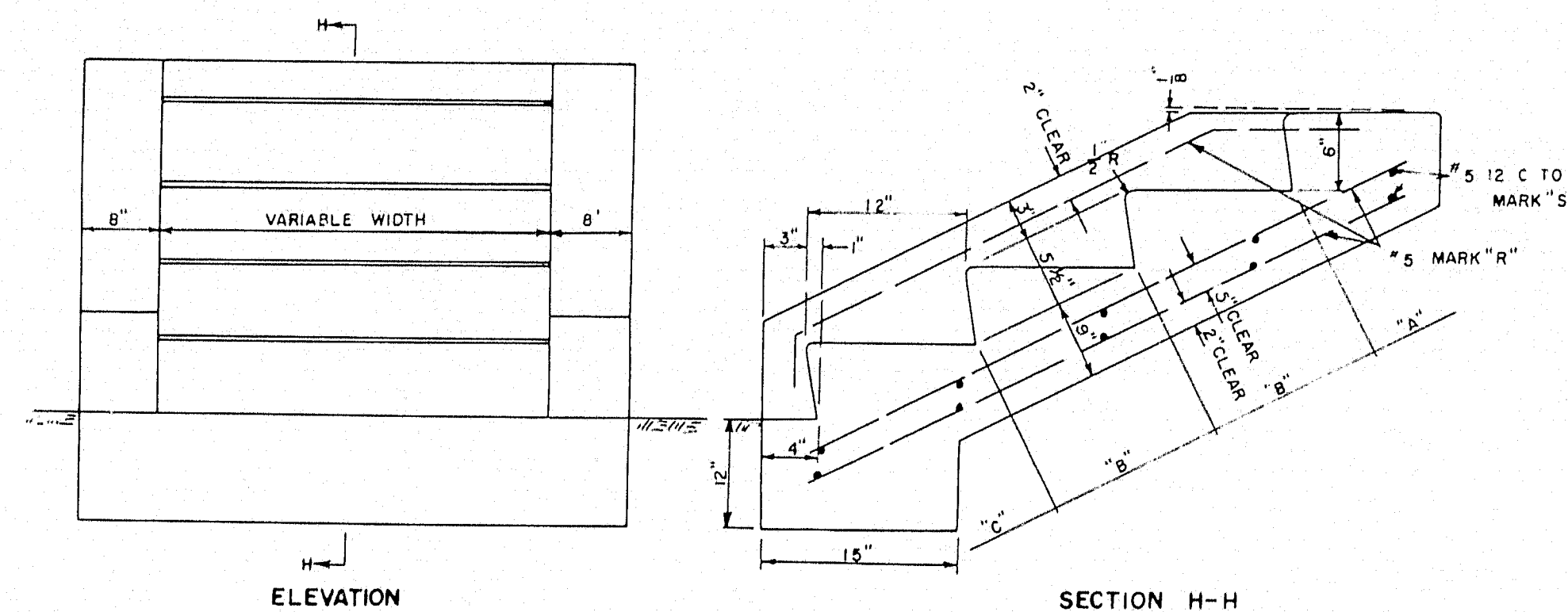
RIGHT OF WAY MONUMENTS & PROJECT MARKERS



GRANITE CURB & EDGING



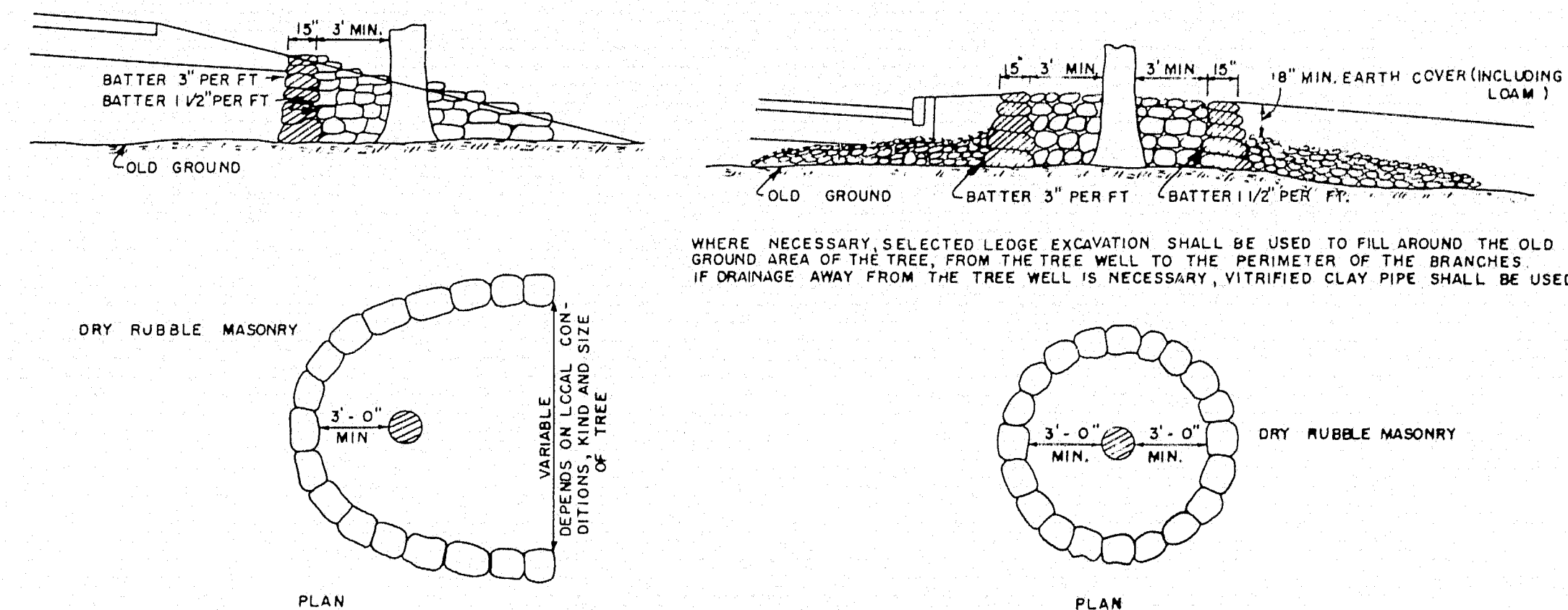
CONCRETE STEPS



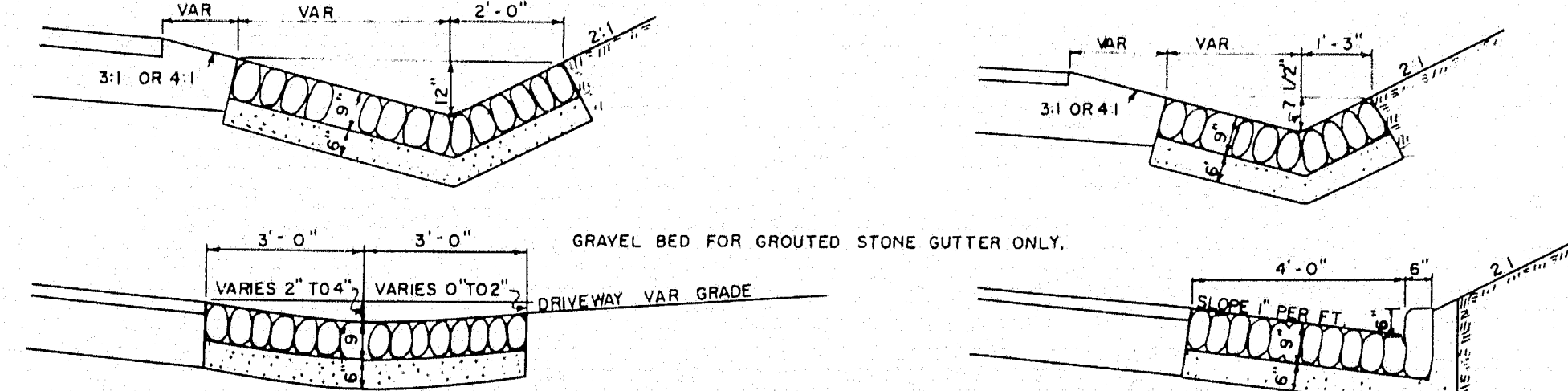
CONCRETE CLASS "A"		
SECTION	STEPS PER FT. OF WIDTH	PARAPET EACH WALL
"A" HEADER	.032 CU. YDS.	.022 CU. YDS.
"B" EA. INTER ST.	.040 CU. YDS.	.040 CU. YDS.
"C" FOOTER	.071 CU. YDS.	.065 CU. YDS.

REINFORCING STEEL			
MARK	SIZE	NUMBER	LENGTH (EACH)
R	#5	3 EACH PARAPET 2 EACH FT. OF WIDTH	8" FOR "A" "H" FOR EACH "B" "H" FOR "C"
S	#5	2 FOR "A" 2 FOR EACH "B" 4 FOR "C"	6 EACH PARAPET H/2 PER FT. OF WIDTH

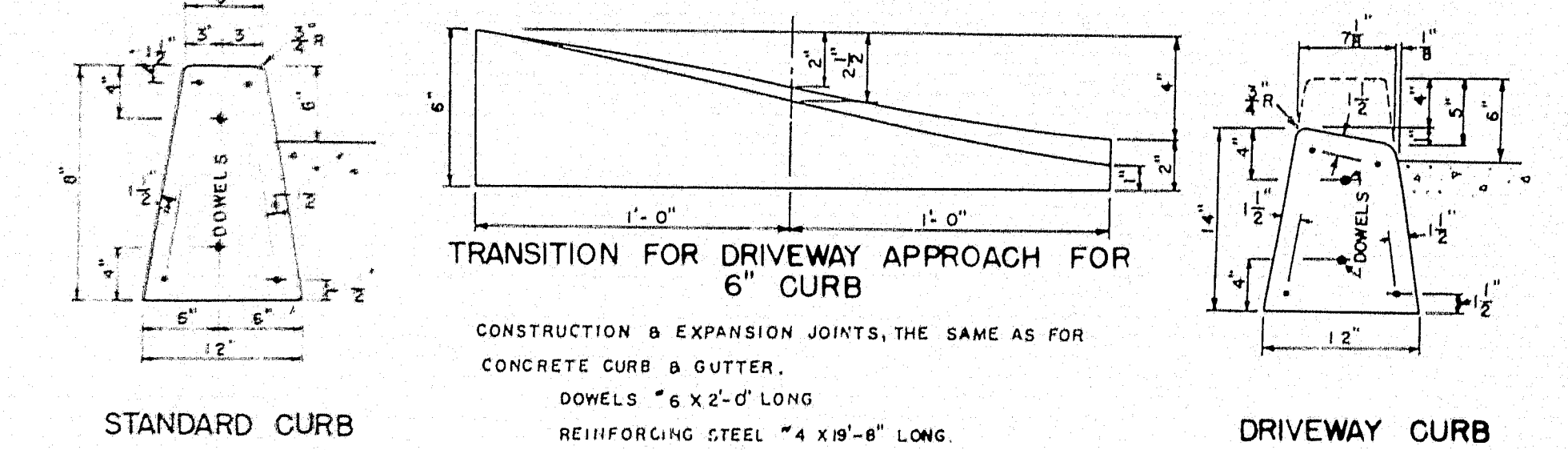
TREE WELLS



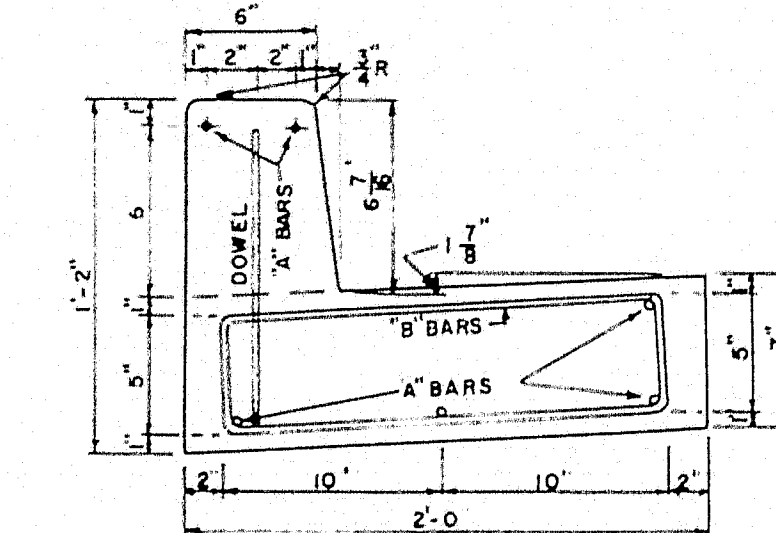
STONE GUTTER



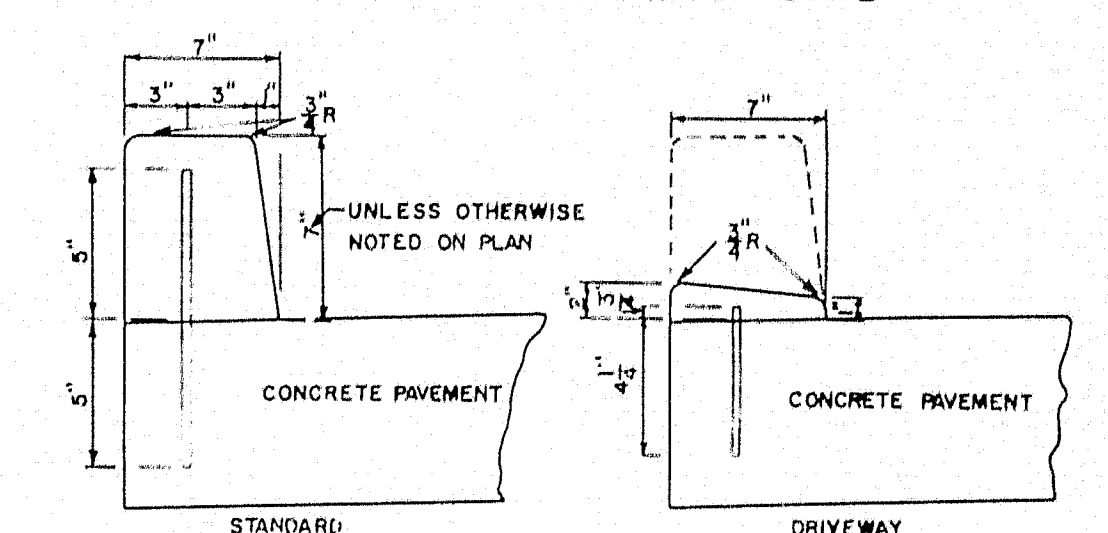
CONCRETE CURB



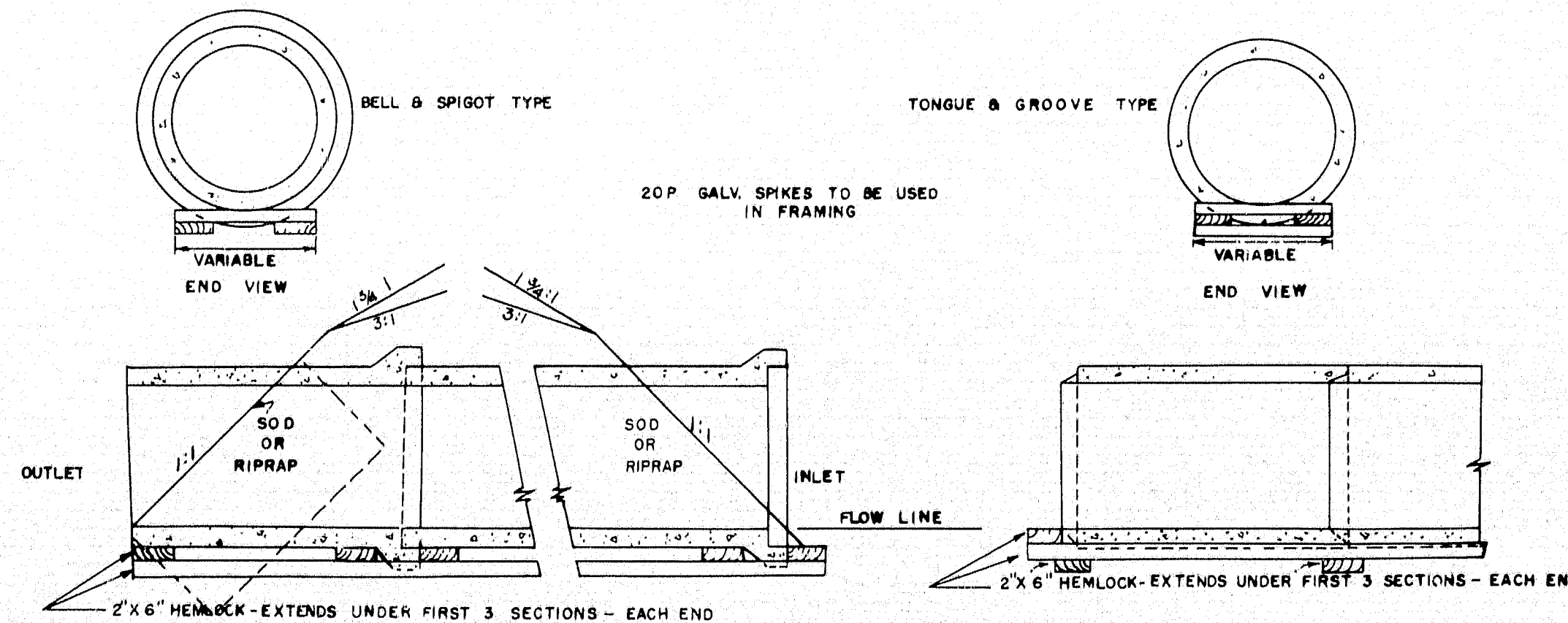
CONCRETE CURB & GUTTER



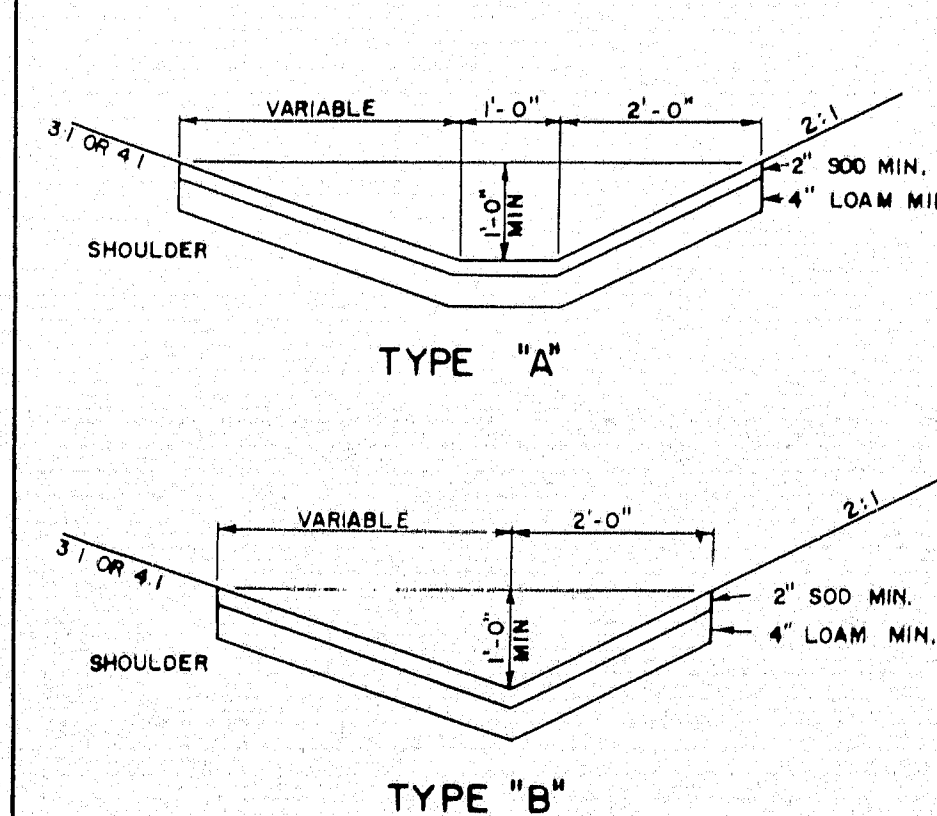
INTEGRAL CONCRETE CURB



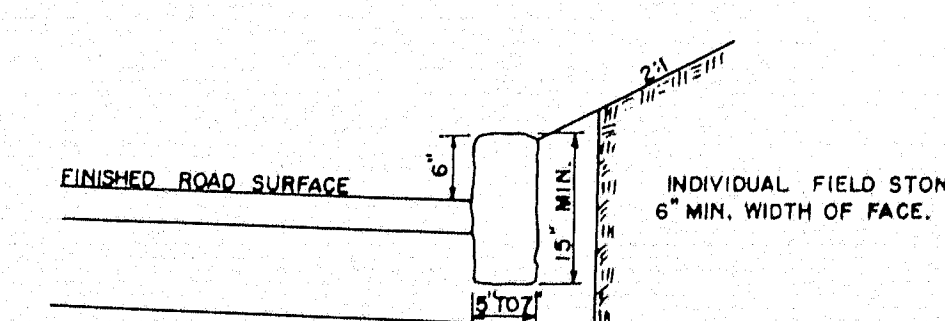
CONCRETE PIPE CRADLE



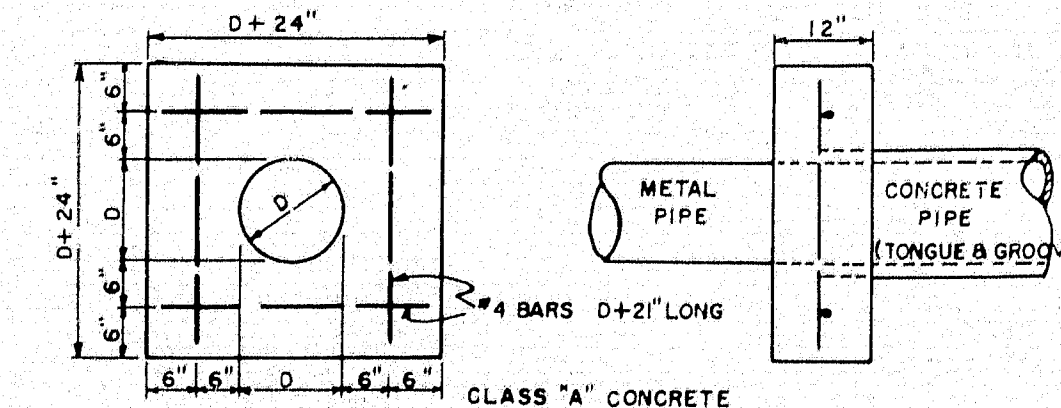
SODDED GUTTER



FIELD STONE CURB

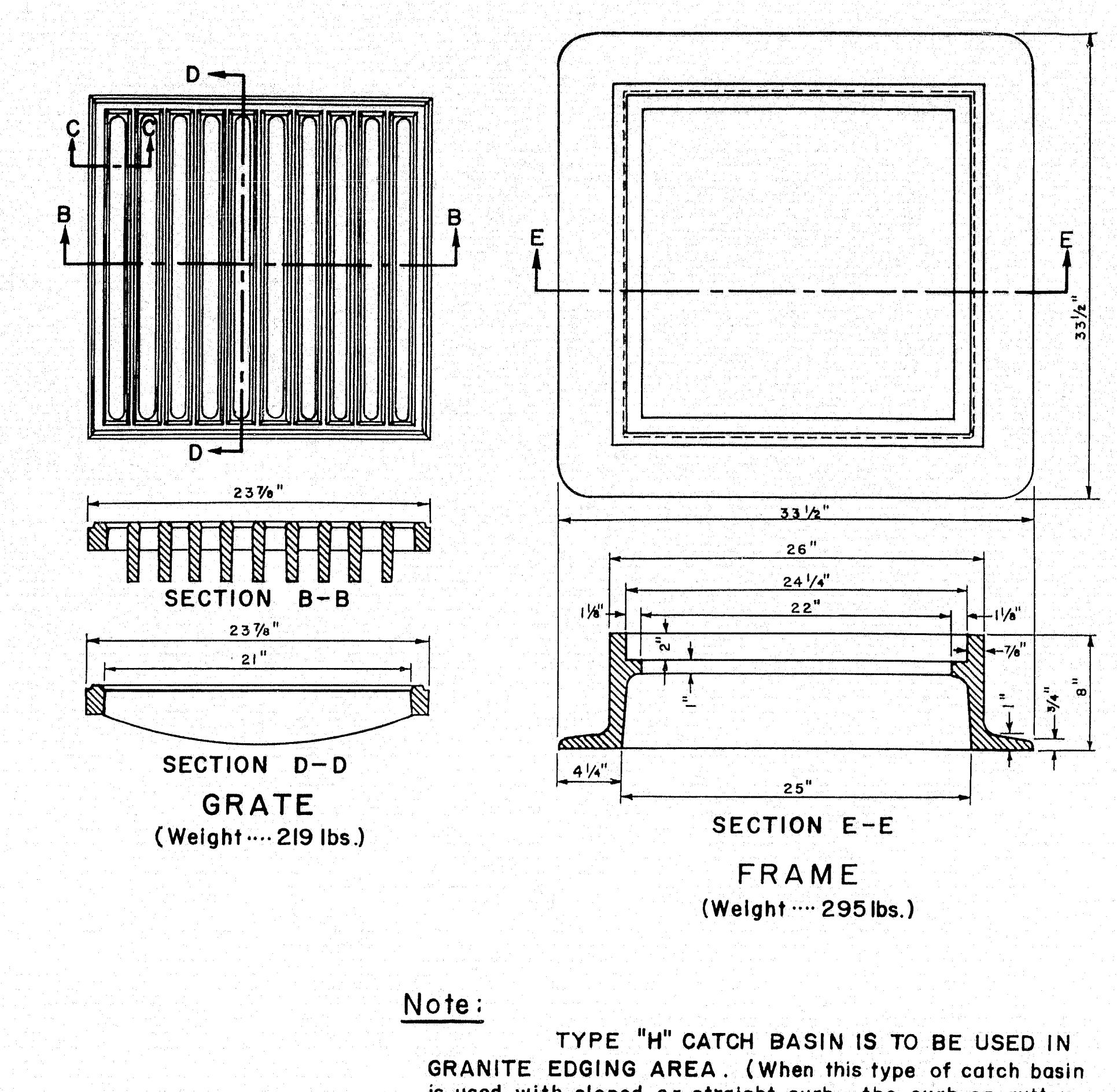
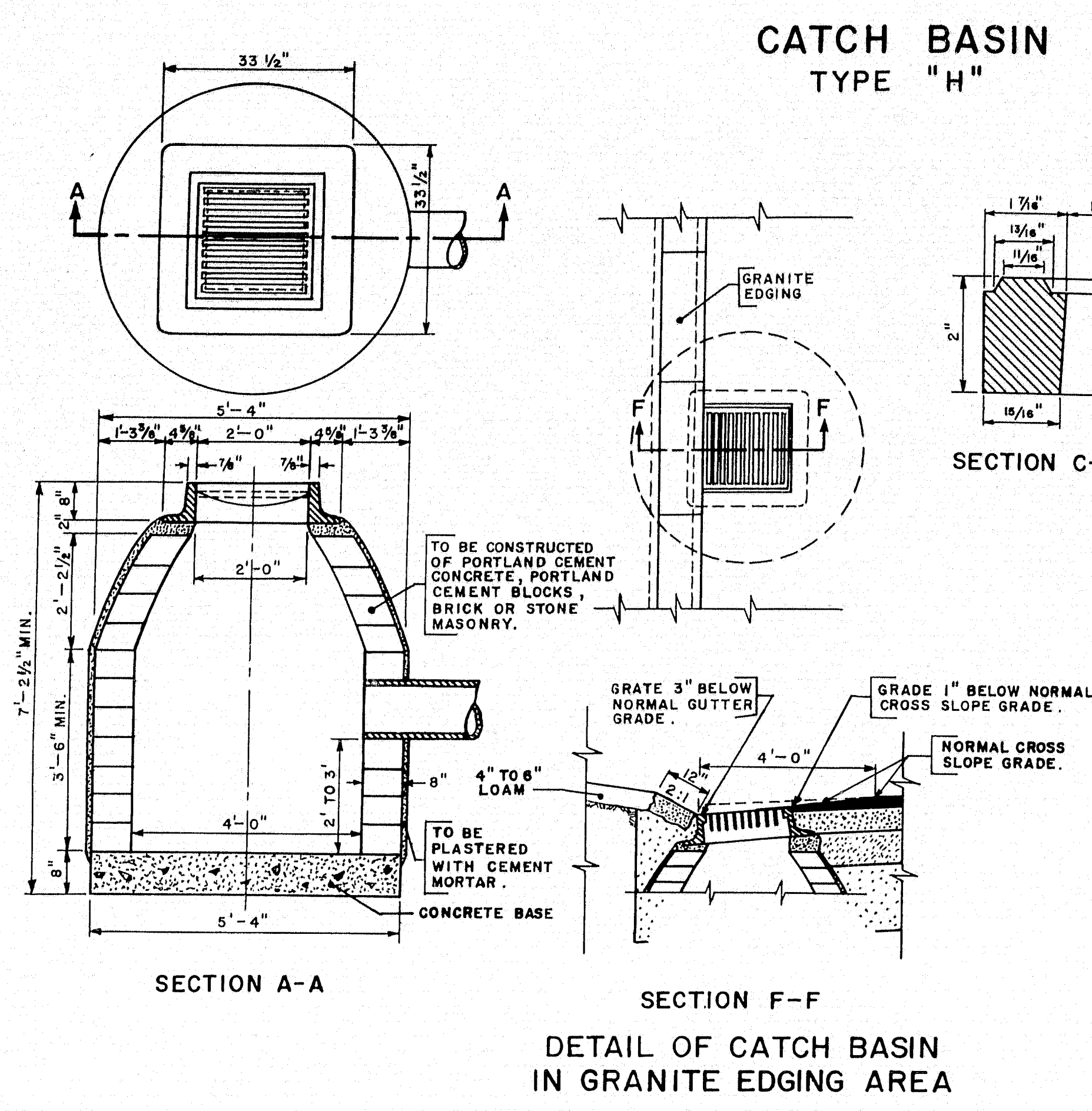


CONCRETE COLLAR

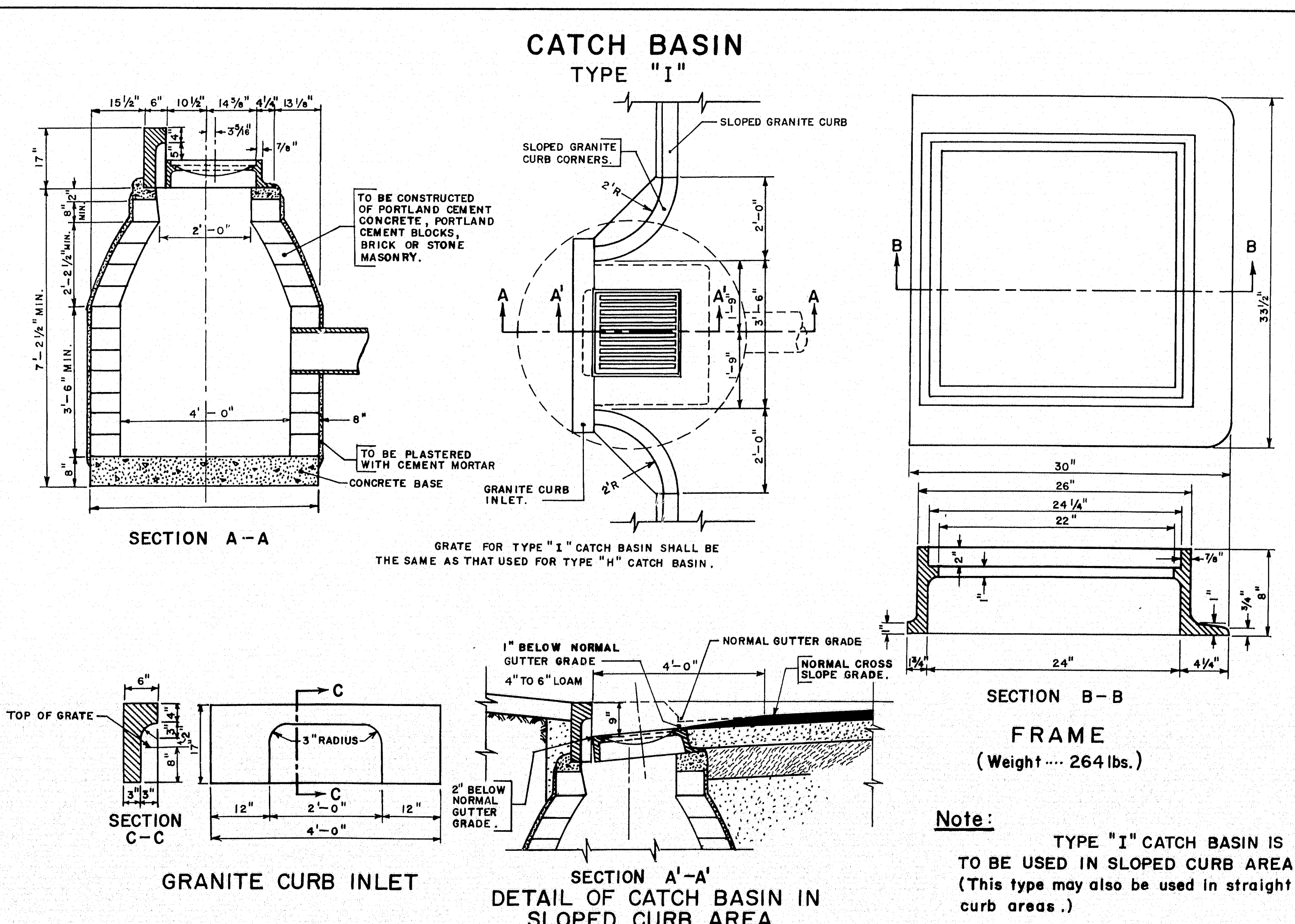
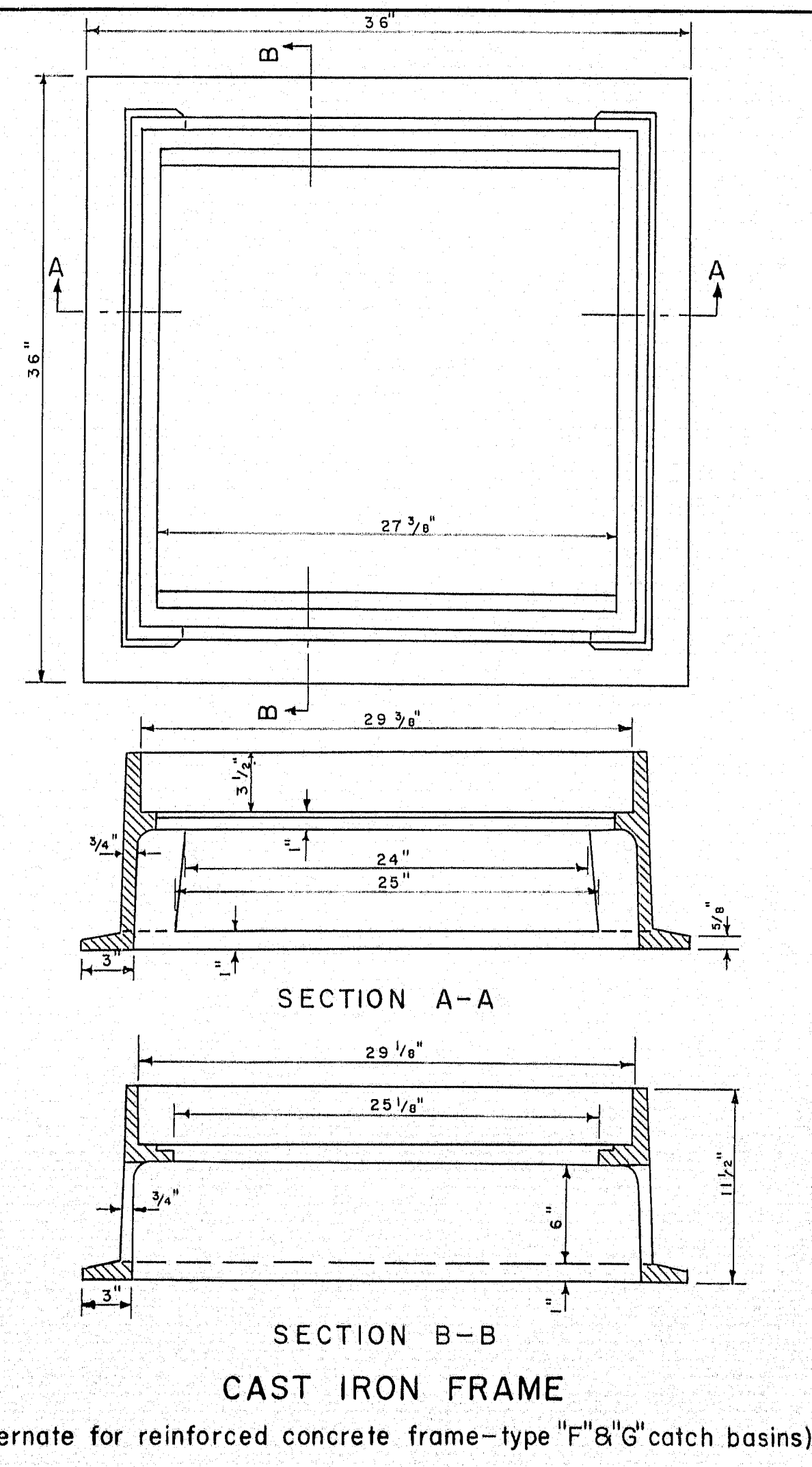
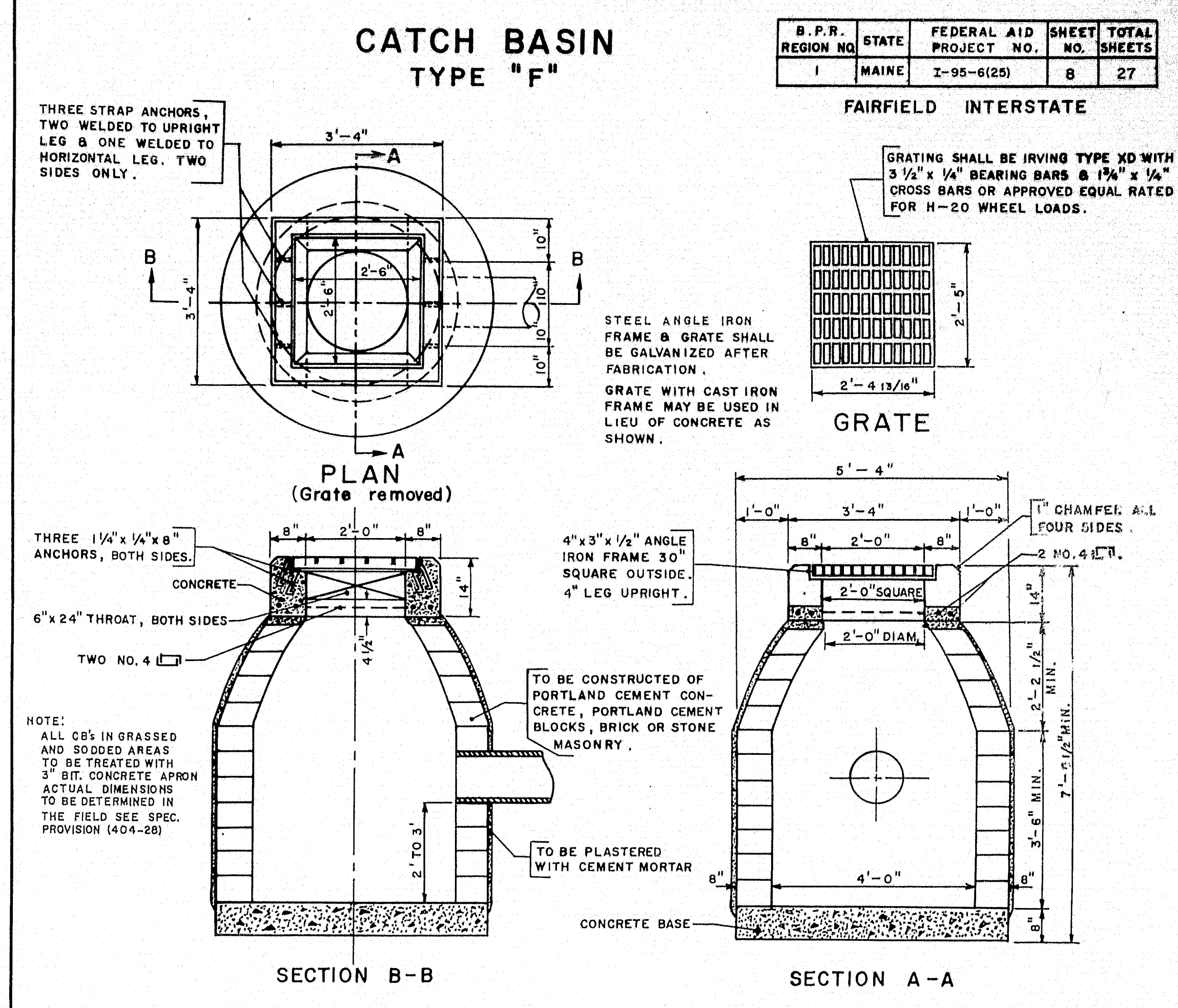


MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

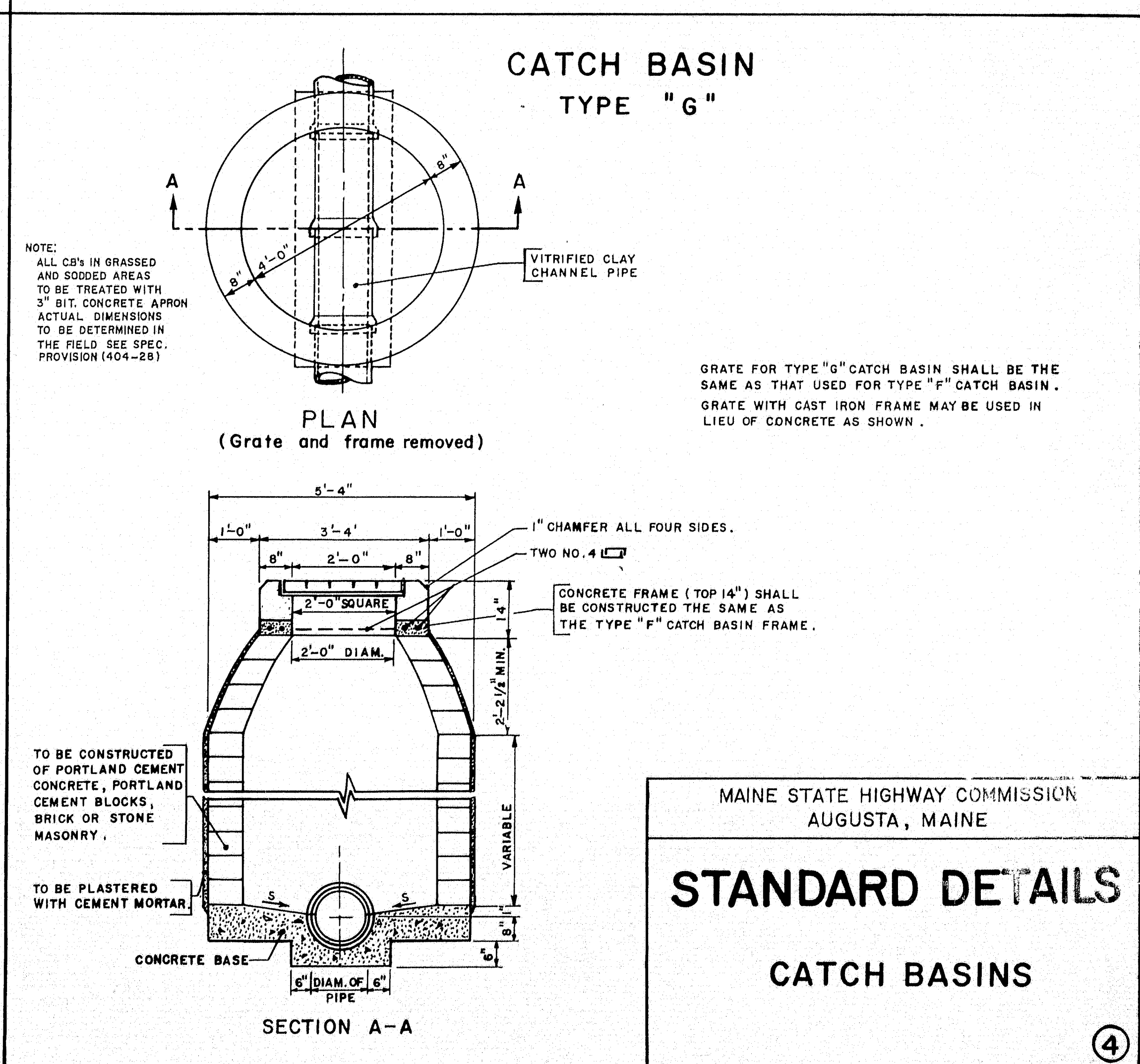
STANDARD DETAILS
MISCELLANEOUS ITEMS



Note: TYPE "H" CATCH BASIN IS TO BE USED IN GRANITE EDGING AREA. (When this type of catch basin is used with sloped or straight curb, the curb on gutter line shall be cut to fit flange.)



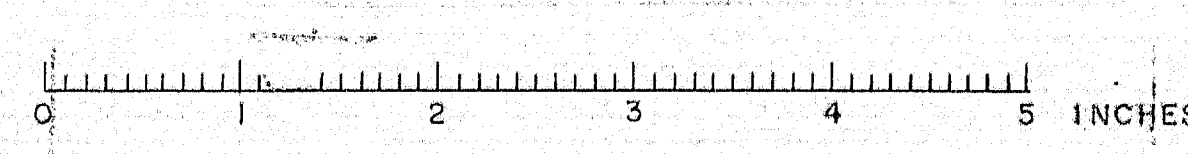
Note: TYPE "I" CATCH BASIN IS TO BE USED IN SLOPED CURB AREA. (This type may also be used in straight curb areas.)



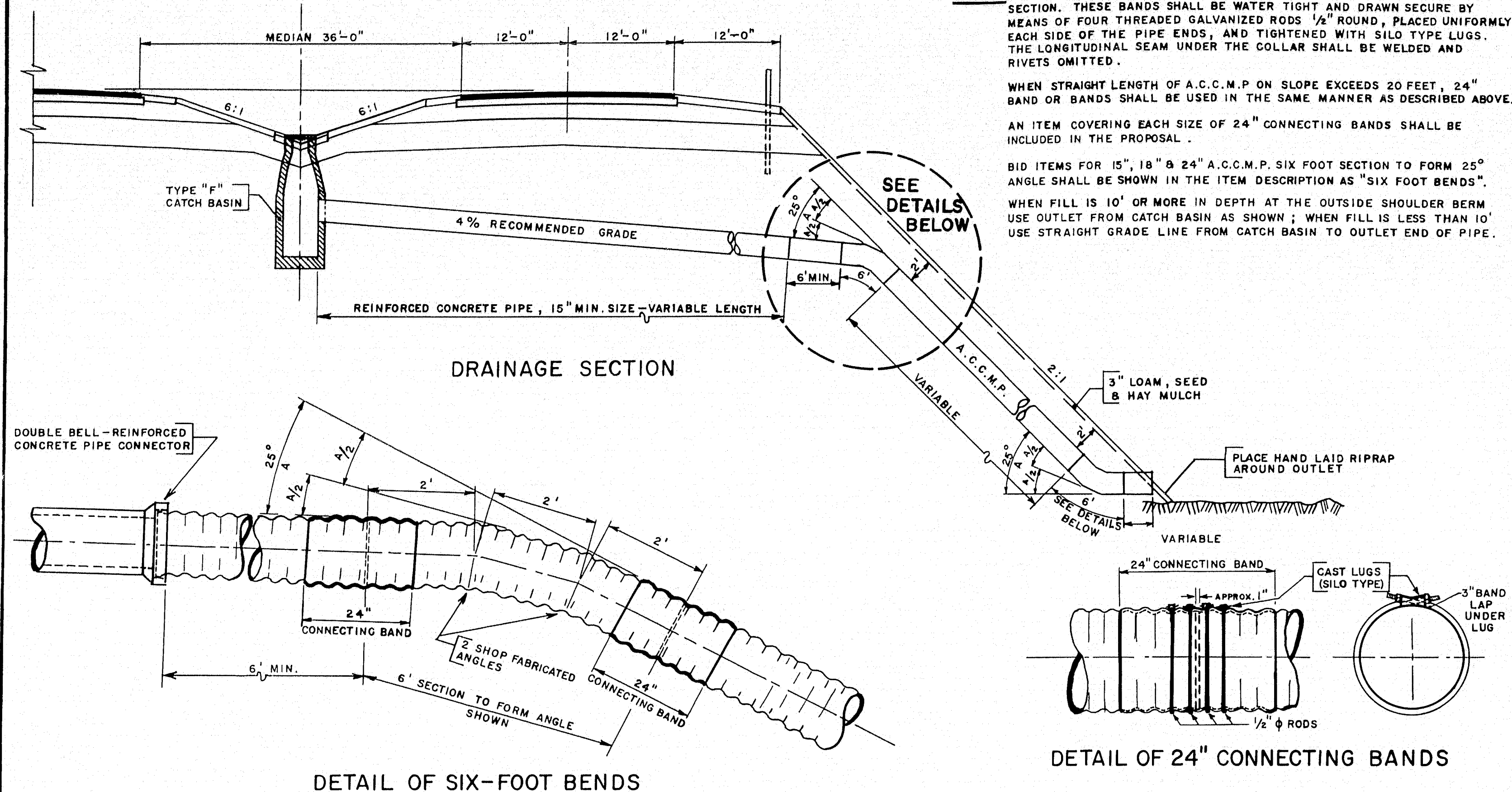
MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

STANDARD DETAILS

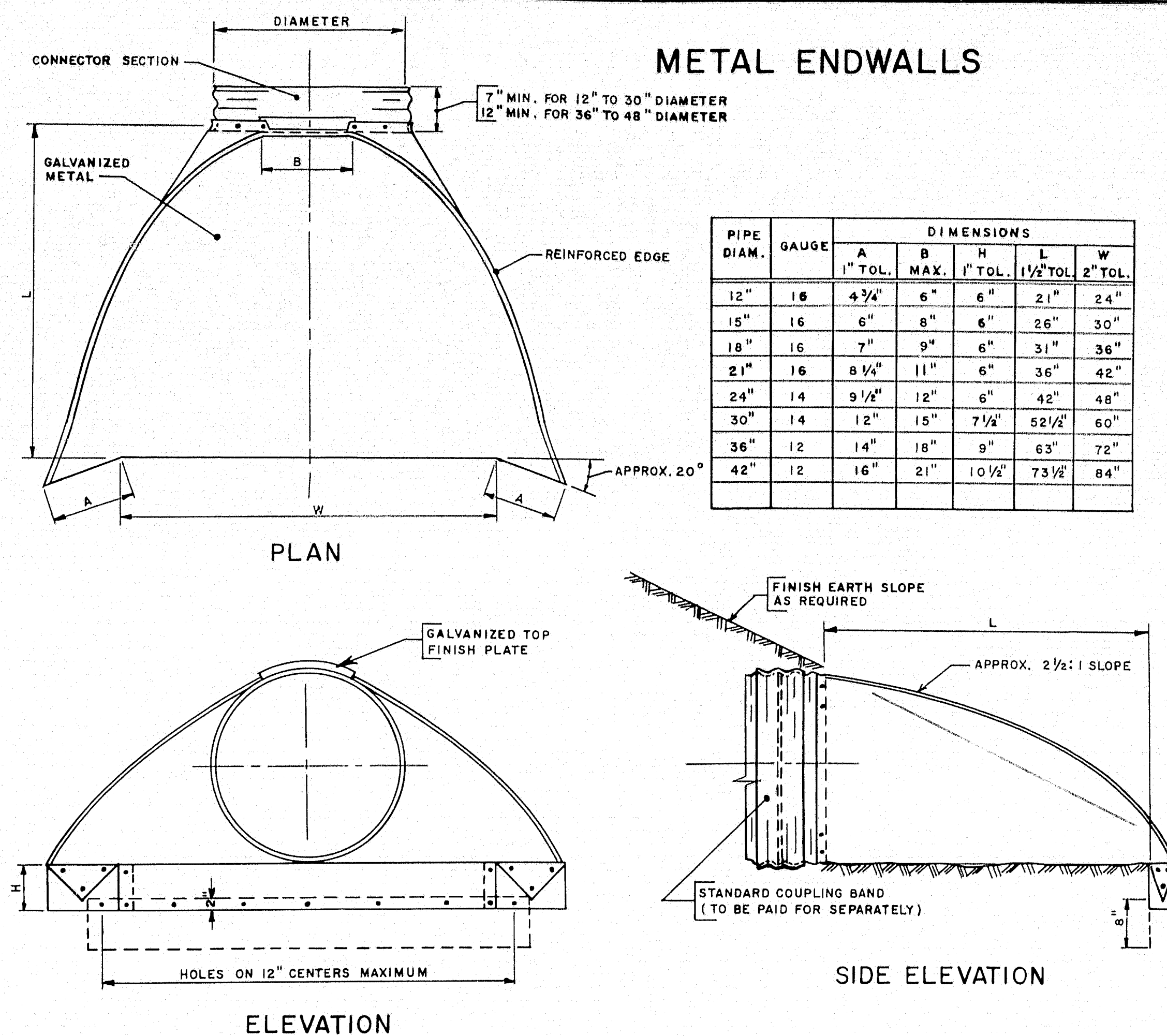
CATCH BASINS



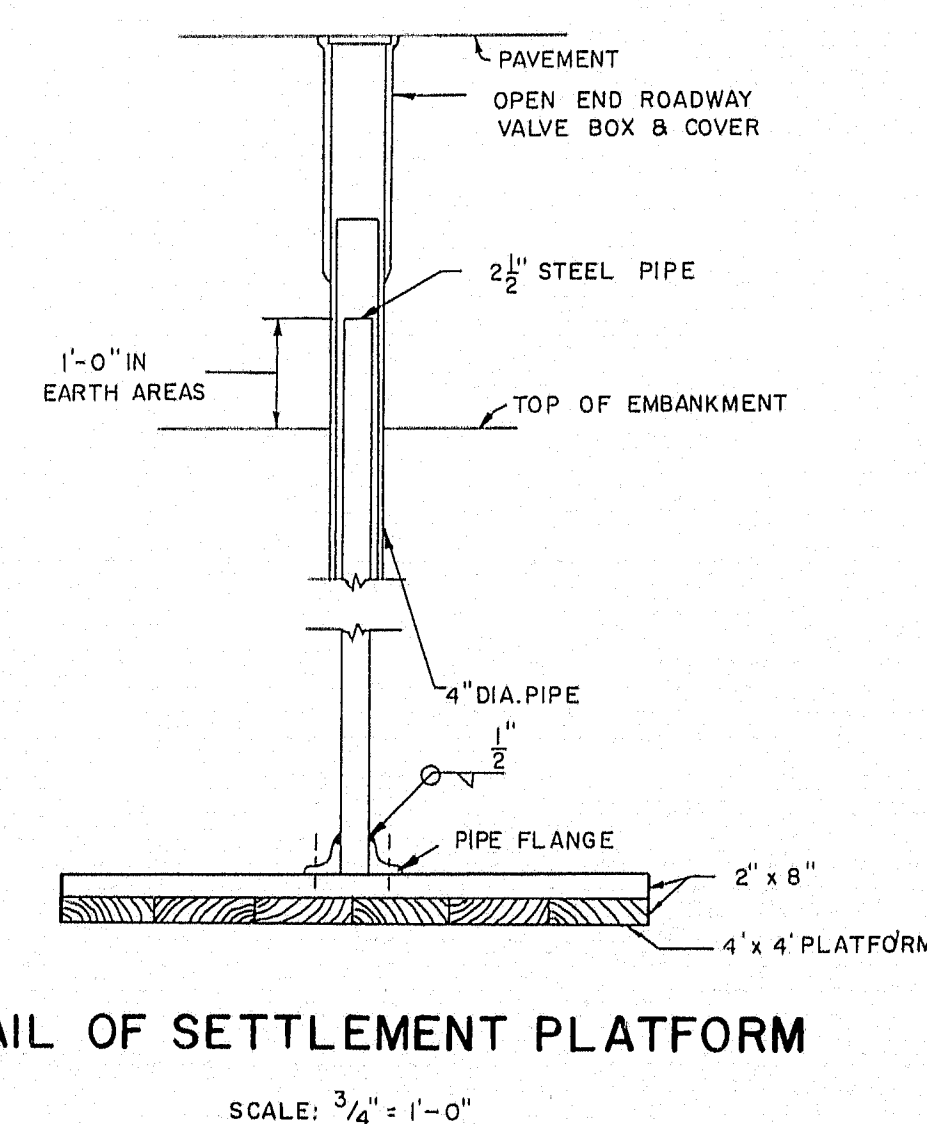
BENDS AND BANDS for A.C.C.M.P. MEDIAN DRAINAGE



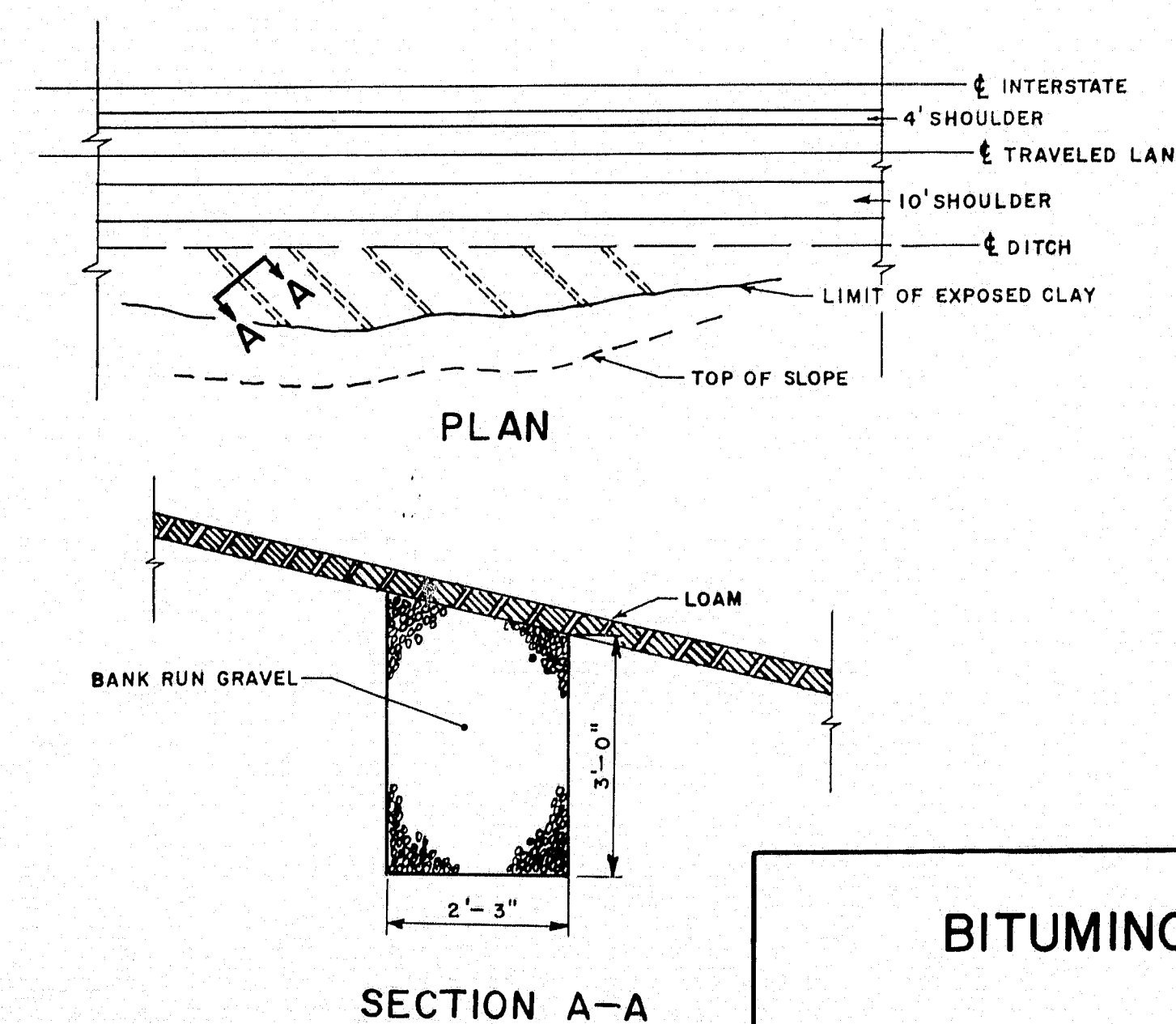
METAL ENDWALLS



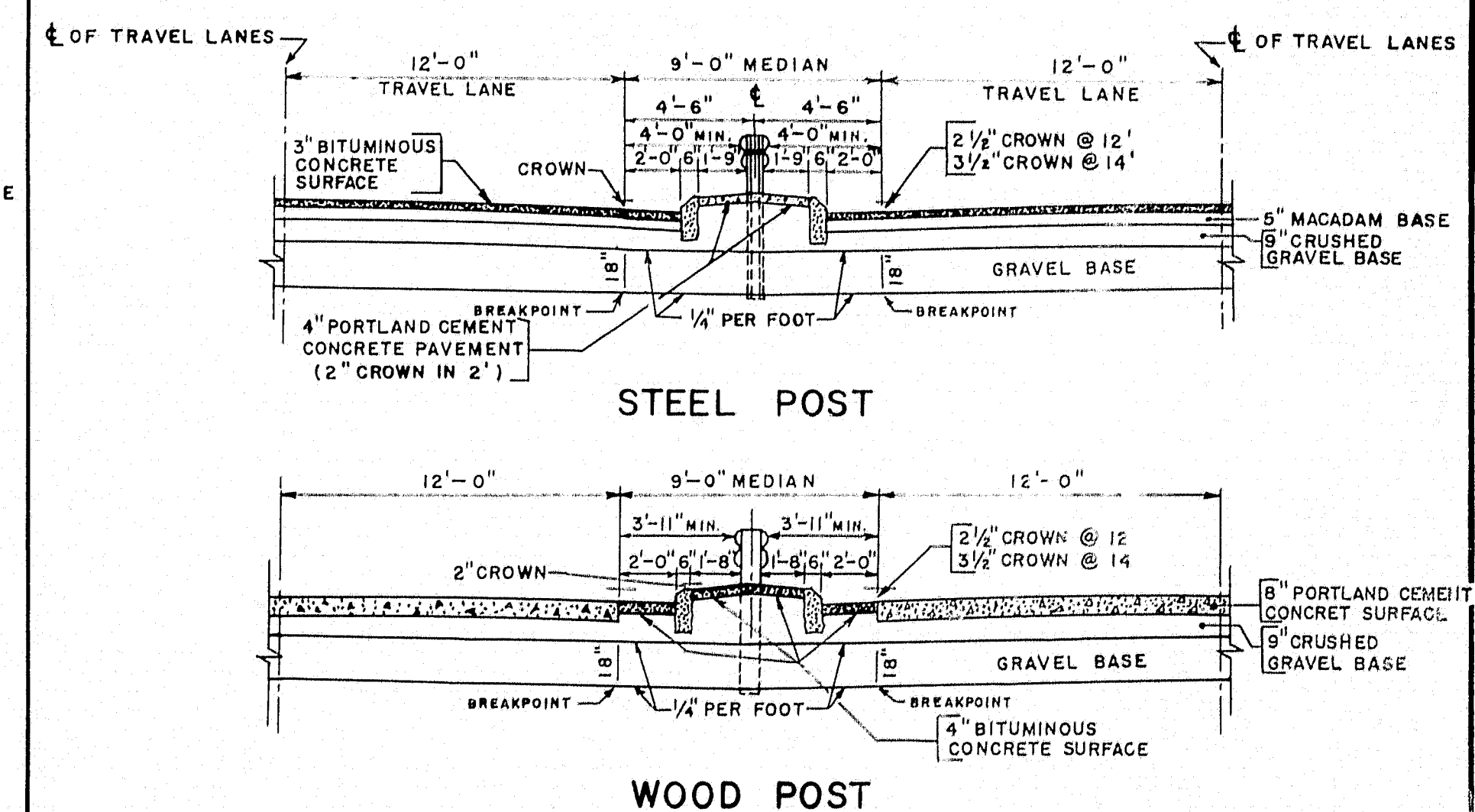
SETTLEMENT PLATFORM



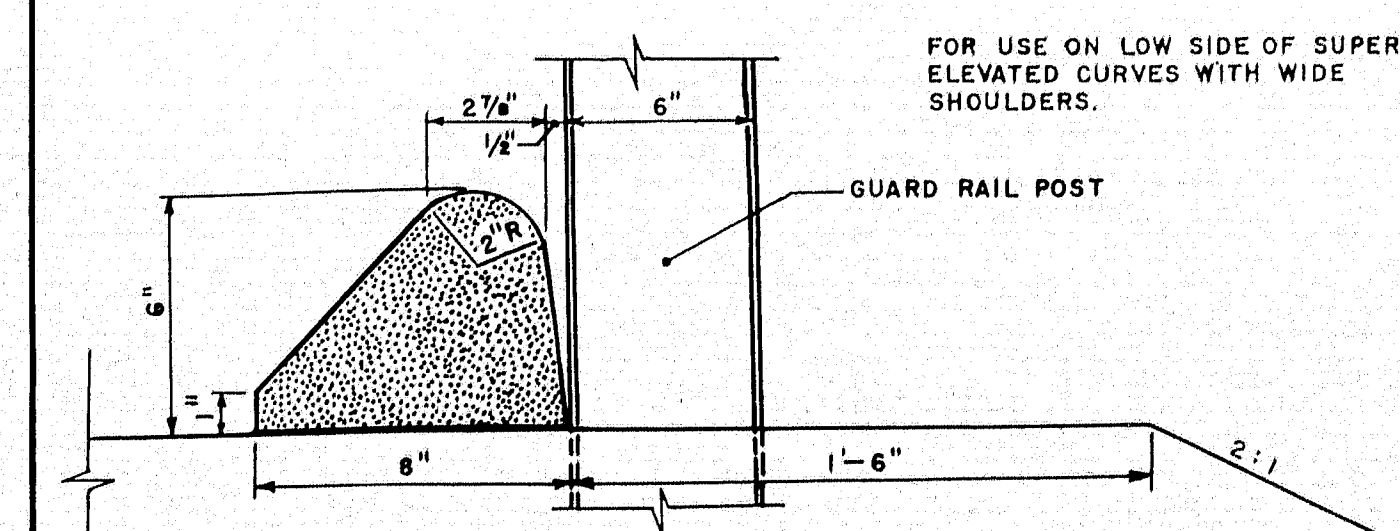
CUT SLOPE DRAIN



9-FOOT MEDIAN



BITUMINOUS CONCRETE CURB



MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

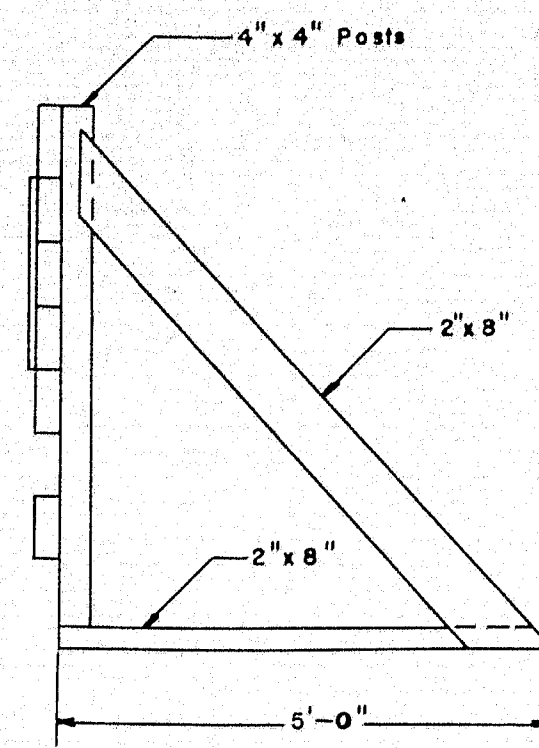
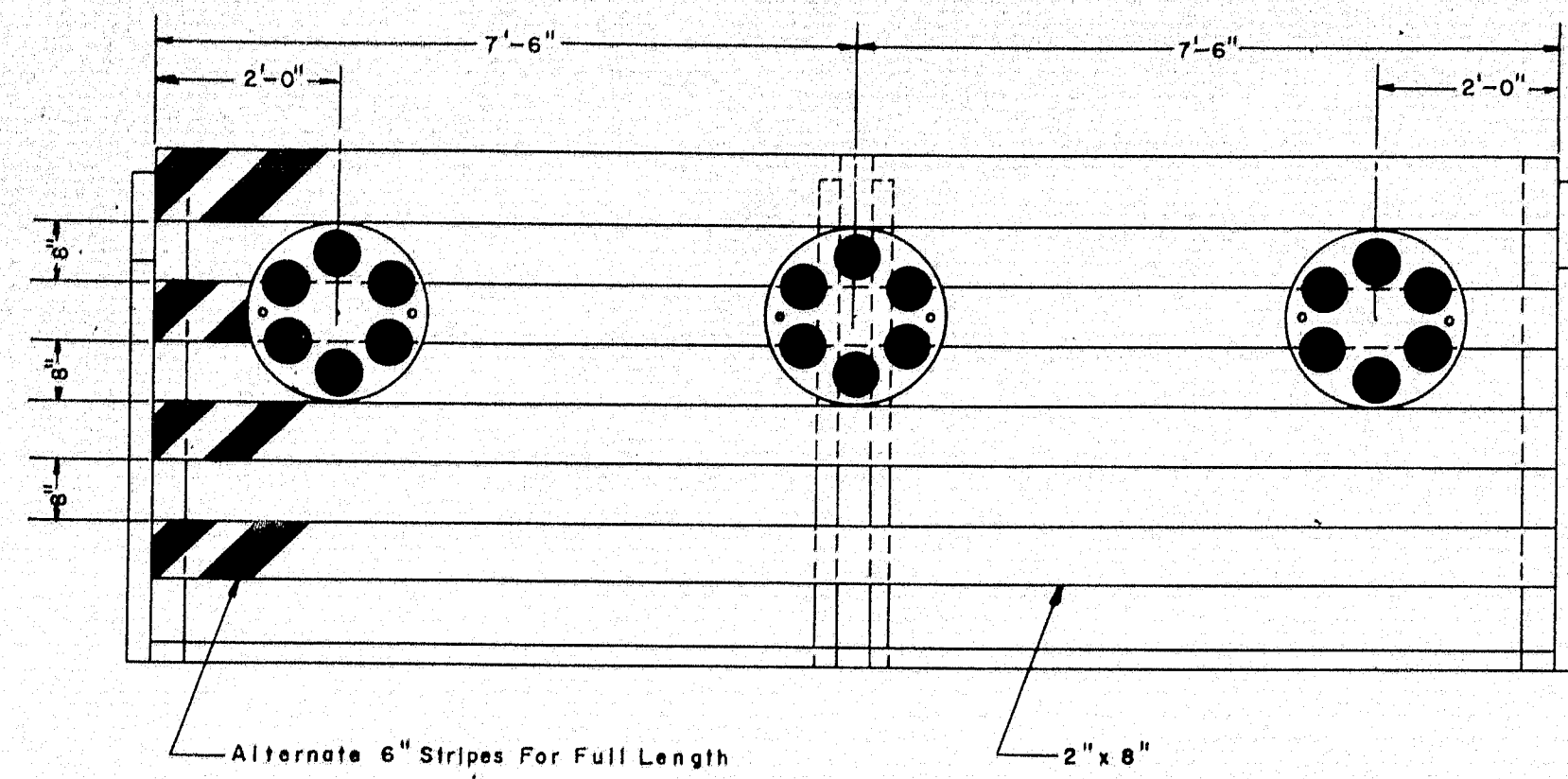
STANDARD DETAILS

BENDS & BANDS, METAL ENDWALLS,
GUARD RAIL ON RAMPS, CUT SLOPE
DRAIN, 9-FOOT MEDIAN & BITUMINOUS
CONCRETE CURB

5

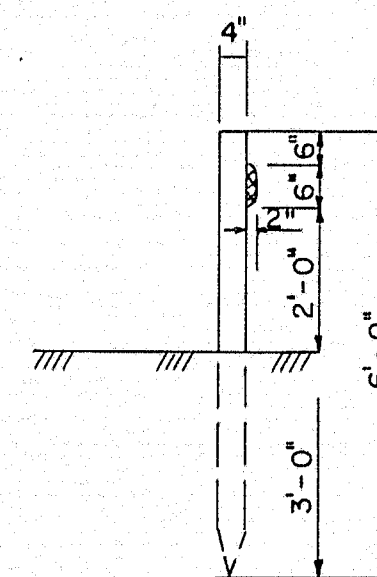
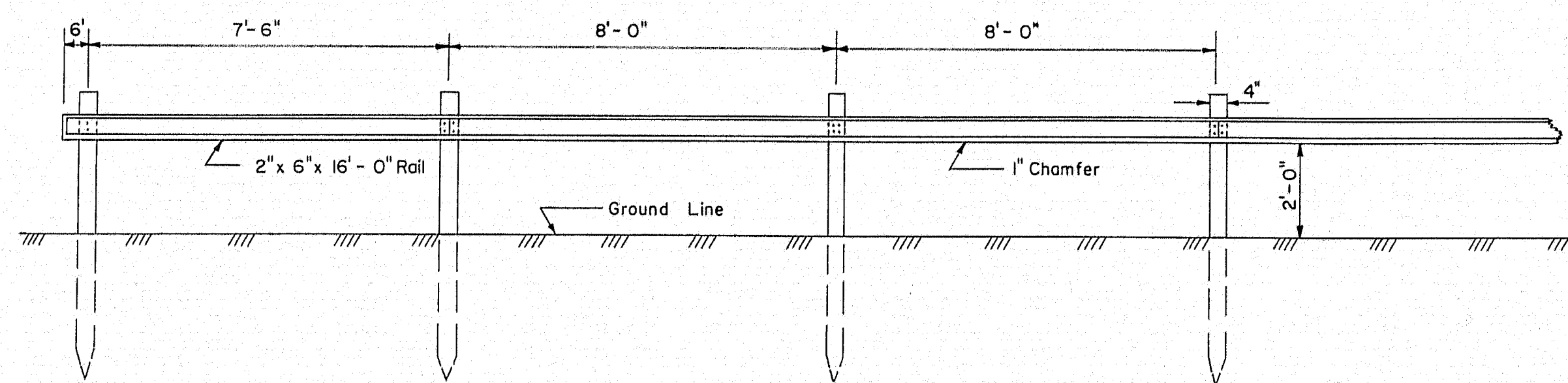
B. P. R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-6(25)	10	27

FAIRFIELD INTERSTATE



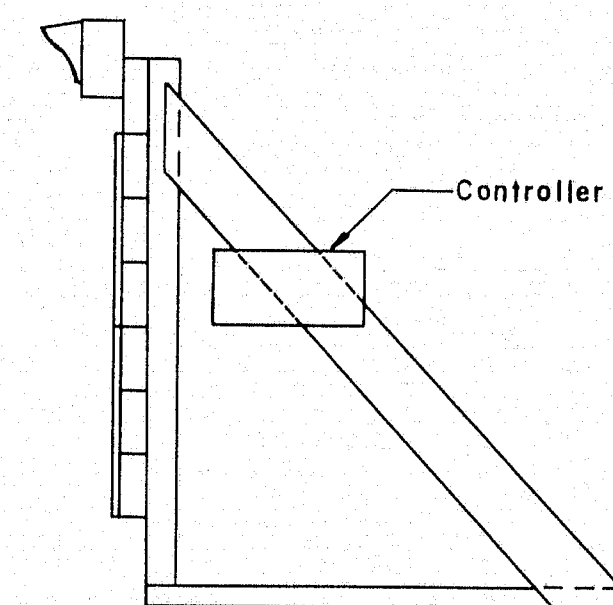
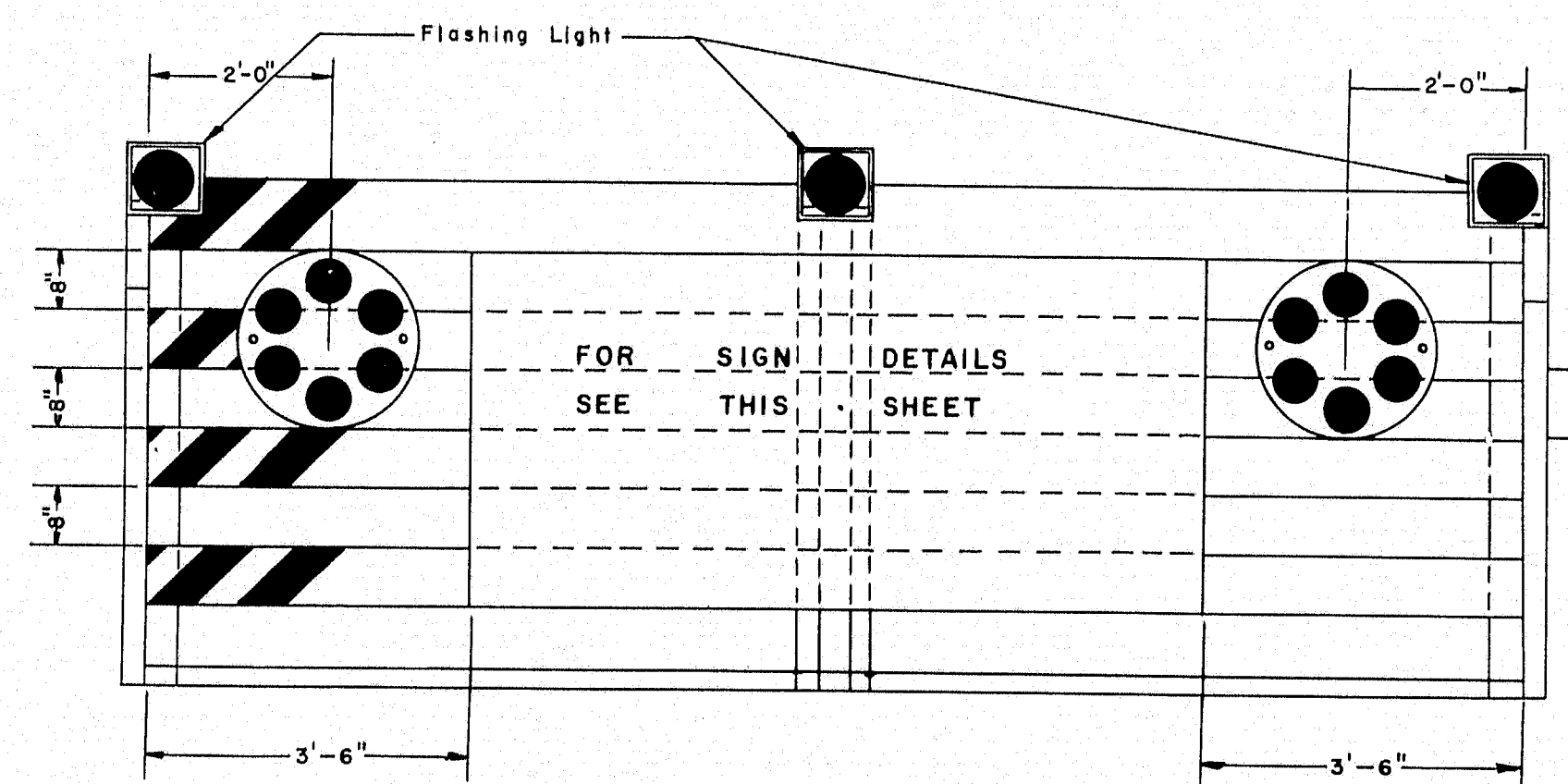
PORTABLE BARRICADE

Scale: $\frac{1}{2}" = 1'$



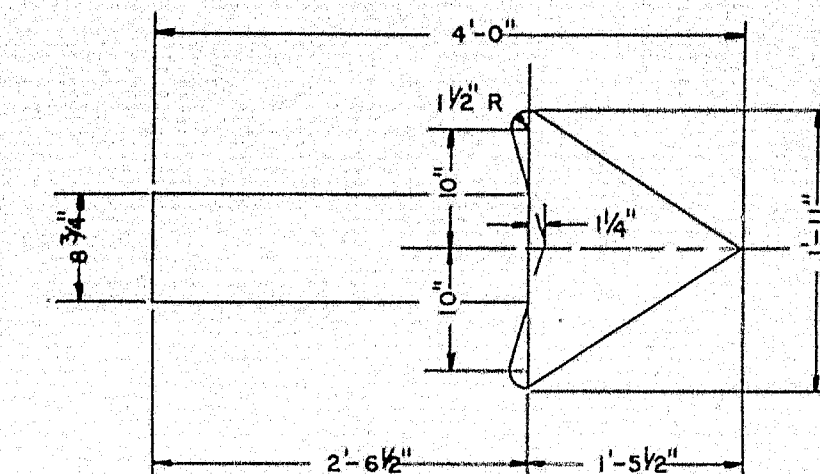
TEMPORARY WOODEN GUARD FENCE

Scale: $\frac{3}{8}" = 1'$

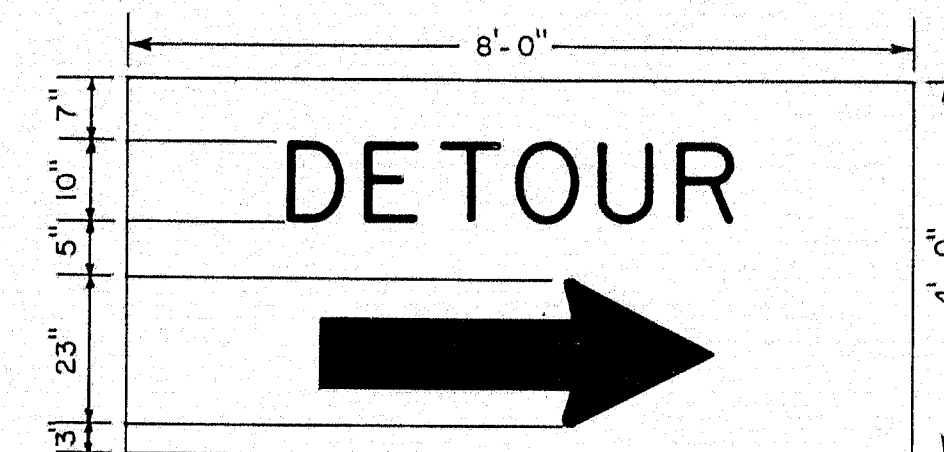


PORTABLE BARRICADE
WITH FLASHING LIGHTS
AND DETOUR SIGN

Scale: $\frac{1}{2}" = 1'$

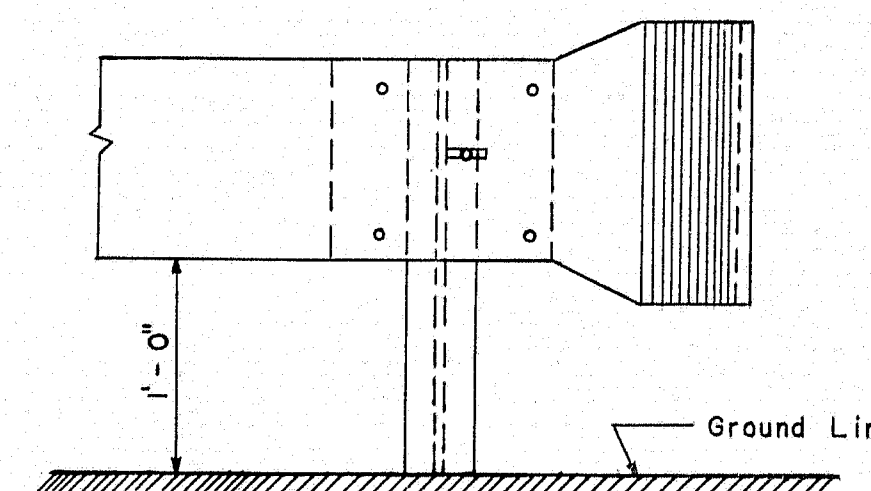
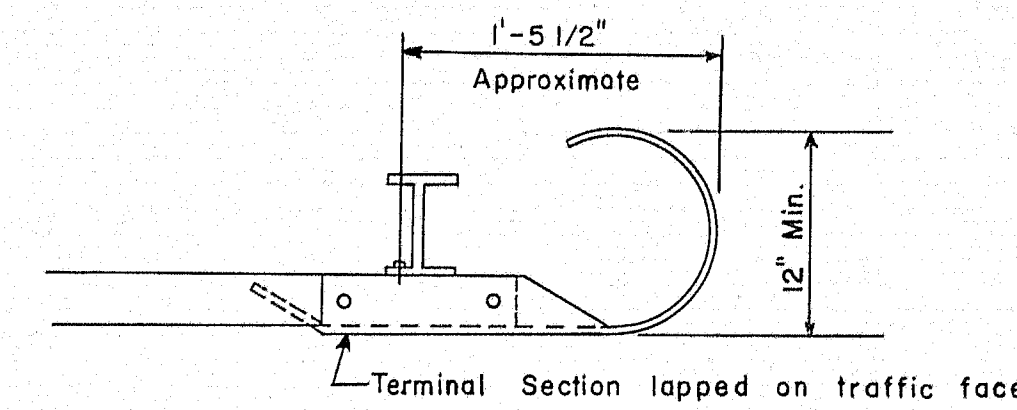


Scale: $\frac{3}{4}" = 1'$



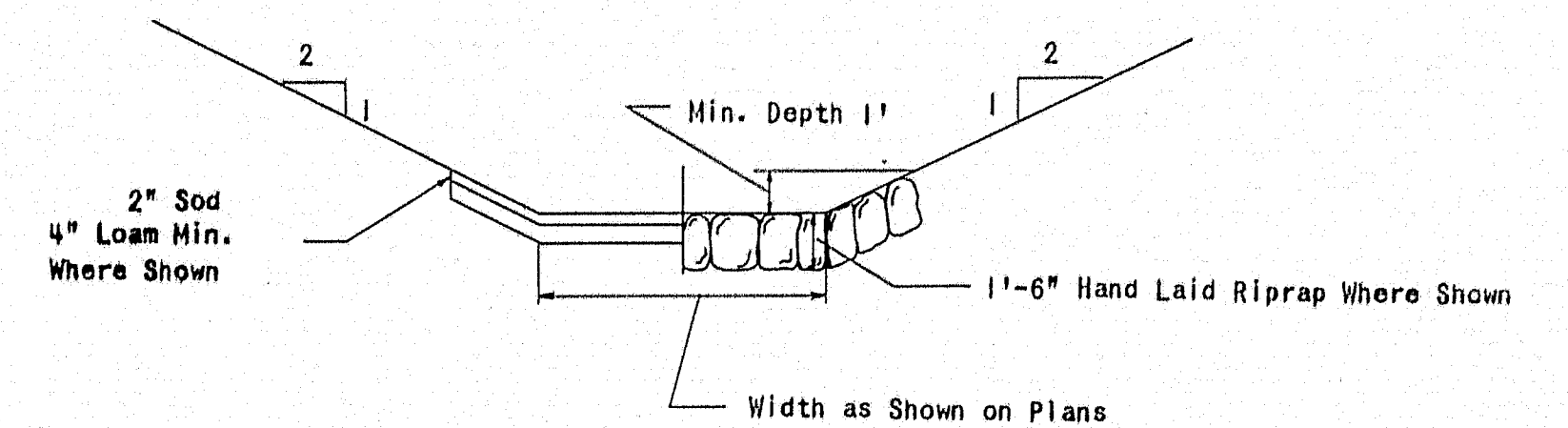
DETOUR SIGN FOR
PORTABLE BARRICADE

Scale: $\frac{1}{2}" = 1'$



GUARD RAIL TYPE "E"
TERMINAL SECTION

Scale: 1" = 1'-0"



TYPICAL DITCH
Not to Scale

GENERAL NOTES:

- Lumber sizes for use on Barricades shall be 2"x8" except for Posts which shall be 4"x4".
- Detour Sign shall be 5/8" thick plywood.
- Alternate 6" stripes shall be painted or screened in black on a background of silver reflective sheeting.
- The word "DETOUR" and Arrow shall be painted or screened in black on a background of yellow reflective sheeting.
- Flashing Lights shall be Mounted to permit rotation to face oncoming traffic.
- Reflector clusters shall consist of a red background with red Reflectors similar to AGA Designation #1816-A1 (PDON).
- The Barricades shall be securely anchored in place by means of sandbags, weights, or large stones.
- Location of Service and Meter to be determined after Power Source has been decided.

MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

SPECIAL DETAILS

PORTABLE BARRICADES

SCALE: AS NOTED

THE CLARKESON ENGINEERING CO., INC.
CONSULTING ENGINEERS
BOSTON MASSACHUSETTS

UTILITY SYMBOLS

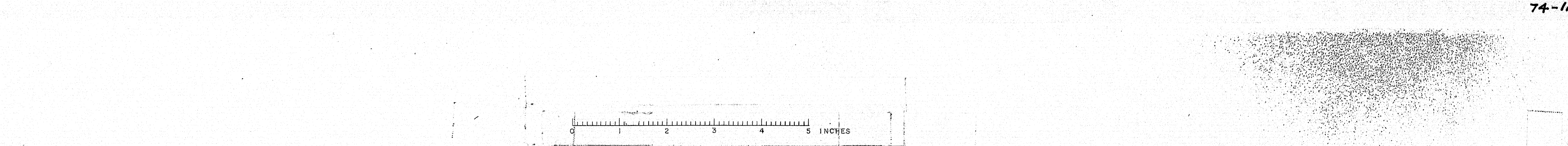
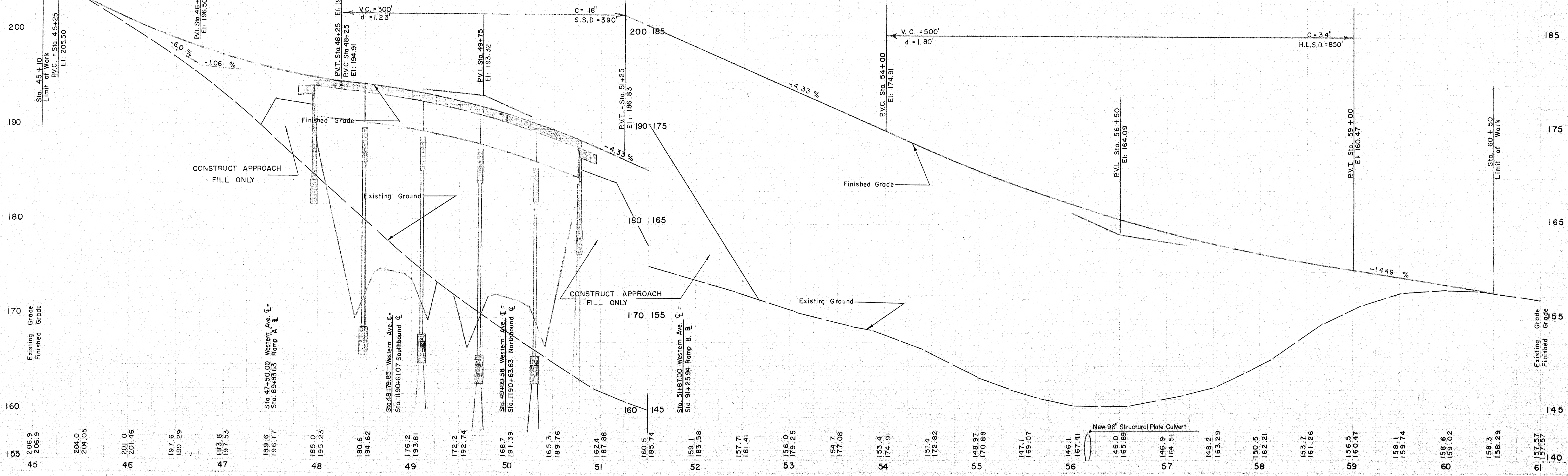
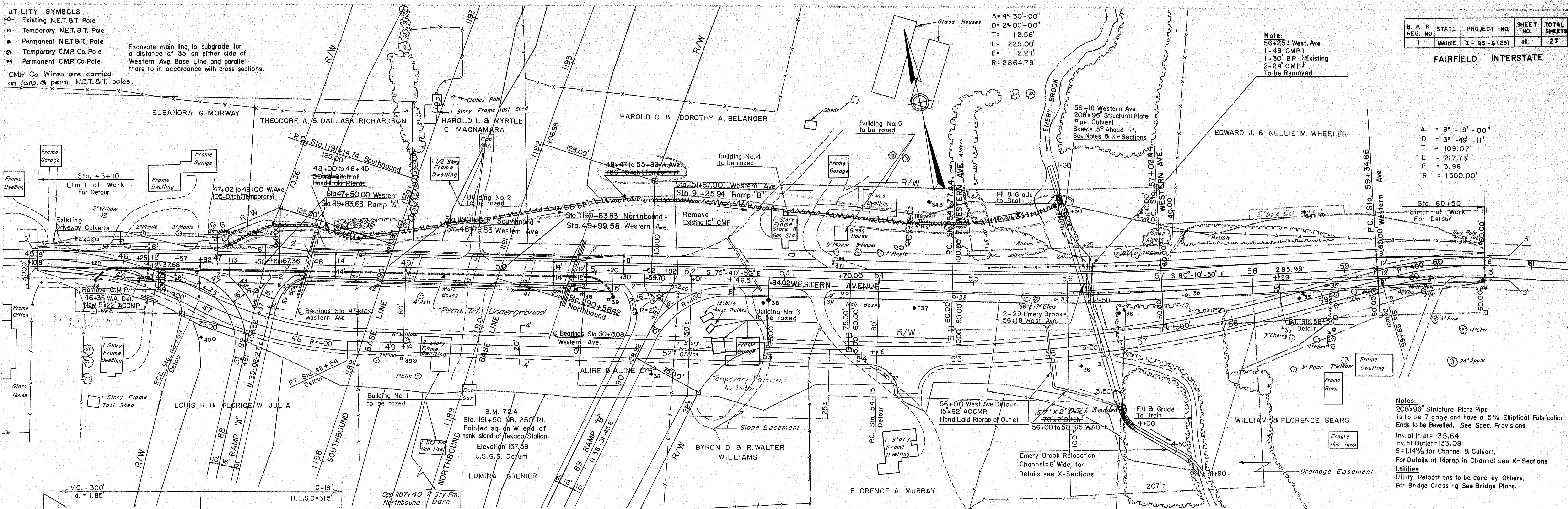
- Existing N.E.T. & T. Pole
- Temporary N.E.T. & T. Pole
- Permanent N.E.T. & T. Pole
- Temporary C.M.P. Co. Pole
- Permanent C.M.P. Co. Pole

Excavate main line, to subgrade for a distance of 35' on either side of Western Ave. Base Line and parallel there to in accordance with cross sections.

C.M.P. Co. Wires are carried on temp. & perm. N.E.T. & T. poles.

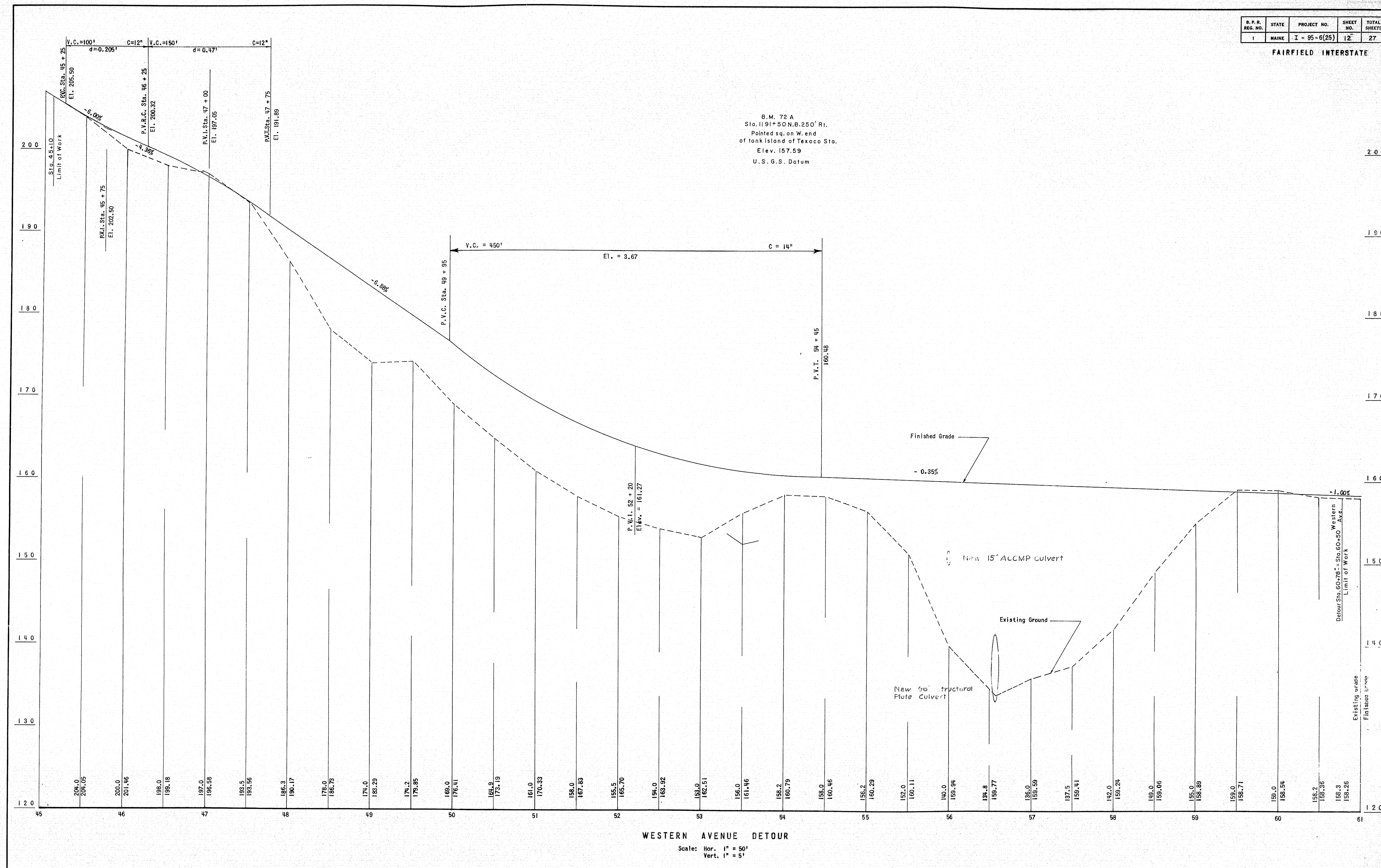
B. P. R.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
I	MAINE	1-95-6 (25)	II	27

FAIRFIELD INTERSTATE

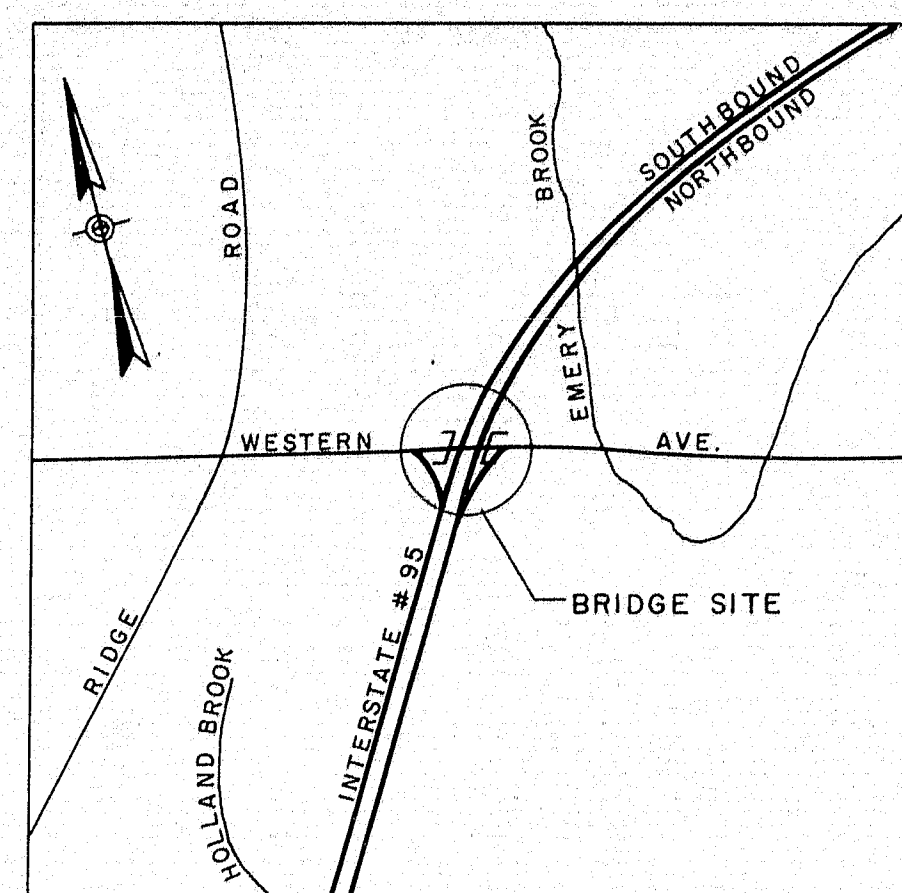


D. P. R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	I - 95-6(25)	12	27

FAIRFIELD INTERSTATE

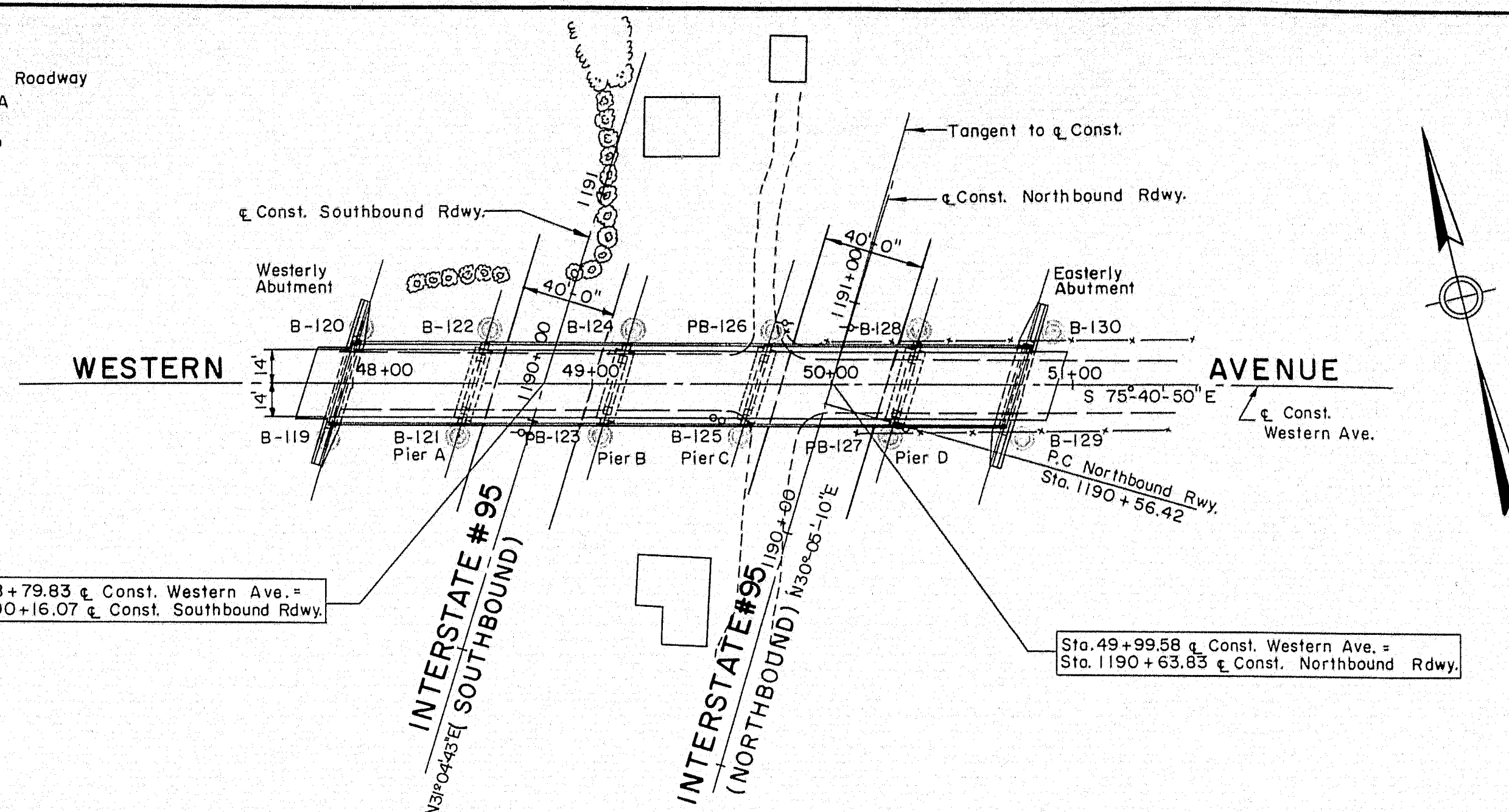


0 1 2 3 4 5 INCHES



LOCATION MAP
Not to Scale

Const. Northbound Roadway
CURVE DATA
 $\Delta = 60^{\circ}59'03''$
 $D = 1^{\circ}00'00''$
 $L = 6098.417'$
 $T = 3373.913'$
 $R = 5729.578'$



KEY PLAN
Scale: 1" = 50'-0"

B.P.R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	J-95-6 (25)	13	27

FAIRFIELD INTERSTATE

GENERAL NOTES

FOUNDATION:
Foundations may be altered, if necessary, to suit conditions encountered in construction.

DESIGN:
In accordance with the specifications of the American Association of State Highway Officials for H-20-S16-44 loading (1957 Edition) modified for military requirements.

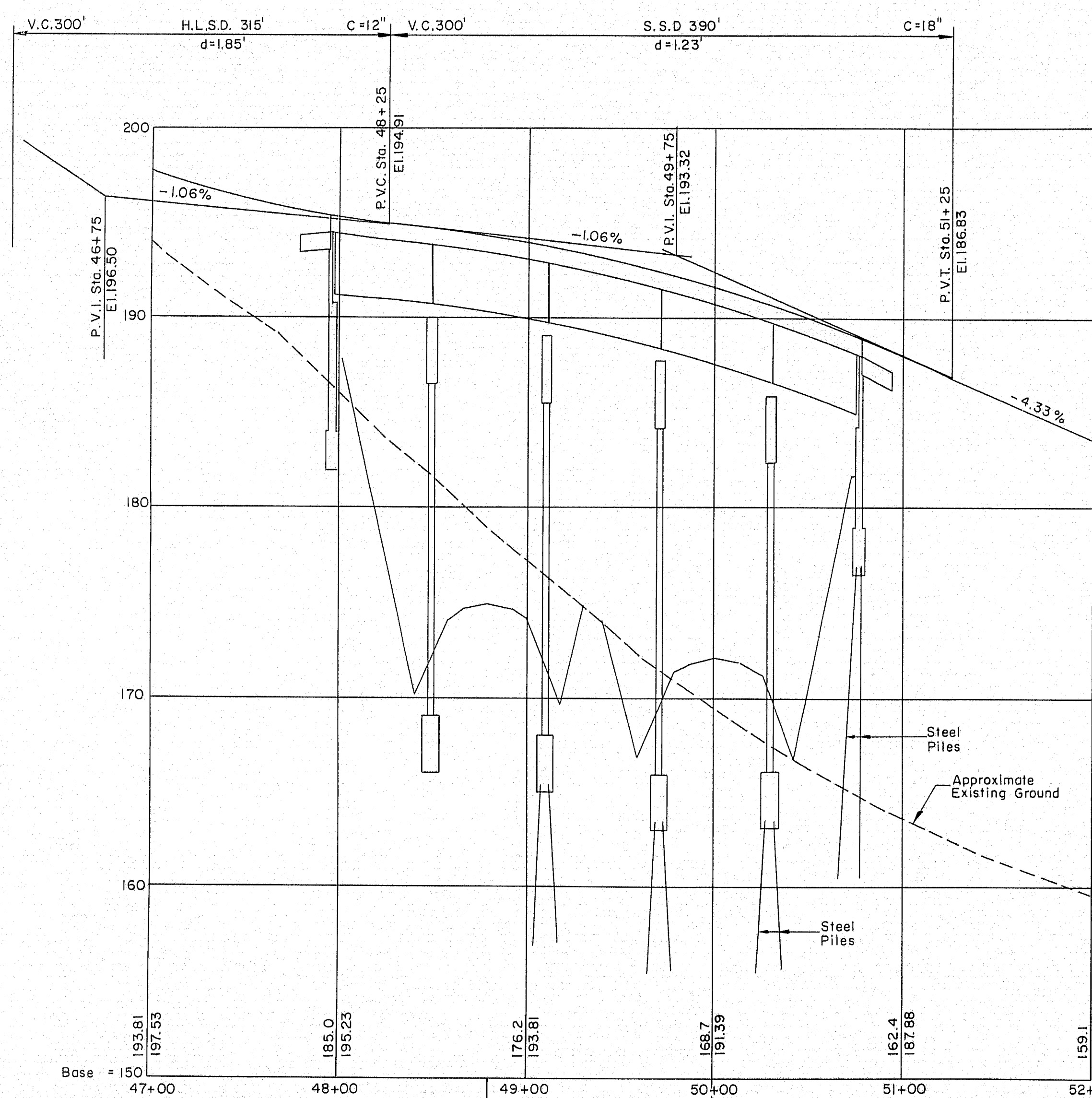
Design Stresses: Structural Steel $f_s = 18,000$ psi
Reinforcing Steel $f_s = 18,000$ psi
Concrete ($n = 10$) $f_c = 1,200$ psi

CONSTRUCTION:
State of Maine Standard Specifications to be followed except as noted in Special Provisions.

REINFORCEMENT:
All bars shall have deformations conforming to A.S.T.M. Designation A305. Unless otherwise shown on plans, reinforcing bars shall be lapped 20 diameters to make a splice, except that main reinforcing bars near the top of slabs and beams having more than 12' of concrete under the bars shall be lapped 35 diameters to make a splice.

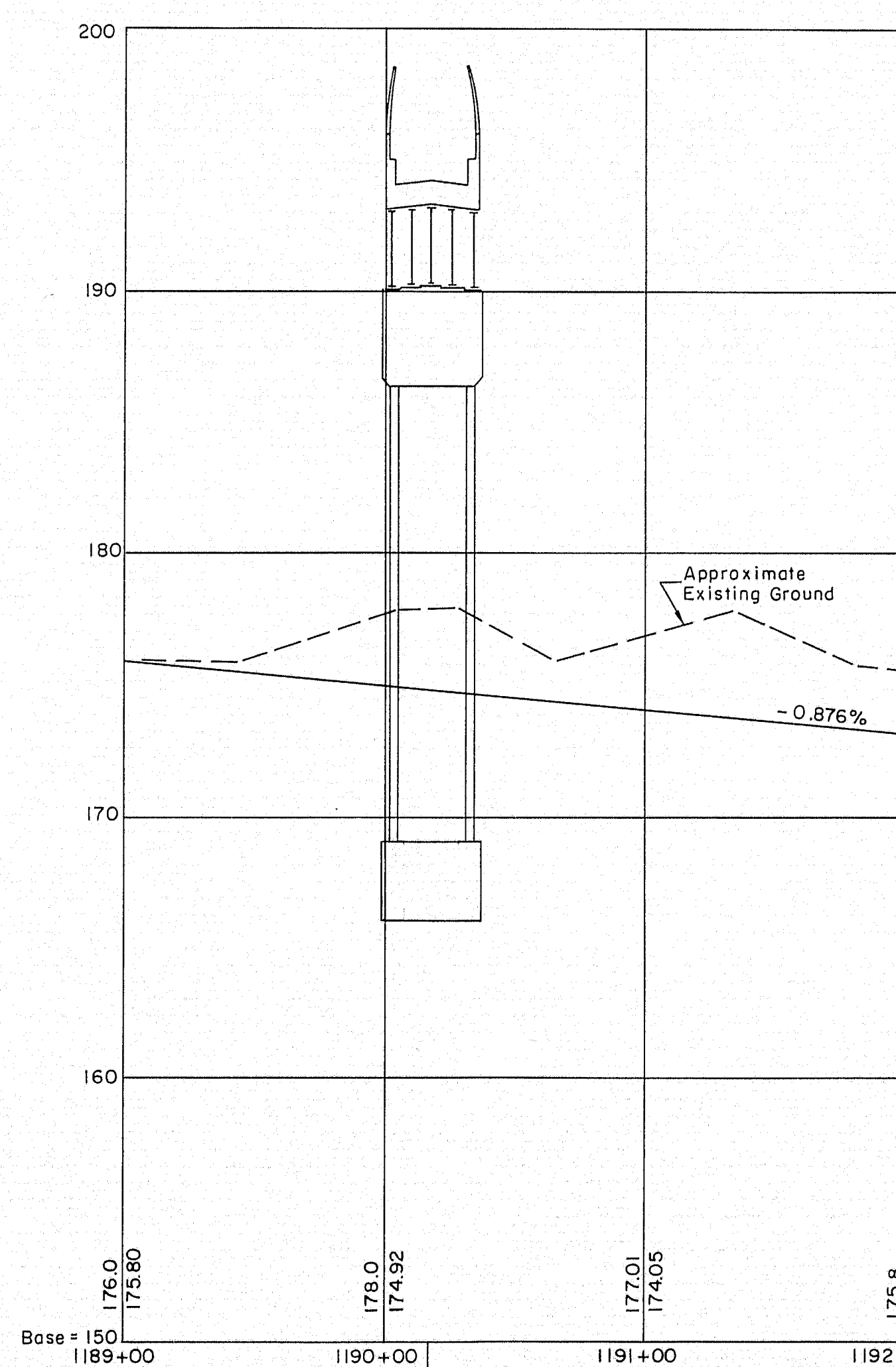
STRUCTURAL STEEL:
Wherever cover plates and/or shear connectors are welded to beams, beams and plates shall be weldable structural steel A.S.T.M. Designation A373.

BENCH MARK:
BM-72A Painted square on West end of tank island of Texaco Station, Sta. 1191+50 (N.B.) 250' Rt. Elevation 157.59 U.S.G.S. Datum.



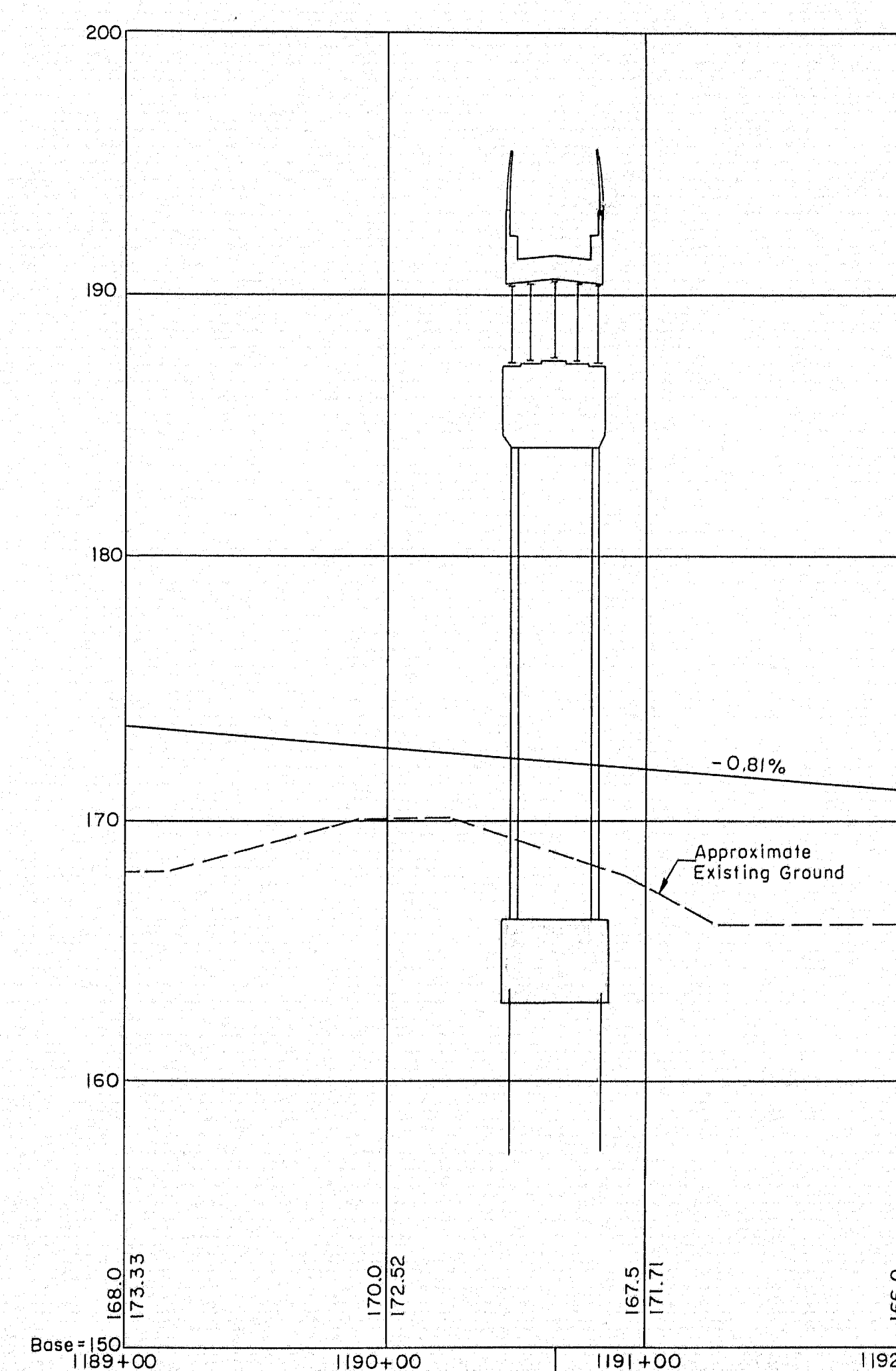
PROFILE ALONG WESTERN AVENUE

Scale: Hor. 1" = 50'-0"
Vert. 1" = 5'-0"



PROFILE ALONG INTERSTATE #95 (SOUTHBOUND)

Scale: Hor. 1" = 50'-0"
Vert. 1" = 5'-0"



PROFILE ALONG INTERSTATE #95 (NORTHBOUND)

Scale: Hor. 1" = 50'-0"
Vert. 1" = 5'-0"

NOTE: Profile Grade at center of 24' roadway up to Station 1188+00 Northbound and 1189+00 Southbound. Beyond these stations profile grade is at inside edge.

FINAL ESTIMATED QUANTITIES (NOT GUARANTEED)

STRUCTURAL EARTH EXCAVATION, ABUTMENTS AND RETAINING WALLS	420	CU. YDS.	72.8
STRUCTURAL EARTH EXCAVATION, PIERS	265	CU. YDS.	24.1
GRAVEL BASE COURSE - IN PLACE MEASUREMENT	230	CU. YDS.	2.9
PORTLAND CEMENT CONCRETE, ABUTMENTS AND RETAINING WALLS	450	CU. YDS.	142.8
PORTLAND CEMENT CONCRETE, PIERS	230	CU. YDS.	22.5
PORTLAND CEMENT CONCRETE, ROADWAY & SIDEWALK SLABS	310	CU. YDS.	34.4
ON STEEL BRIDGES	420	CU. YDS.	105.4
PORTLAND CEMENT	420	BBLs	105.4
BRIDGE DRAINAGE	1	LUMP SUM	
STRUCTURAL STEEL, FABRICATED & DELIVERED	276,500	LBS.	276,500
STRUCTURAL STEEL, ERECTION	276,500	LBS.	276,500
REINFORCING STEEL, DELIVERED	123,500	LBS.	123,500
REINFORCING STEEL, PLACING	123,500	LBS.	123,500
SHEAR CONNECTORS, DELIVERED & PLACED	1	LUMP SUM	
STEEL H-BEAM PILES 42 LBS / FOOT	1780	LIN. FT.	1780
FRENCH DRAINS	76	CU. YDS.	58.3
ALUMINUM RAILING	460	LIN. FT.	560.2
SLOPE PAVING FOR BRIDGES	460	SQ. YDS.	57.9

Note: Membrane Waterproofing, Bituminous Surfacing and Approach Slabs are not in this contract.

DESIGN	CHECK G.B. & H.P.	BRIDGE NO.
DRAWN V.S.	APPROVED W.A.H.-C.J.M.	SURVEY PLOT
STATE HIGHWAY COMMISSION		
INTERSTATE #95		
UNDER		
WESTERN AVENUE		
IN THE CITY OF		
FAIRFIELD		
SOMERSET COUNTY		
KEY PLAN & PROFILES		

APPROVED BY *John A. Henderson* DATE 11/19/1958
THE CLARKSON ENGINEERING CO., INC.
CONSULTING ENGINEERS
BOSTON MASSACHUSETTS

SHEET 1 OF 8 SHEETS AUGUSTA, MAINE

FAIRFIELD INTERSTATE

PB - 126

A refusal was obtained 26.2 feet below the ground surface at this location.

Elevation	Description of Materials	Casing Blows Per Foot	Spoon Blows Per Foot	Percent Recovery	Vane Shear Strength (Tons Per Sq. Ft.)
170.5	Brown silty gravel	10	10	100	
166.5	Brown silty clay	7	11	25	0.80
154.5	Gray silty clay & pebbles	19	2	2	2.35
153.5	Gray silty sand & pebbles	15	15	15	
142.75	Rock	20			

* Penetration 9"

Bottom of Proposed Pier C Footing

B - 124

Elevation	Description of Materials	Casing Blows Per Foot	Spoon Blows Per Foot	Percent Recovery	Vane Shear Strength (Tons Per Sq. Ft.)
172.7	Brown sandy silty clay	21	14	14	
153.7	Gray silty sand & stones	17			
149.5	Rock	33			

* Penetration 2"

Bottom of Proposed Pier B Footing

PB - 123

A refusal surface was noted 25.5 feet below the ground surface at this location.

Elevation	Description of Materials	Casing Blows Per Foot	Spoon Blows Per Foot	Percent Recovery
179.0	Brown sand & gravel	10		
174.0	Brown silty sand & gravel	29		
166.0	Gray silty sand & gravel	13		
152.93	Rock	20		

Weight of hammer 305#
Average fall on casing 16"
Average fall on rods 14"

B - 121

Elevation	Description of Materials	Casing Blows Per Foot	Spoon Blows Per Foot	Percent Recovery
179.8	Brown silty sand & stones	15		
168.8	Gray silty sand & stones	61		
155.05	Rock	33		

* Penetration 9"

Bottom of Proposed Pier A Footing

B - 122

Elevation	Description of Materials	Casing Blows Per Foot	Spoon Blows Per Foot	Percent Recovery
179.0	Brown sand & gravel	10		
174.0	Brown silty sand & gravel	29		
166.0	Gray silty sand & gravel	13		
152.93	Rock	20		

Weight of hammer 305#
Average fall on casing 16"
Average fall on rods 14"

B - 120

Elevation	Description of Materials	Casing Blows Per Foot	Spoon Blows Per Foot	Percent Recovery
189.0	Brown silty sand & rocks	82		
177.0	Gray silty sand & rocks	37		
172.0	Rock	32		
167.0	Rock	50		

B - 119

Elevation	Description of Materials	Casing Blows Per Foot	Spoon Blows Per Foot	Percent Recovery
185.9	Brown silty sand & rocks	27		
172.9	Gray silty sand & rocks	35		
162.9	Rock	51		
157.9	Rock	63		

Bottom of Proposed Westerly Abutment Footing

B - 130

Elevation	Description of Materials	Casing Blows Per Foot	Spoon Blows Per Foot	Percent Recovery	Vane Shear Strength (Tons Per Sq. Ft.)
169.3	Brown silty sand	10			
160.3	Brown silty clay (Rocks)	120			
156.3	Gray silty clay	.41			
142.3	Gray sandy silt	.36			
139.3	Gray silty sand & rocks	.75			

El. 176.50

B - 129

Elevation	Description of Materials	Casing Blows Per Foot	Spoon Blows Per Foot	Percent Recovery	Vane Shear Strength (Tons Per Sq. Ft.)
164.5	Brown sandy silt	5			
150.5	Gray silty clay	.38			
140.5	Gray silty sand & rocks	.31			
139.5	Gray silty sand & rocks	10			
119.5	Gray silty sand & rocks	19			
	Gray silty sand & rocks	59			
	Rock	67			
	Rock	70			

Bottom of Proposed Easterly Abutment Footing

B - 128

Elevation	Description of Materials	Casing Blows Per Foot	Spoon Blows Per Foot	Percent Recovery	Vane Shear Strength (Tons Per Sq. Ft.)
166.0	Brown sand silt	6			
152.0	Brown silty clay	1.17			
147.0	Gray silty sand & rocks	.94			
128.5	Rock	22			
123.5	Rock	50			

* Penetration 6"

Bottom of Proposed Pier D Footing



PB - 127

The rod was driven 34 feet without keeping a record of the blow count. Between 34 and 35 feet, 695 blows; between 35-36 feet, 1850 blows.

Base = 160

BORING LOG

Scale: 1/8" = 1' - 0"

- Location of borings are shown on Key Plan thus  B - 119
Location of punchings are shown on Key Plan thus  PB - 126
- Borings are taken for purpose of design and show condition at boring points only, but do not necessarily show nature of materials to be encountered during construction.
- The Contractor is to form his own opinion of the character of the material, and to make his own interpretation of the borings.
- The Engineer does not warrant the findings as being accurate or complete.
- Figures in boring columns indicate blows per foot on 2 1/2" casing or 1" spoon produced by a 355# hammer with a fall of 14" and 12" respectively.
- Punchings made by a 1" rod and a 50# hammer with an 18" drop.
- Borings & Punchings were taken by the Maine State Highway Commission during the months of June, July and August 1958.

THE CLARKESON ENGINEERING CO., INC.

DESIGN	CHECK G. B. - H.P.	BRIDGE NO.
DRAWN J.M.T.	APPROVED W.A.H.-C.J.M.	SURVEY PLOT

STATE HIGHWAY COMMISSION

INTERSTATE #95

UNDER
WESTERN AVENUE

IN THE CITY OF
FAIRFIELD
SOMERSET COUNTY

BORING LOG

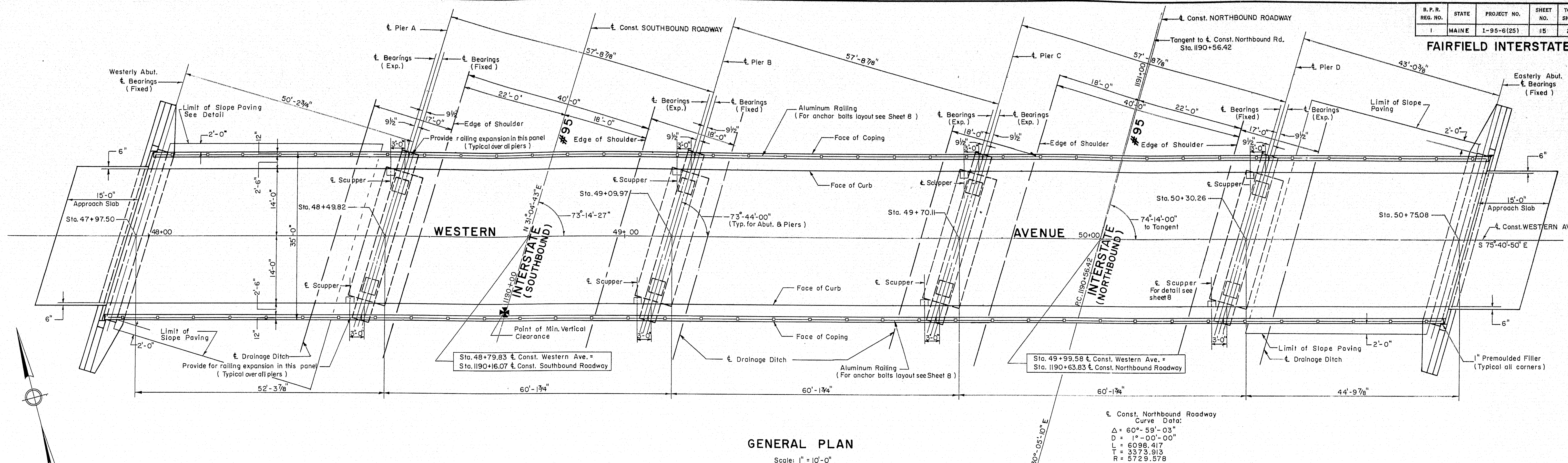
SHEET 2 OF 8 SHEETS

AUGUSTA, MAINE

74-14

B.P.R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-6(25)	15	27

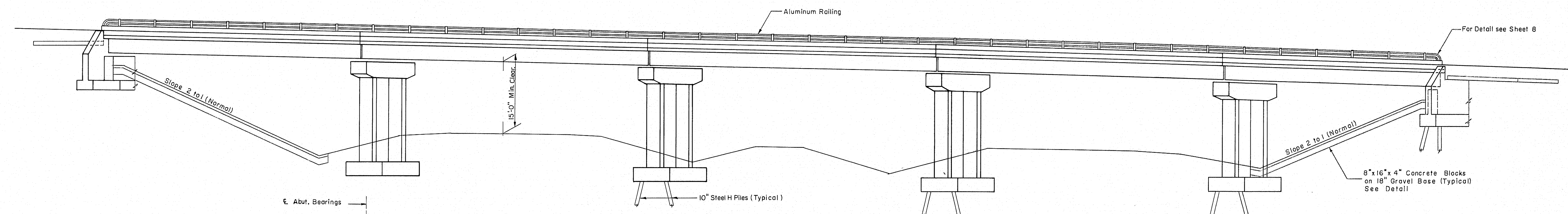
FAIRFIELD INTERSTATE



GENERAL PLAN

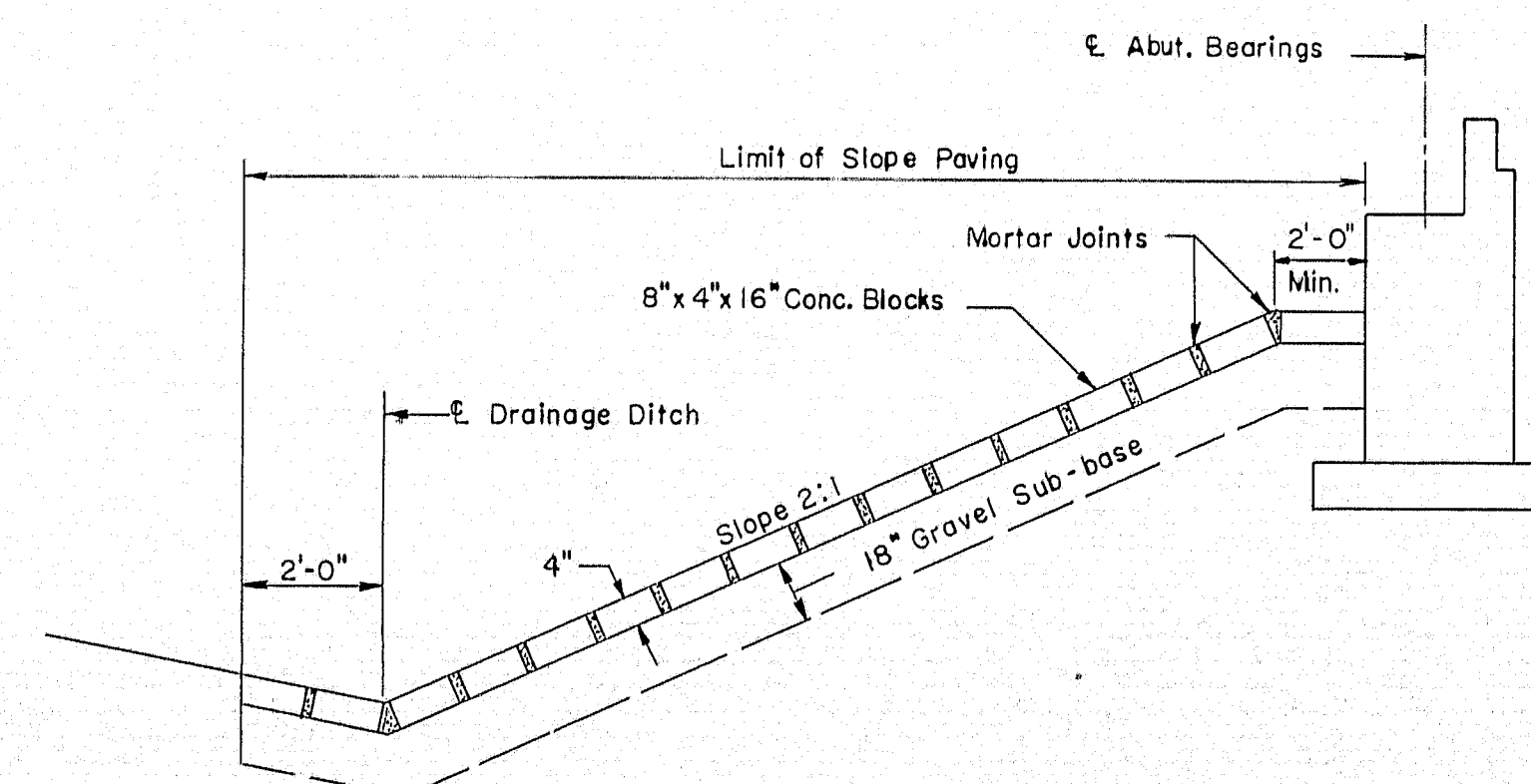
Scale: 1" = 10'-0"

Note: Approach Slabs are not in this contract.



GENERAL ELEVATION

Scale: 1" = 10'-0"



SLOPE PAVING DETAIL

Southerly Side

No Scale

Note: Slope Paving to stop at ledge if ledge is encountered during construction.

THE CLARKESON ENGINEERING CO., INC.

DESIGN G.B.-H.L.	CHECK G.B.-H.R.	BRIDGE NO.
DRAWN E.K.	APPROVED W.A.H.-C.J.M.	SURVEY PLOT

STATE HIGHWAY COMMISSION
INTERSTATE #95

UNDER
WESTERN AVENUE

IN THE CITY OF
FAIRFIELD
SOMERSET COUNTY

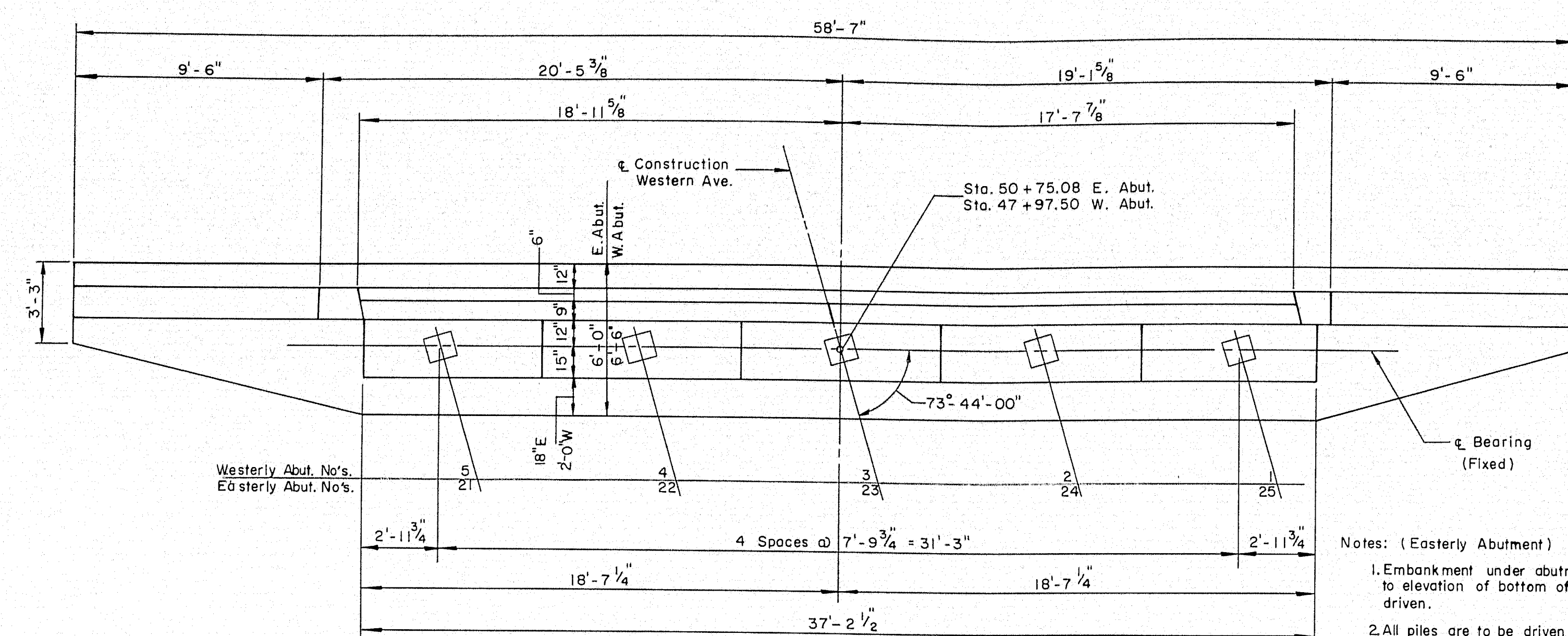
GENERAL PLAN & ELEVATION

SHEET 3 OF 8 SHEETS

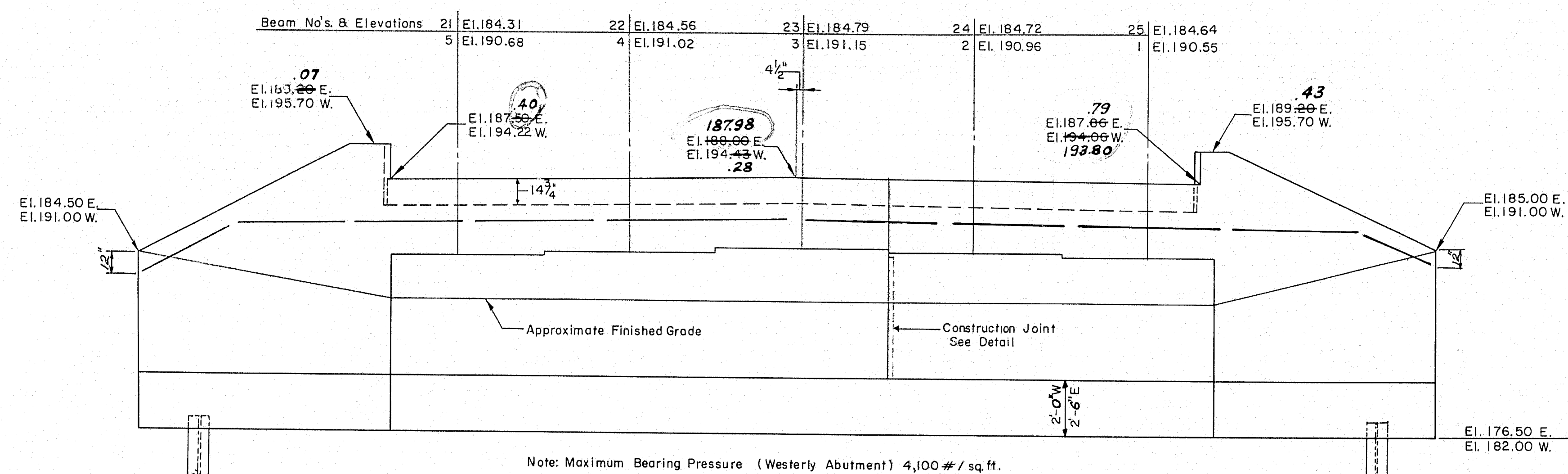
AUGUSTA, MAINE

74-15

B. P. R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
I	MAINE	I-95-6(25)	16	27



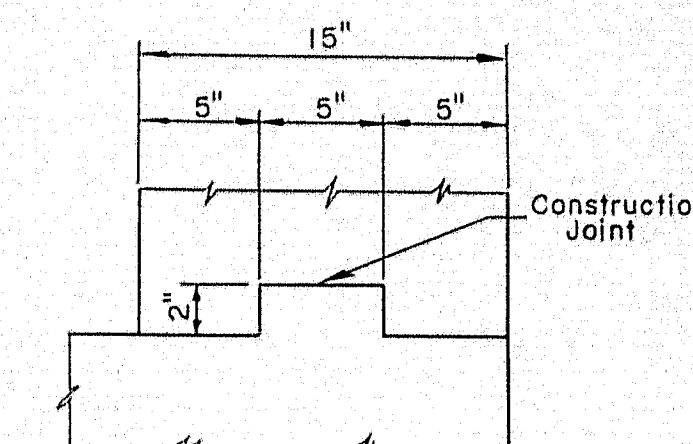
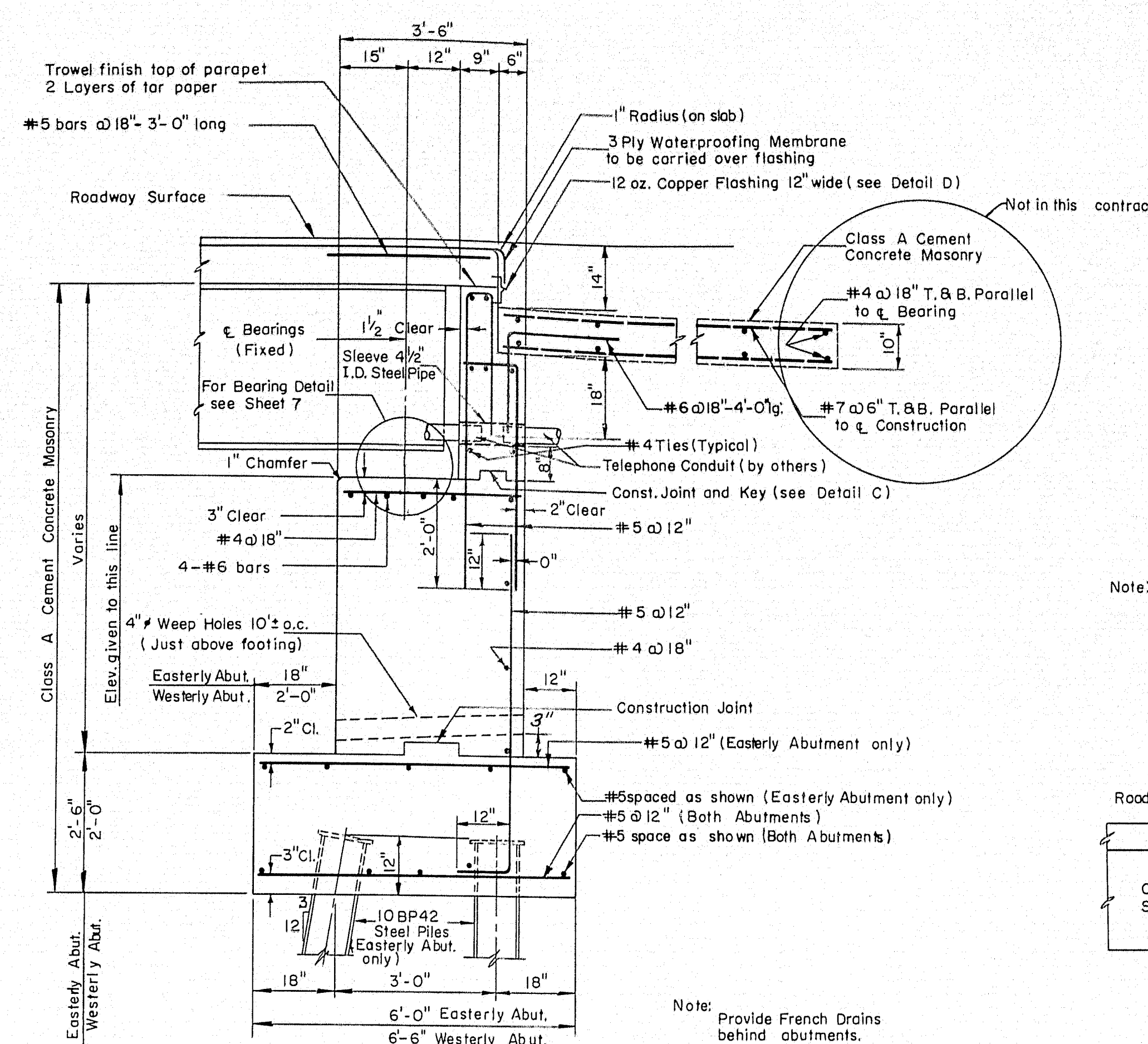
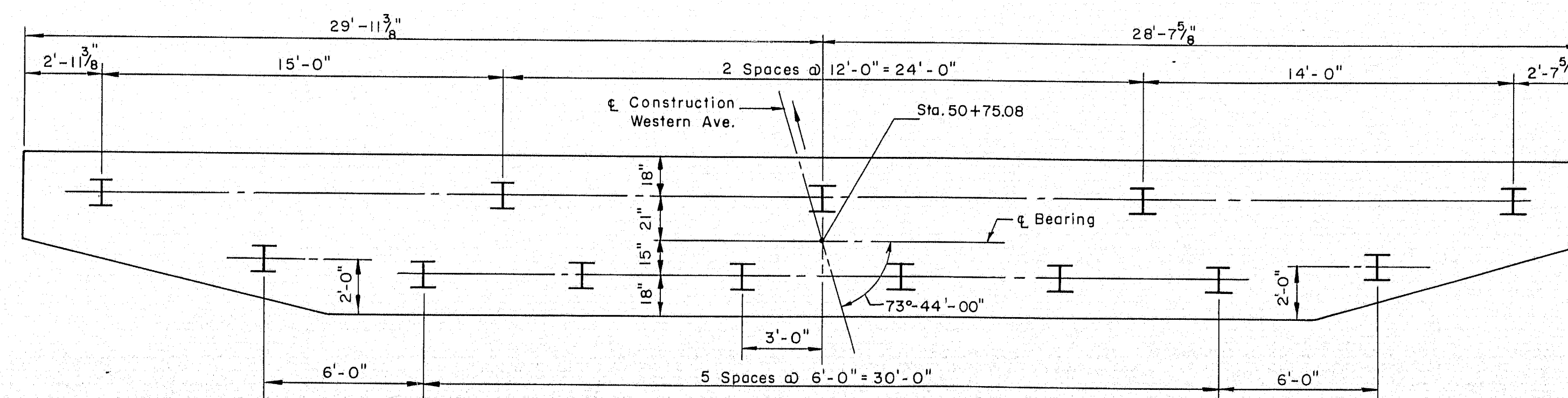
- Notes: (Easterly Abutment)
1. Embankment under abutment to be constructed to elevation of bottom of footing before piles are driven.
 2. All piles are to be driven to ledge or practical refusal
 3. All piles are to be 10 BP 42# steel piles.
 4. Maximum pile load = 30 tons.
 5. Front piles to be battered 3 on 12.
 6. Cap all piles (see detail sheet 5).



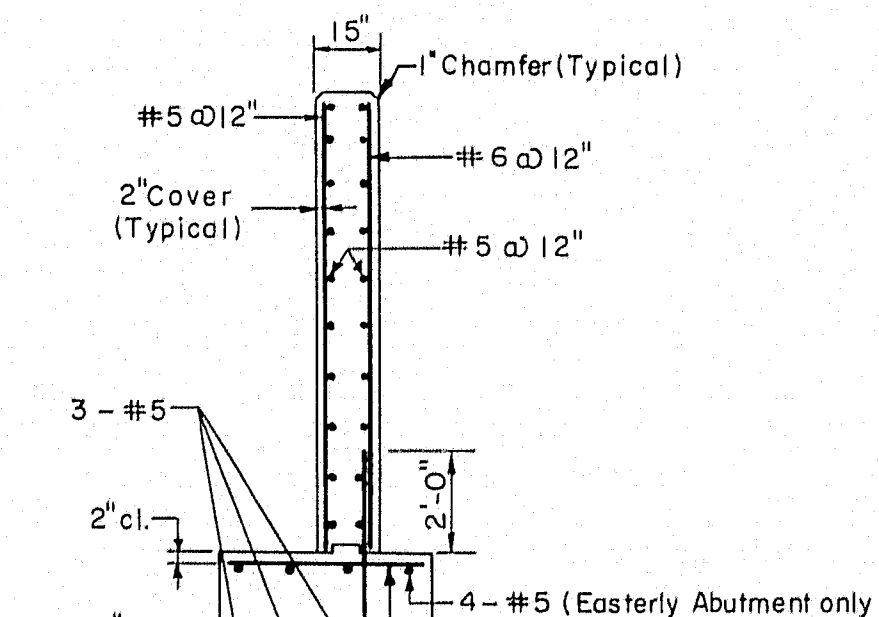
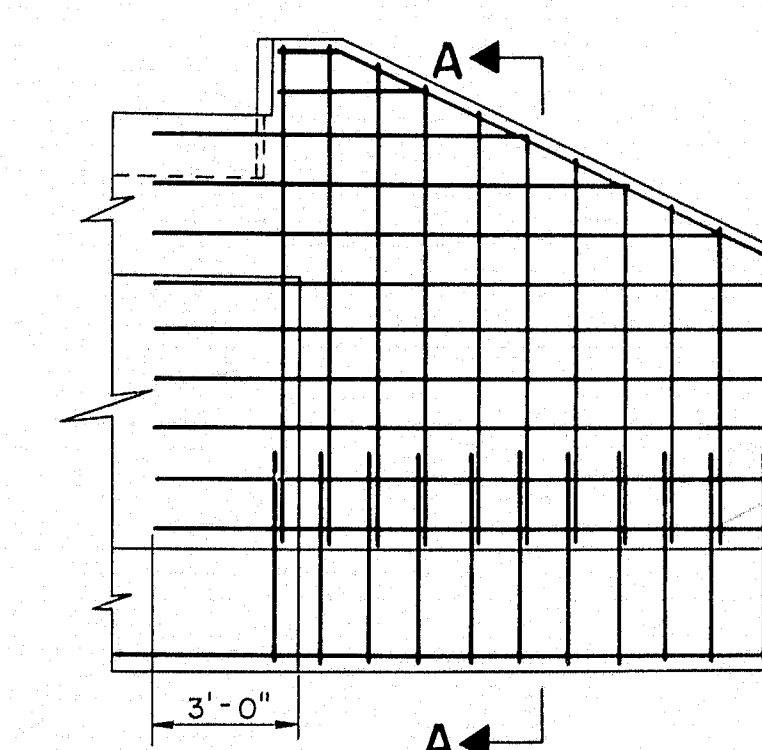
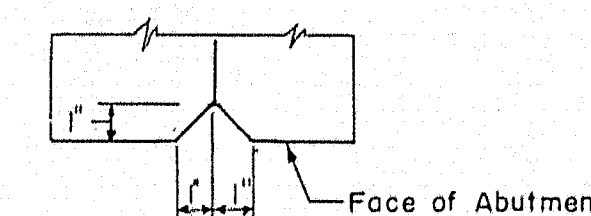
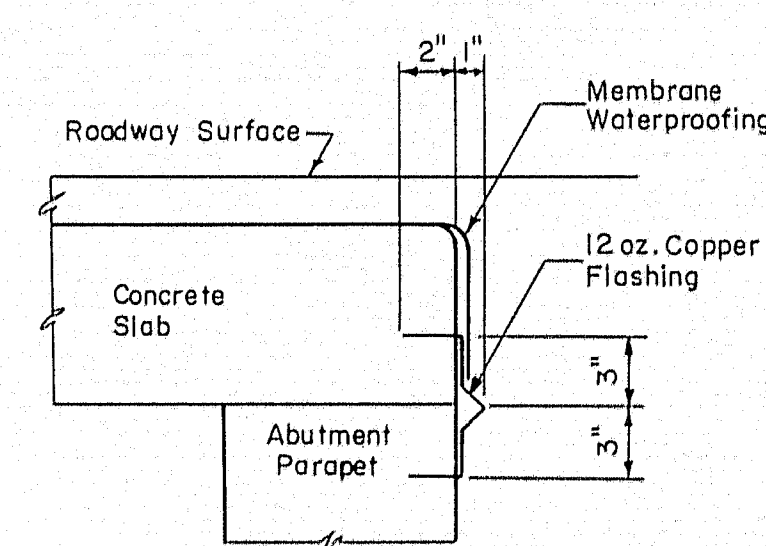
Note: Maximum Bearing Pressure (Westerly Abutment) 4,100 # / sq. ft

Note: Piles in easterly abutment only

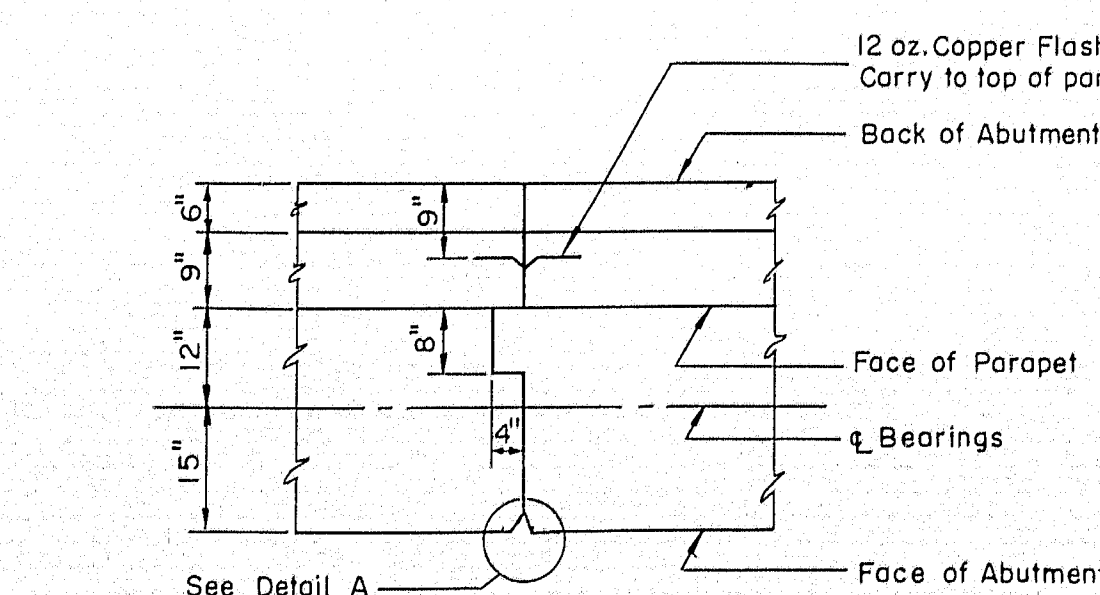
Note: Provide sleeves for Telephone Conduit thru abutment parapets (See Typical Abutment Section this Sheet and Typical Cross Section Sheet 6.)



Note: Concrete in parapet, not to be placed until Structural Steel has been erected.
Construction Joint to be at or above Bridge Seat



Note: Membrane Waterproofing, Bituminous Surfacing and Approach Slabs are not in this contract



THE CLARKESON ENGINEERING CO., INC.		
J.T. & J.M.	CHECK G.B. & H.P.	BRIDGE NO. SURVEY
DRAWN V.S.	APPROVED C.J.M. & W.A.H.	PLOT

STATE HIGHWAY COMMISSION

INTERSTATE #95

UNDER

WESTERN AVENUE

IN THE CITY OF

FAIRFIELD

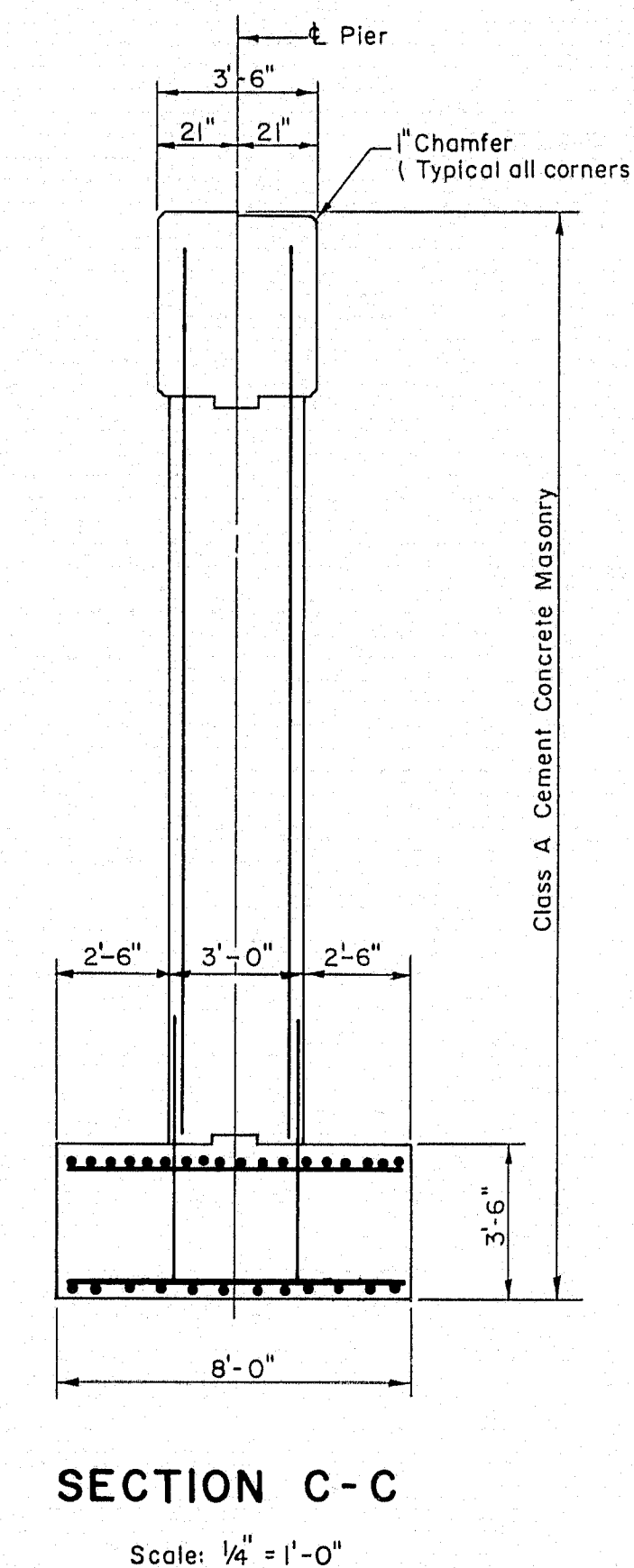
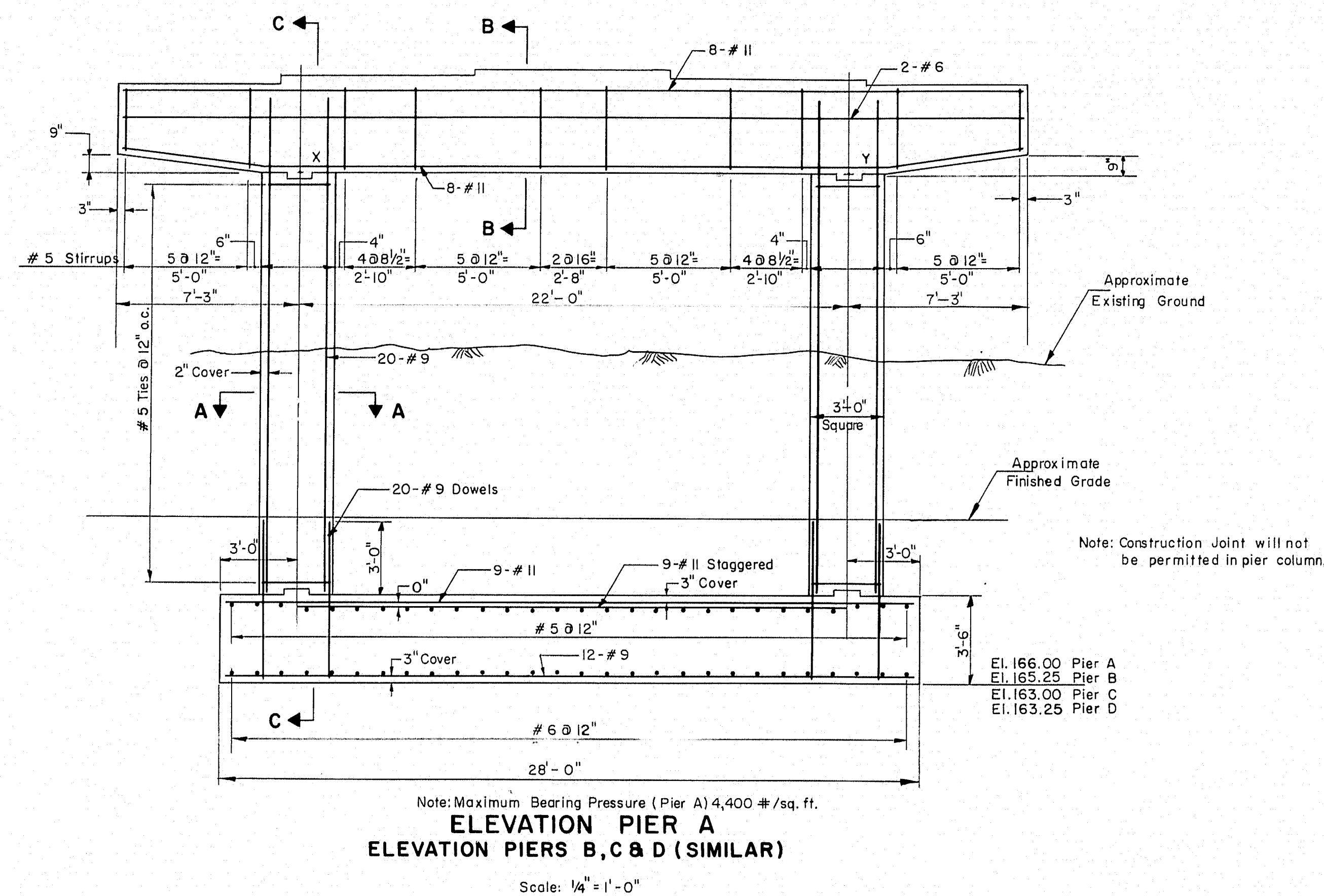
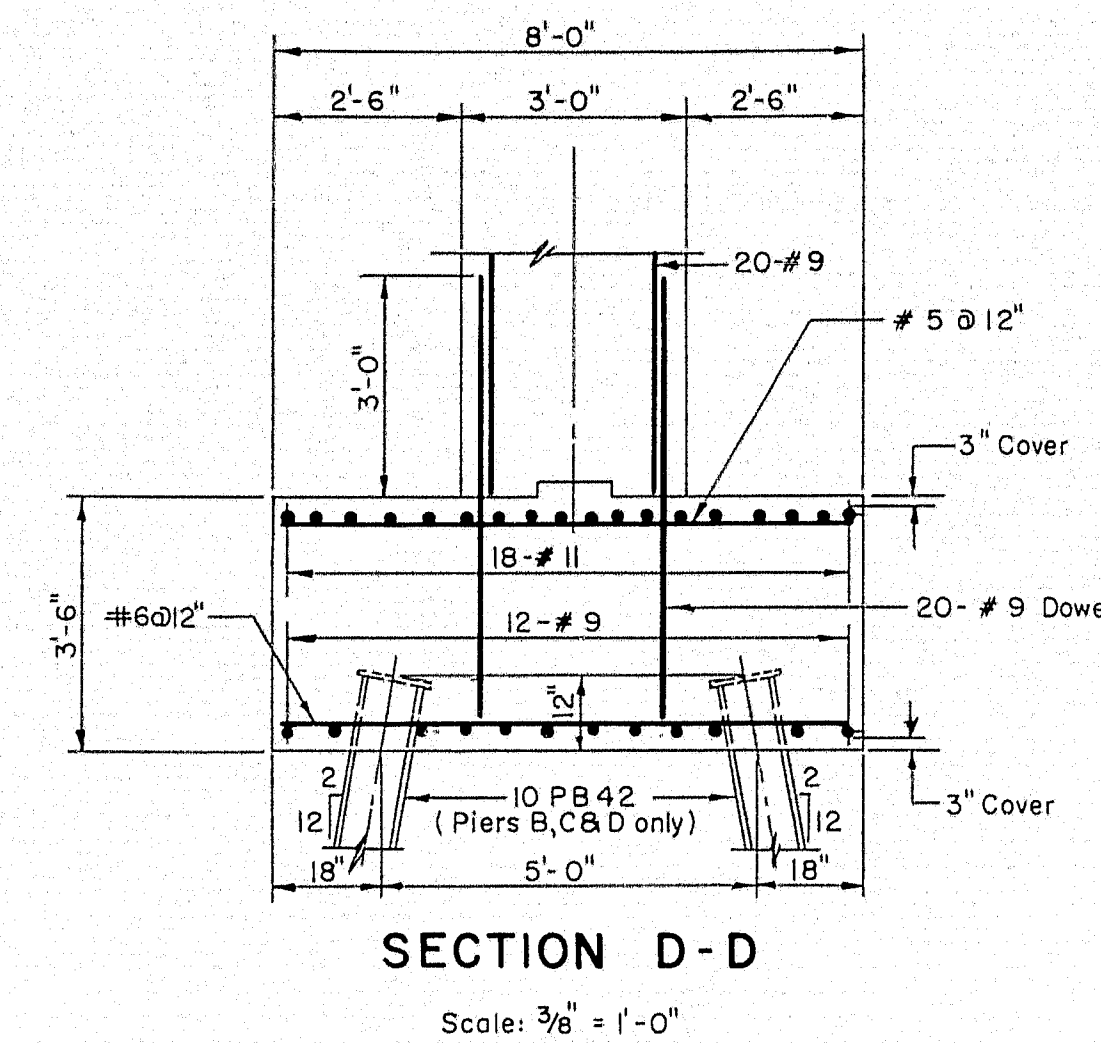
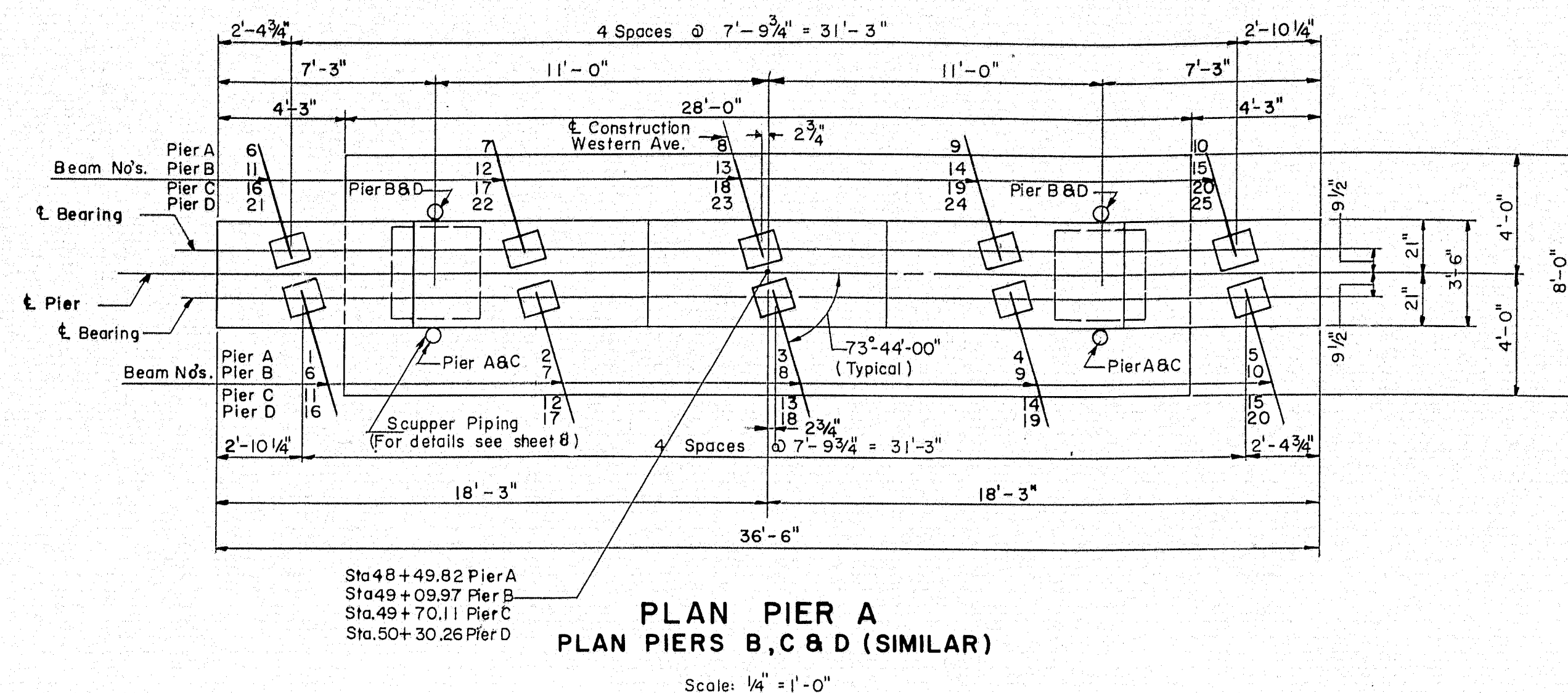
SOMERSET COUNTY

ABUTMENTS

SHEET 4 OF 8 SHEETS

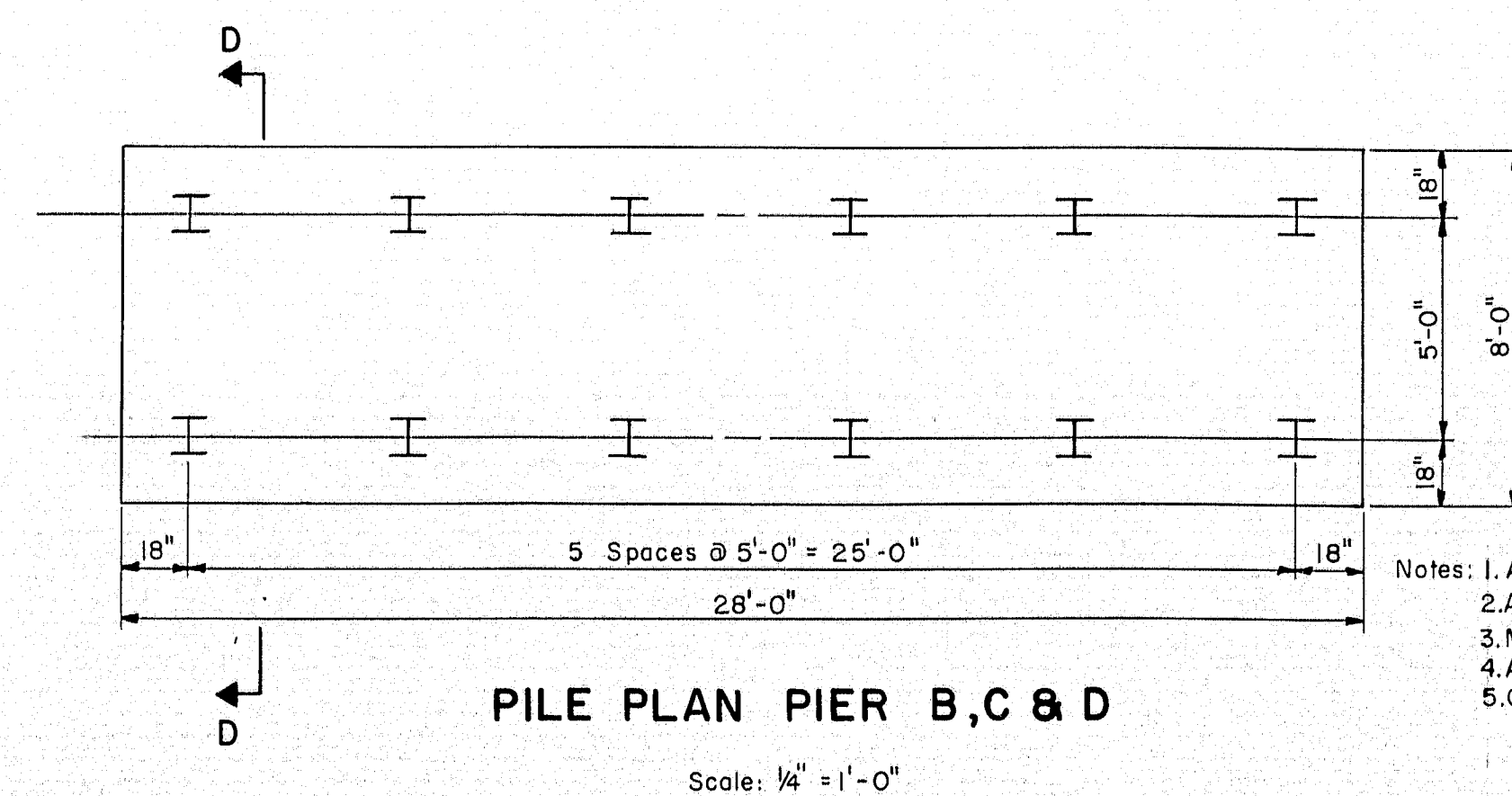
AUGUSTA, MAINE

74-16

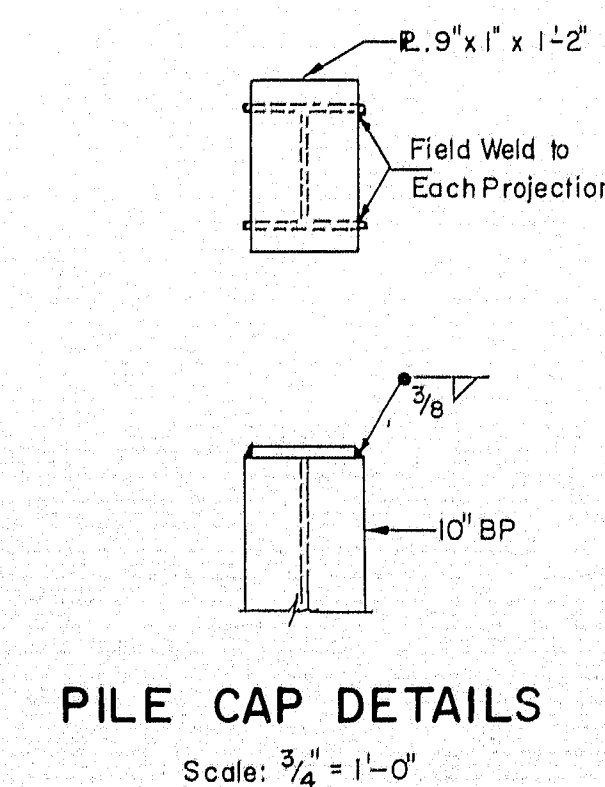
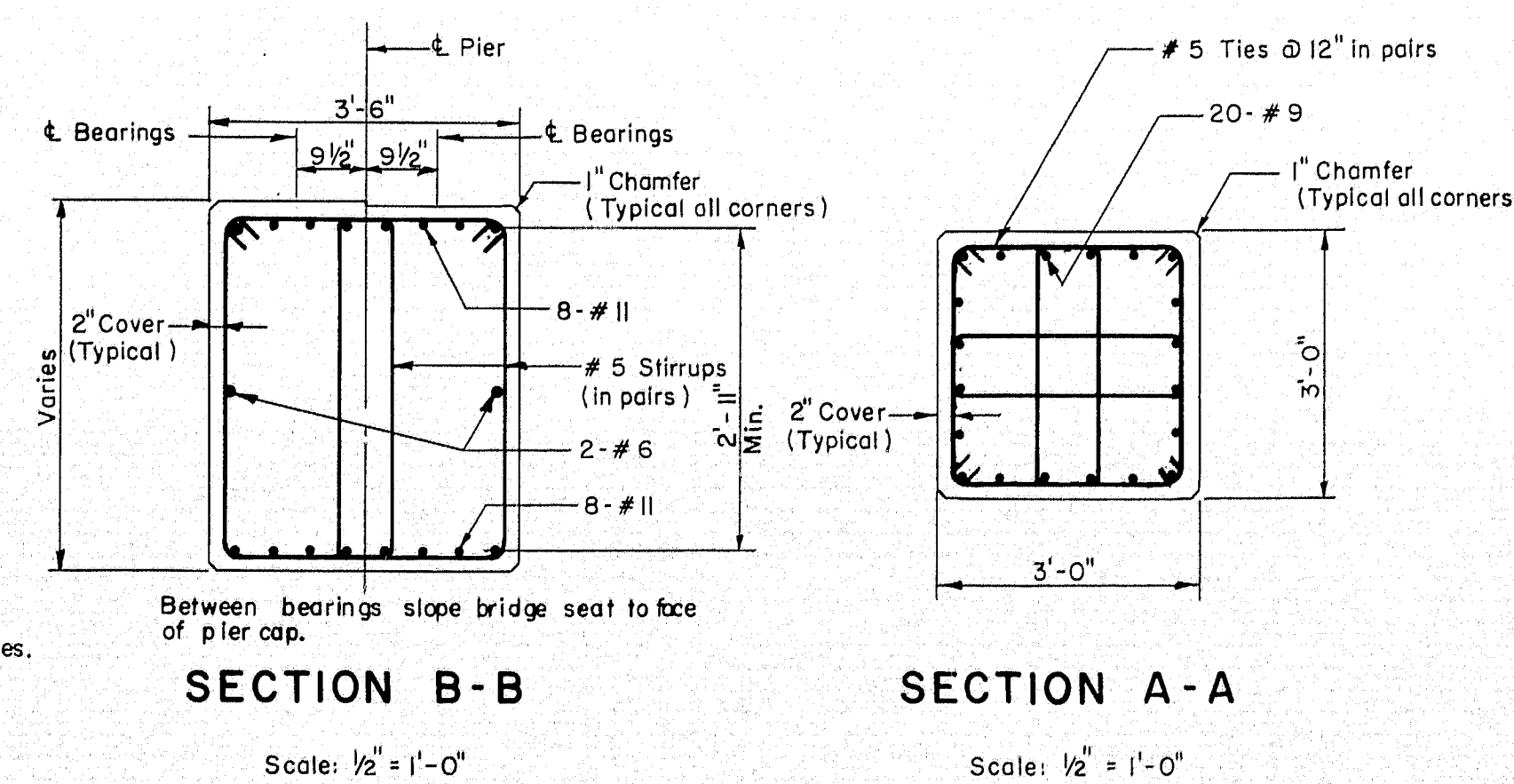


SCHEDULE OF BEAM SEAT ELEVATIONS									
Beam No.	Pier A	Beam No.	Pier B	Beam No.	Pier C	Beam No.	Pier D	Beam No.	
1	189.91	6	188.95	11	187.53	16	185.73	21	
2	190.32	7	189.15	12	187.74	17	185.95	22	
3	190.51	8	189.35	13	187.96	18	186.19	23	
4	190.38	9	189.24	14	187.86	19	186.10	24	
5	190.03	10	189.12	15	187.76	20	186.02		

ELEVATION AT TOP OF COLUMNS		
Pier	Elevation	
	X	Y
A	186.40	186.50
B	185.40	185.60
C	184.00	184.20
D	182.20	182.50



- Notes: 1. All piles are to be driven to ledge.
2. All piles are to be 10BP42 Steel Piles.
3. Maximum Pile load = 35 Tons.
4. All piles are to be battered 2 on 12.
5. Cap all piles (see detail).



THE CLARKSON ENGINEERING CO., INC.

DESIGN J.T. & J.M. CHECK G.B. & H.P. BRIDGE NO. SURVEY PLOT

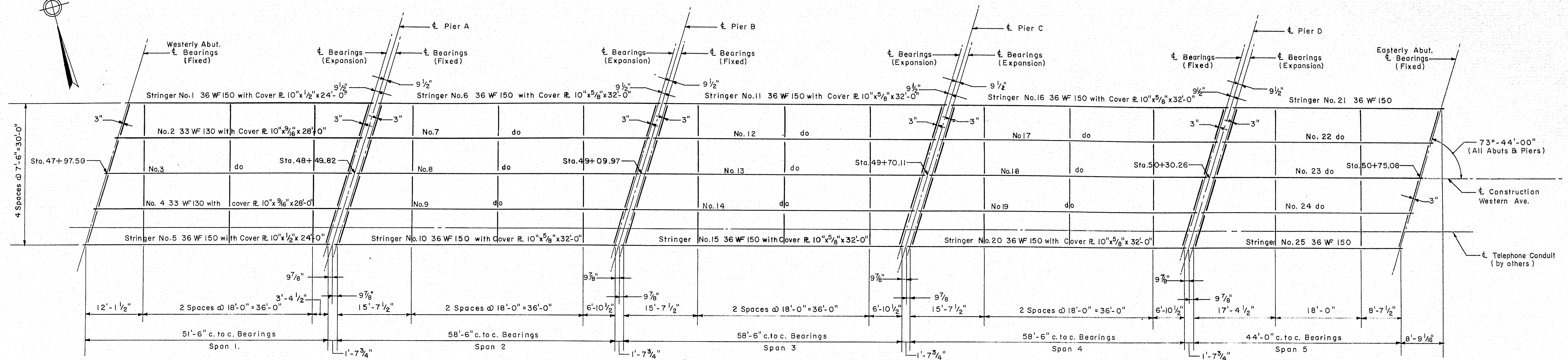
DRAWN E.K. APPROVED C.J.M. - W.A.H.

STATE HIGHWAY COMMISSION
INTERSTATE #95
UNDER
WESTERN AVENUE
IN THE CITY OF
FAIRFIELD
SOMERSET COUNTY
PIERS

SHEET 5 OF 8 SHEETS AUGUSTA, MAINE

B.P.R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-6(25)	18	27

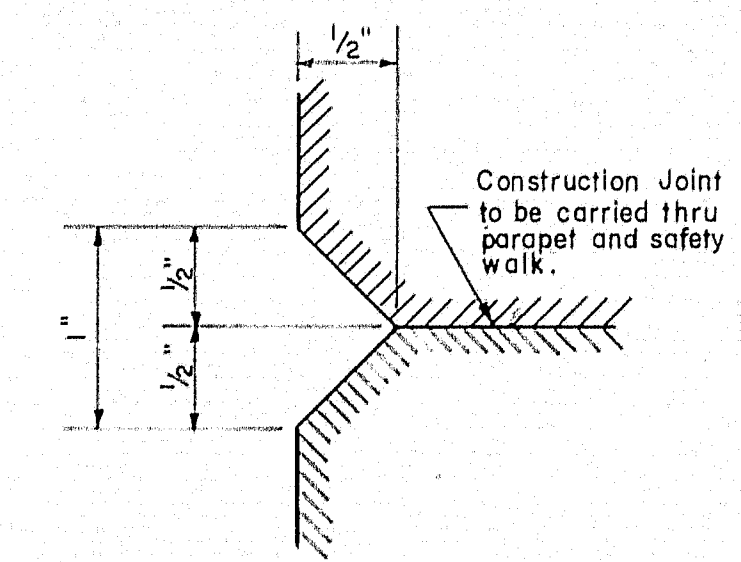
FAIRFIELD INTERSTATE



FRAMING PLAN

Scale: $\frac{3}{32}'' = 1'-0''$

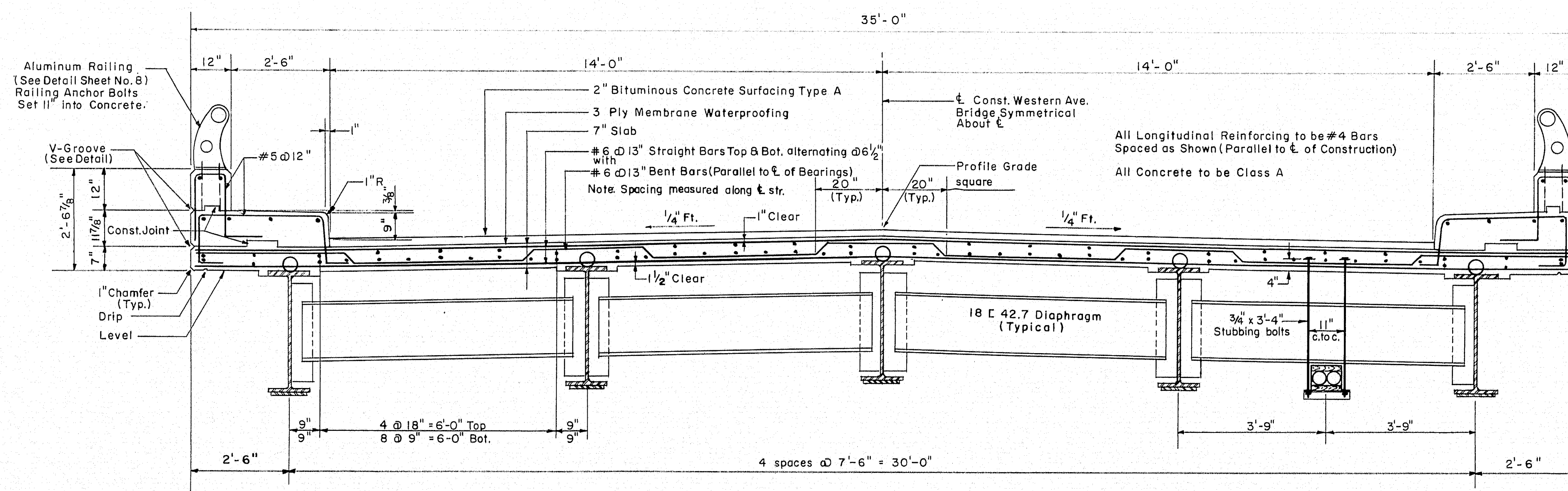
- Notes:
- All intermediate diaphragms to be 18 C 42.7 for detail see Sheet No. 7
 - All end diaphragms to be 15 C 33.9 for detail see Sheet No. 7
 - For cover plate and shear connector details see Sheet No. 7
 - All beams to be cambered to the extent that the bridge deck under full dead load will follow as nearly as practicable the design profile, and in no case with less camber than is likely to remain permanent. The proposed camber diagram shall be submitted to the Engineer for approval.
 - Parapet and safety walks to have transverse construction joints 20'± on centers. Joints are not to be located under rolling posts. Contact surfaces of joints to be painted with a bituminous material, exposed surfaces to be chamfered $\frac{1}{2}''$. Reinforcing steel to be carried thru joints.
 - All concrete for the deck slab in each span shall be placed in one continuous operation.
 - Top flange of stringers to be cut parallel to ℓ bearings.



V-GROOVE DETAIL

Not to Scale

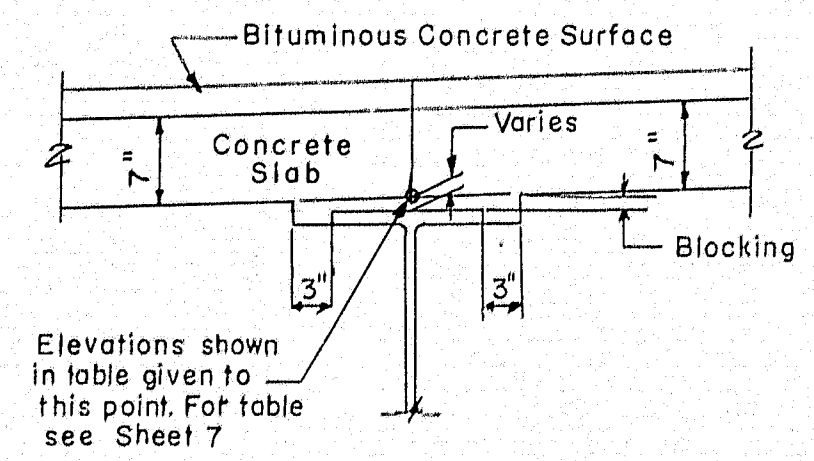
Note: Membrane Waterproofing and Bituminous Surfacing are not in this contract.



TYPICAL CROSS SECTION

Scale: $\frac{1}{2}'' = 1'-0''$

- Utility Notes:
- Hangers and conduit are to be furnished and installed by the New England Telephone and Telegraph Company.
 - No part of cable, conduit or support rocks shall hang below the bottom level of the stringers.

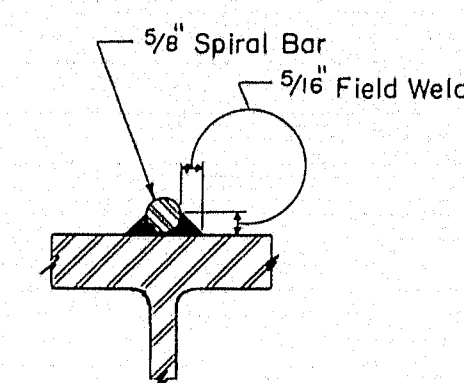
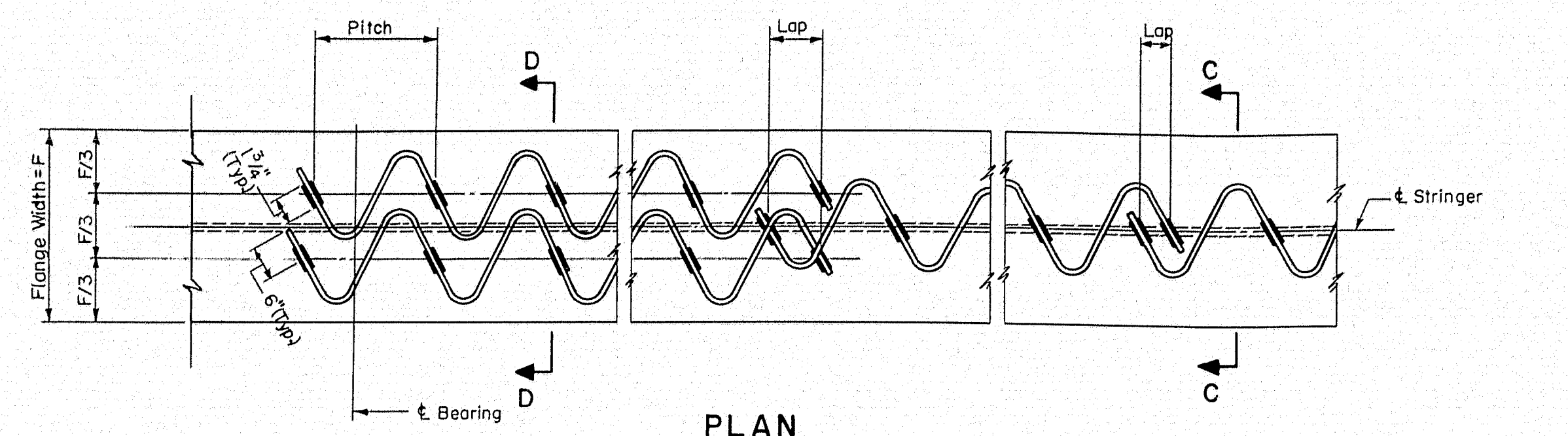


DETAIL OF HAUNCH

Scale: $\frac{3}{4}'' = 1'-0''$

THE CLARKESON ENGINEERING CO., INC.			
DESIGN J.M.-J.T.	CHECK G.B.-H.P.	BRIDGE NO.	
DRAWN J.M.T.	APPROVED W.A.H. SURVEY	PLOT	
STATE HIGHWAY COMMISSION			
INTERSTATE #95			
UNDER			
WESTERN AVENUE			
IN THE CITY OF			
FAIRFIELD			
SOMERSET COUNTY			
FRAMING PLAN & TYPICAL CROSS SECTION			
SHEET 6 OF 8 SHEETS			
AUGUSTA, MAINE			

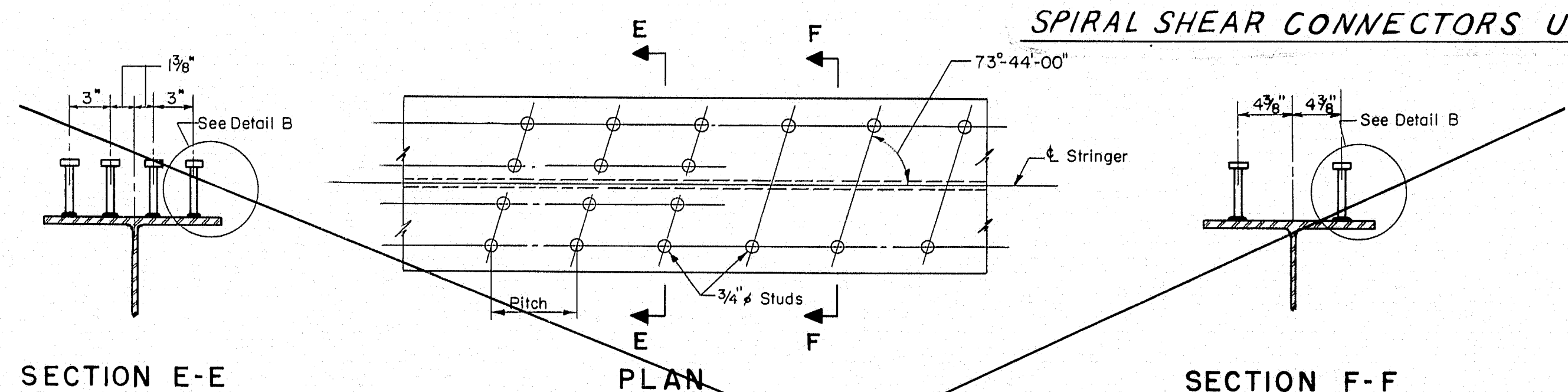
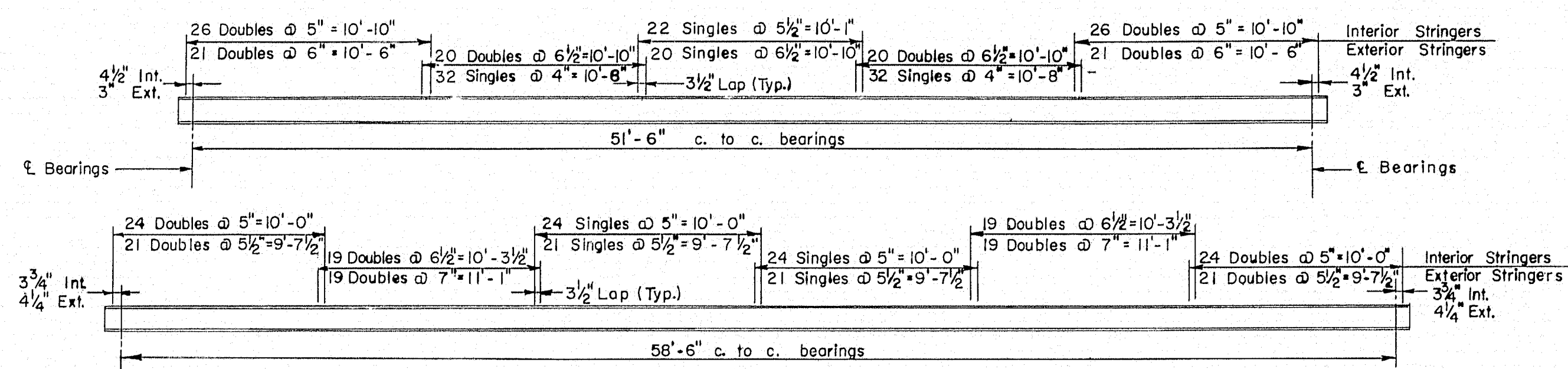
FAIRFIELD INTERSTATE



SECTION D-D

ELEVATION

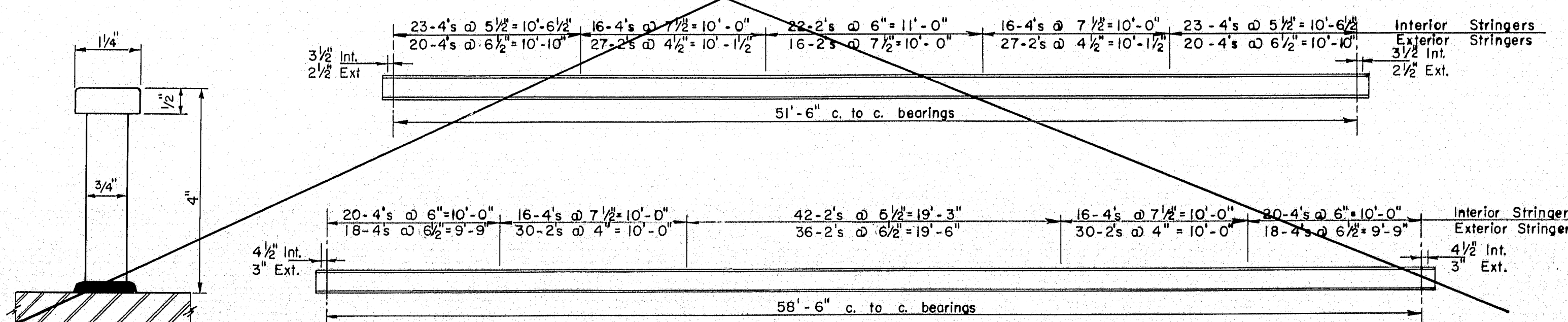
SECTION C-C



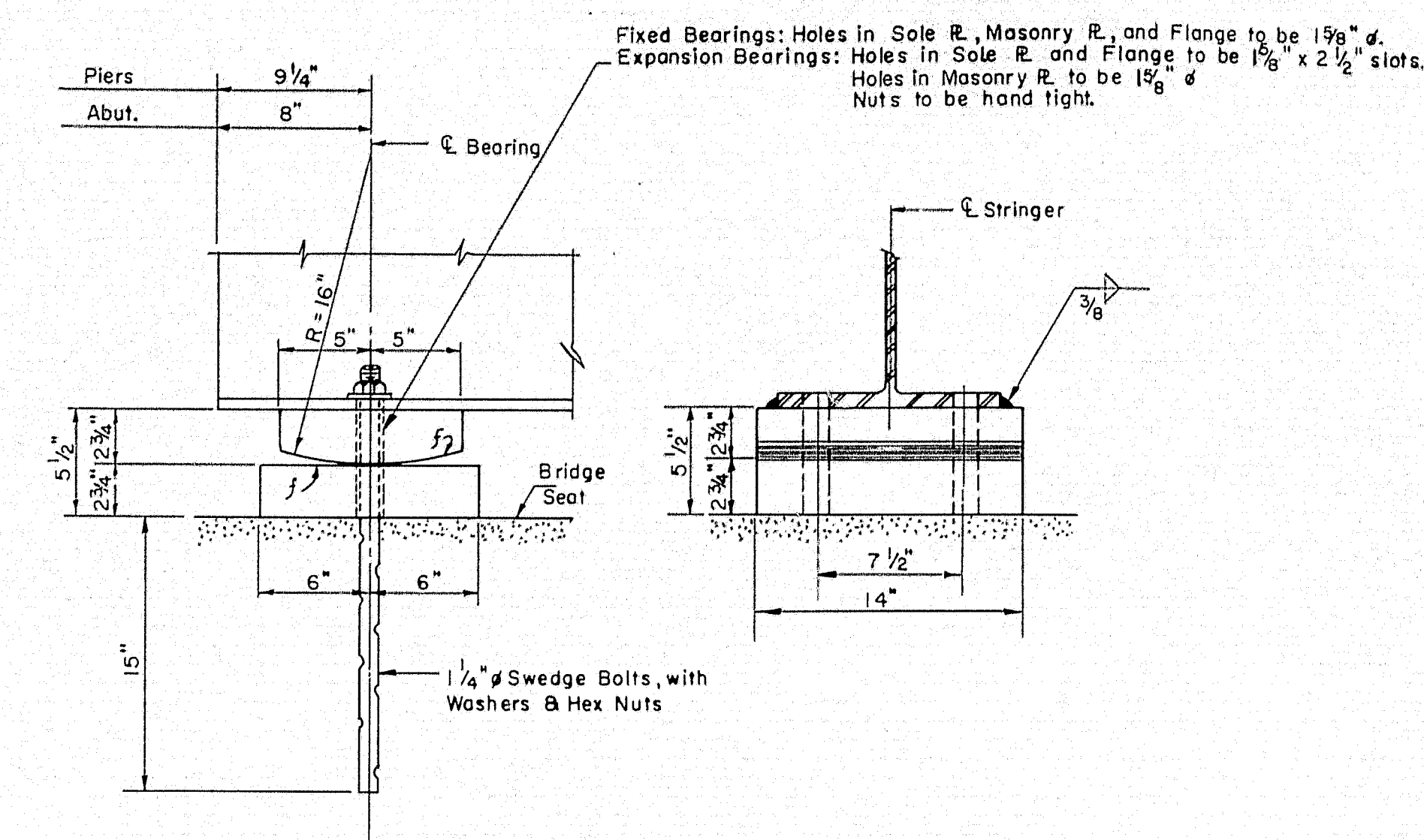
SECTION E-E

PLAN

SECTION F-F

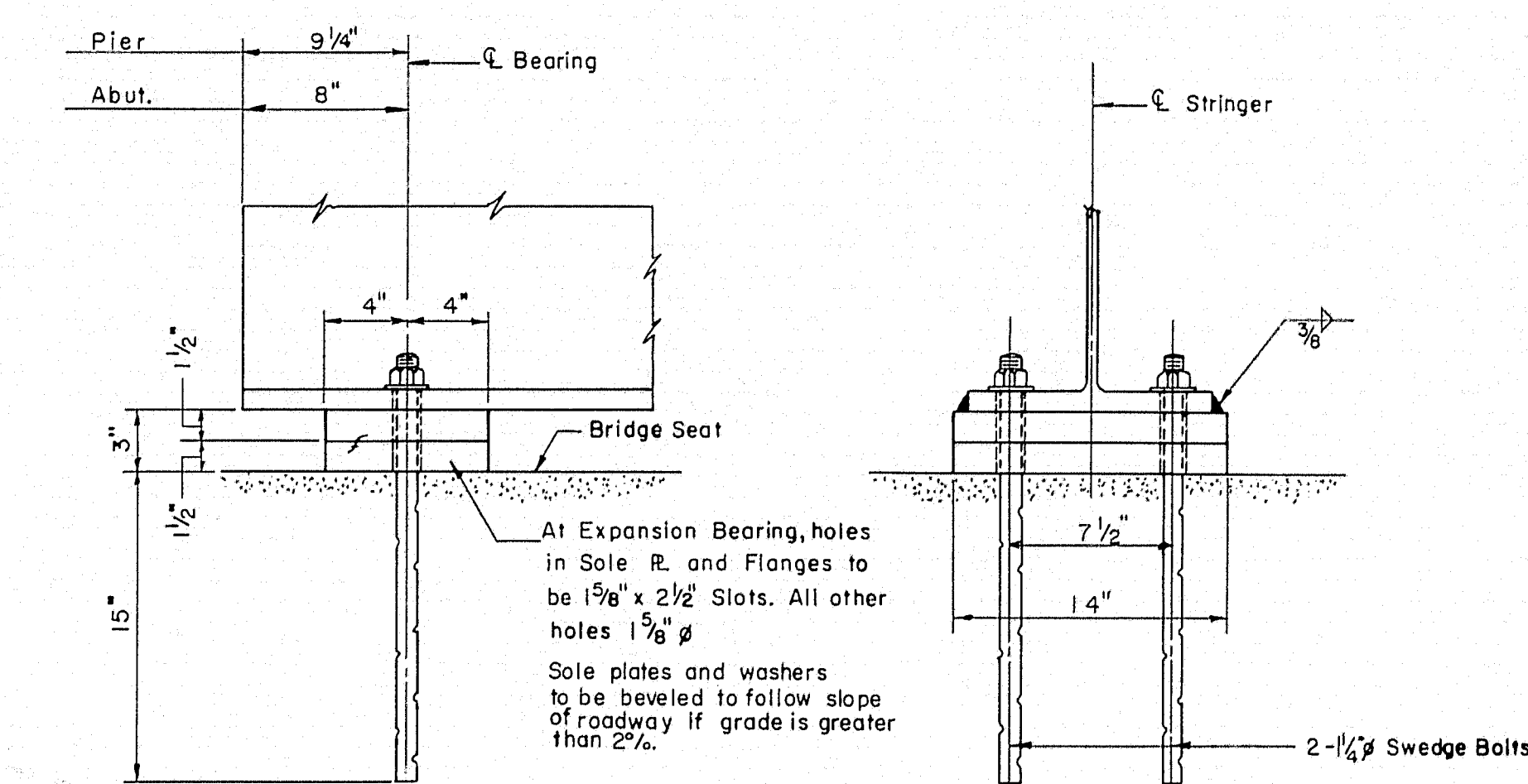


DETAIL B



EXPANSION & FIXED BEARING DETAIL (SPANS 1, 2, 3, & 4) Scale 1/2" = 1'-0"

Note: All anchor bolts to be centered true before bearing plates are set in position.

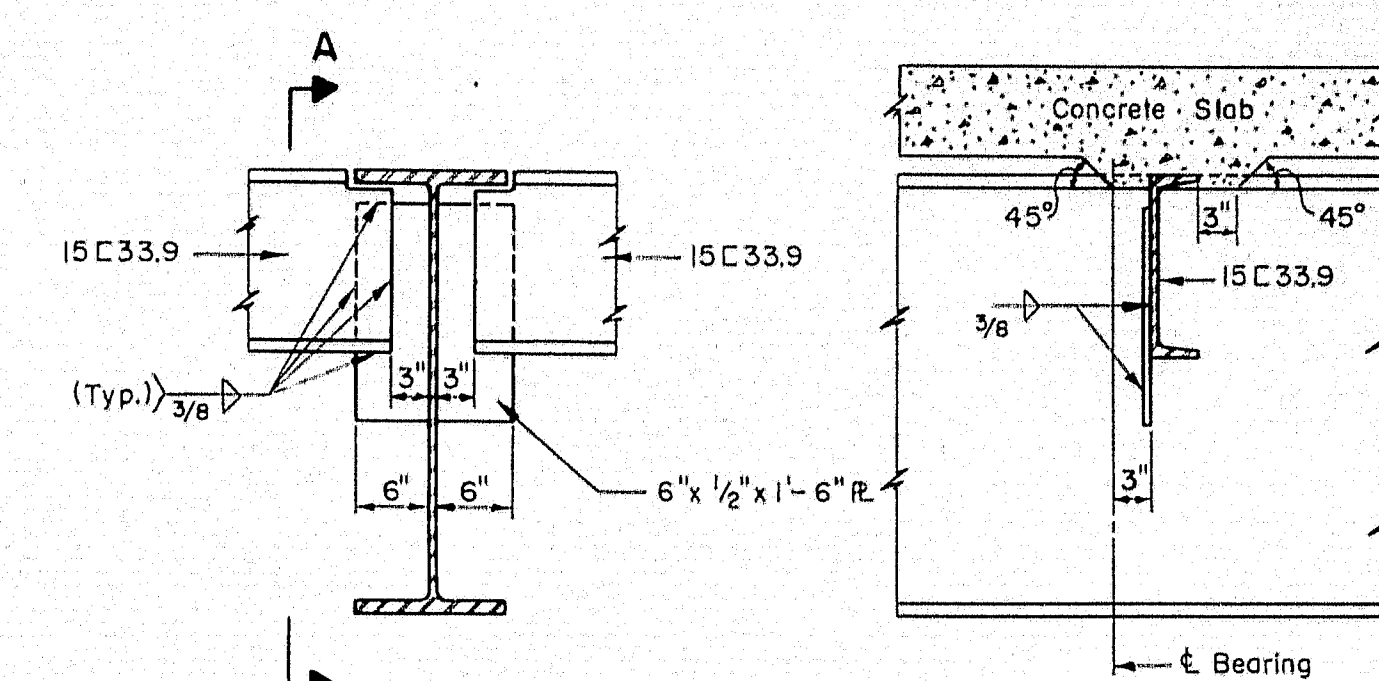


FIXED AND EXPANSION BEARING DETAILS (SPAN 5) Scale 1/2" = 1'-0"

ELEVATIONS OF BOTTOM OF ROADWAY SLAB												
Beam No.	1/4 Point	1/2 Point	3/4 Point	Center	1/4 Point	1/2 Point	3/4 Point	Center	1/4 Point	1/2 Point	3/4 Point	Center
1	194.12	193.97	193.84	193.67	193.48	192.71	192.46	192.15	191.79	191.37		
2	194.31	194.16	194.03	193.87	193.67	192.60	192.35	192.04	191.68	191.27		
3	194.49	194.35	194.21	194.05	193.85	191.00	190.64	190.22	189.76	189.24		
4	194.37	194.22	194.08	193.92	193.72	191.21	190.86	190.45	189.99	189.47		
5	194.25	194.08	193.93	193.78	193.50	191.43	191.07	190.67	190.21	189.70		
6	193.46	193.29	193.07	192.79	192.46	191.33	190.98	190.58	190.12	189.61		
7	193.65	193.48	193.26	192.99	192.66	191.23	190.88	190.49	190.03	189.53		
8	193.83	193.67	193.46	193.18	192.86	189.19	188.83	188.45	188.04	187.61		
9	193.70	193.55	193.34	193.07	192.75	189.42	189.06	188.69	188.28	187.84		
10	193.58	193.42	193.21	192.95	192.63	189.64	189.29	188.92	188.51	188.08		
11	192.43	192.16	191.84	191.47	191.04	189.56	189.21	188.84	188.45	188.01		
12	192.63	192.36	192.05	191.68	191.26	189.48	189.13	188.76	188.36	187.94		
13	192.83	192.57	192.26	191.89	191.47							

* Low station end of beam.

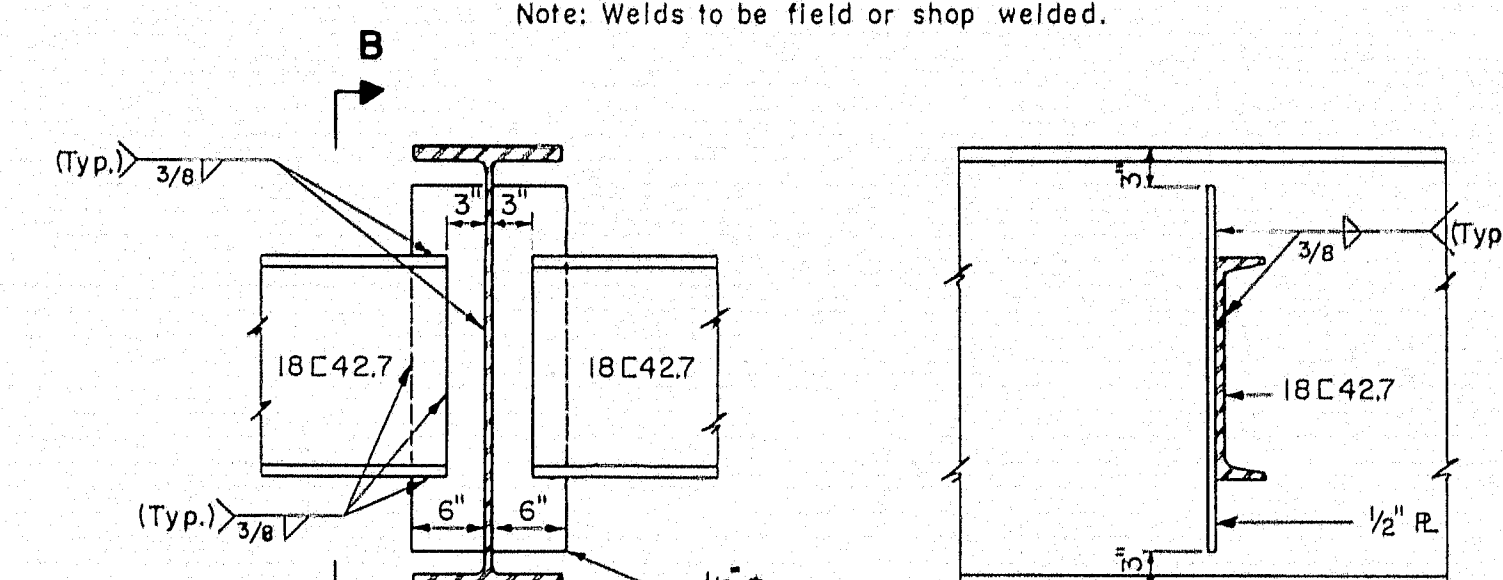
Note: After the structural steel is erected and before forms are built or concrete is placed, elevations on the top flange of the beam are to be obtained at the points indicated in the table. The difference between these elevations and those shown in table gives the actual blocking distance from top of beam to bottom of slab. See Haunch Detail on Sheet 6.



WELDED END DIAPHRAGM CONNECTION

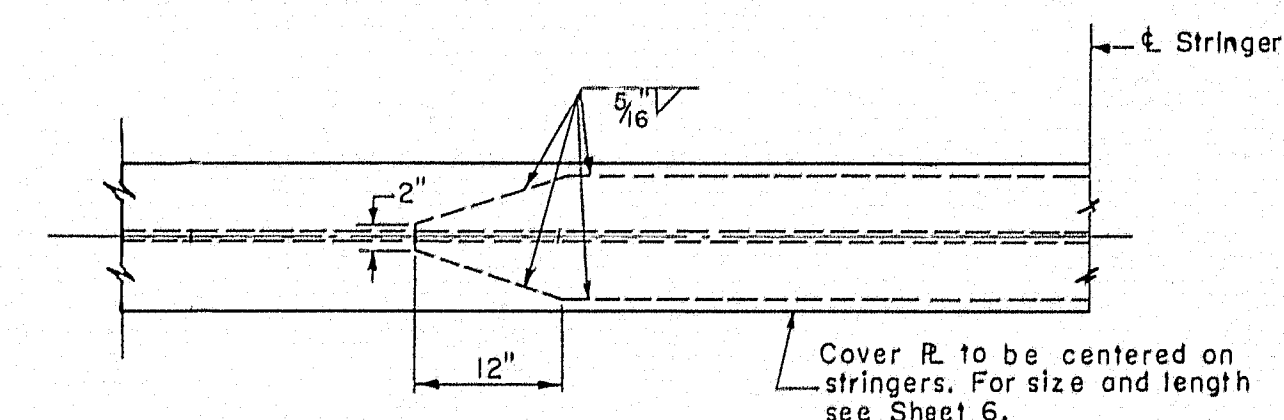
Scale: 3/4" = 1'-0"

Note: Welds to be field or shop welded.



WELDED DIAPHRAGM CONNECTION

Scale: 3/4" = 1'-0"



COVER PLATE DETAIL

NOT TO SCALE

THE CLARKSON ENGINEERING CO., INC.

DESIGN J.M.-J.T. CHECK G.B.-H.P. BRIDGE NO. SURVEY
DRAWN E.K. APPROVED PLOT

STATE HIGHWAY COMMISSION

INTERSTATE #95

UNDER

WESTERN AVENUE

IN THE CITY OF

FAIRFIELD

SOMERSET COUNTY

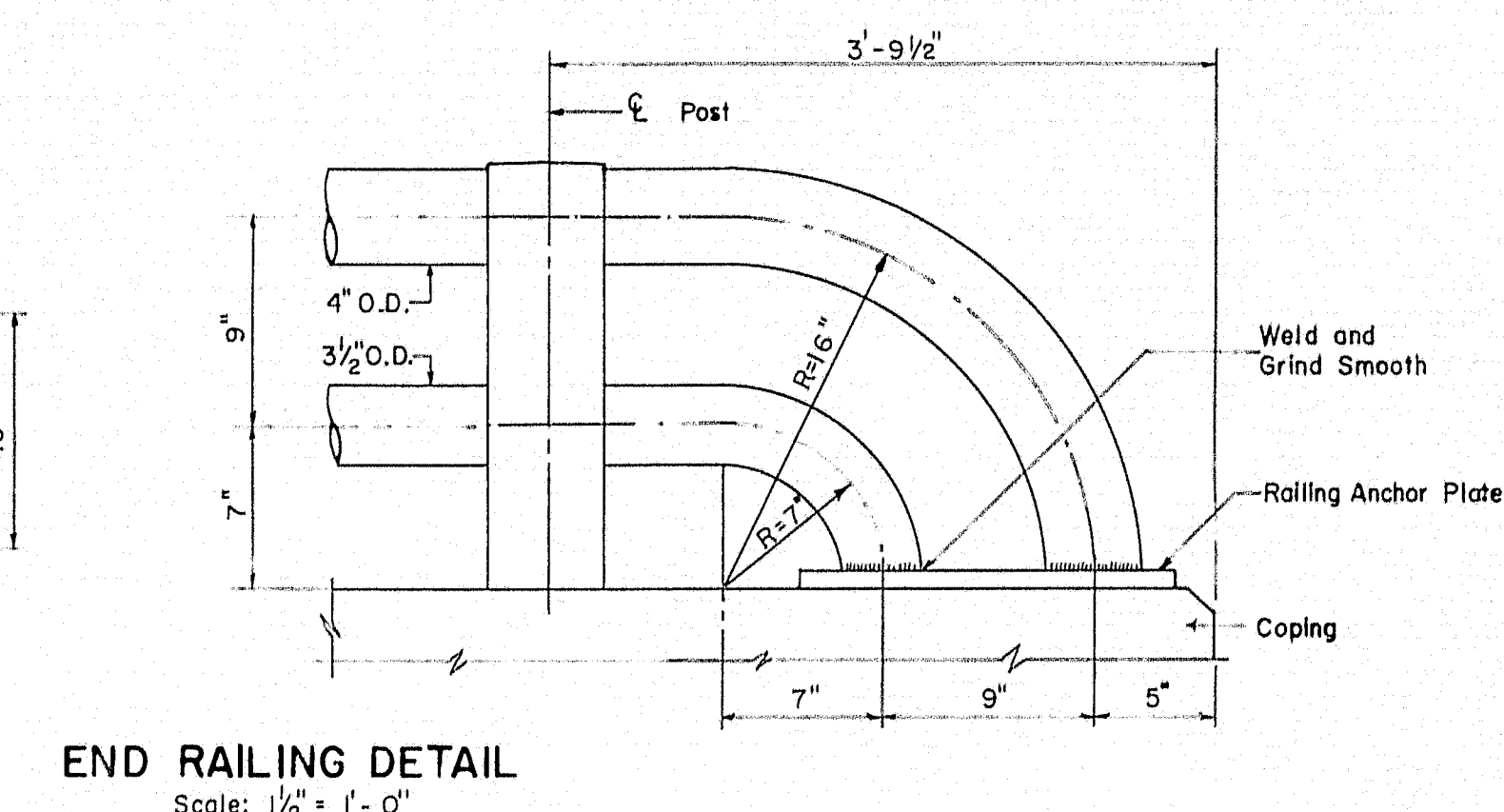
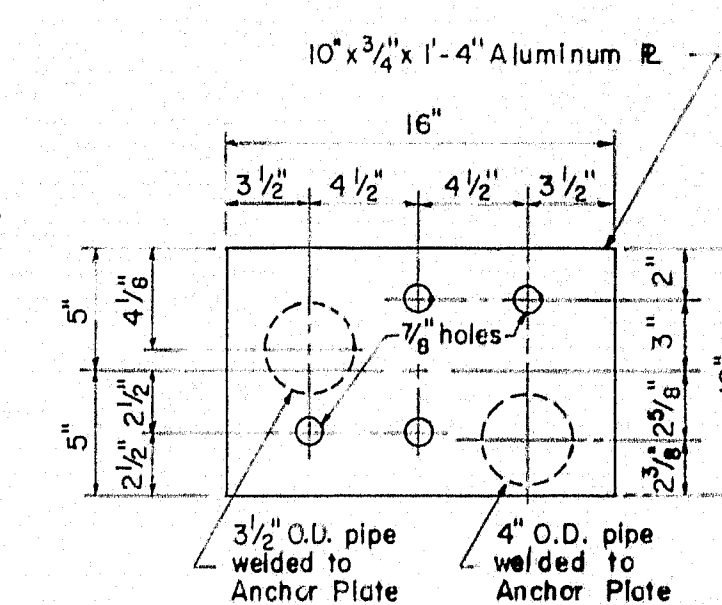
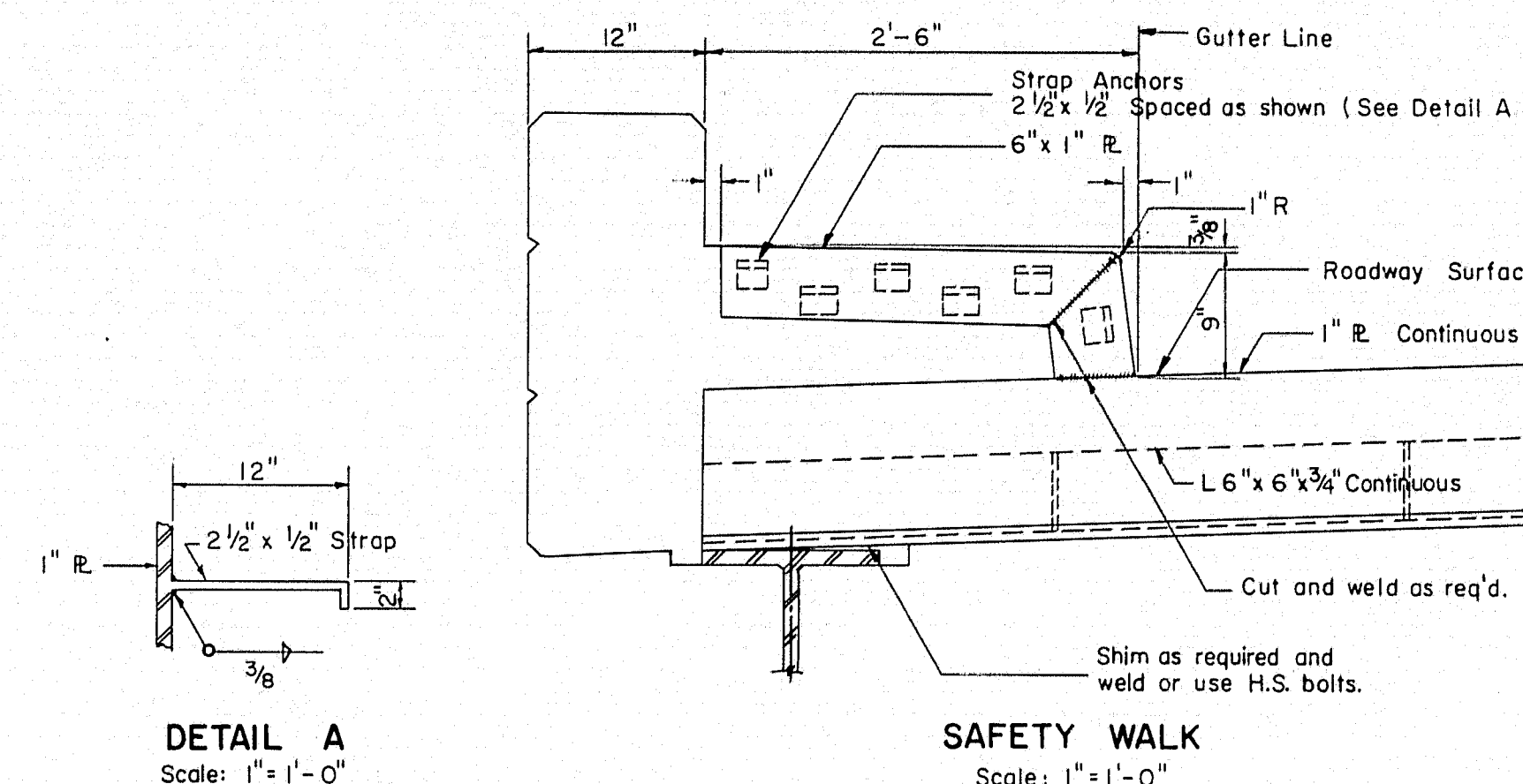
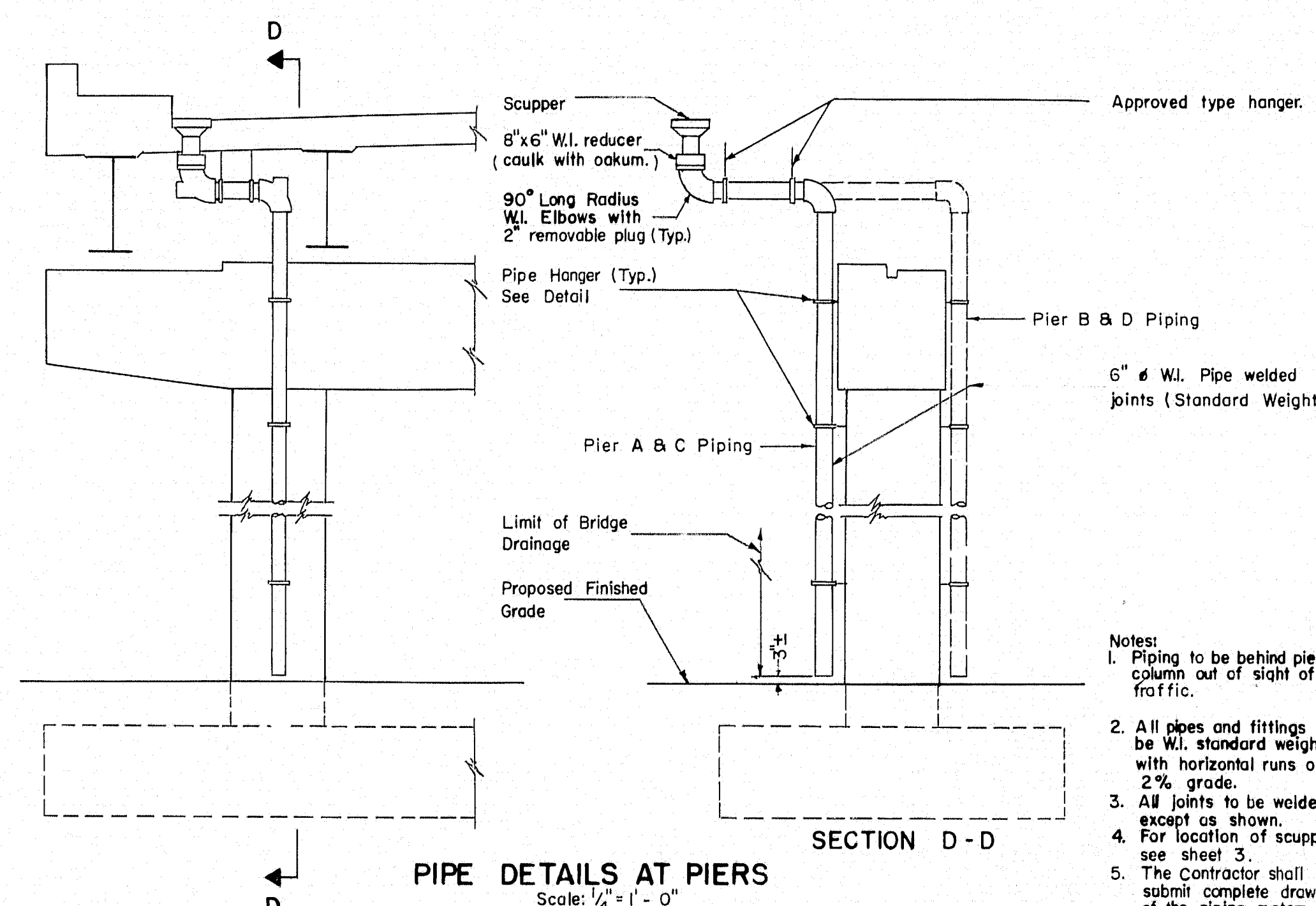
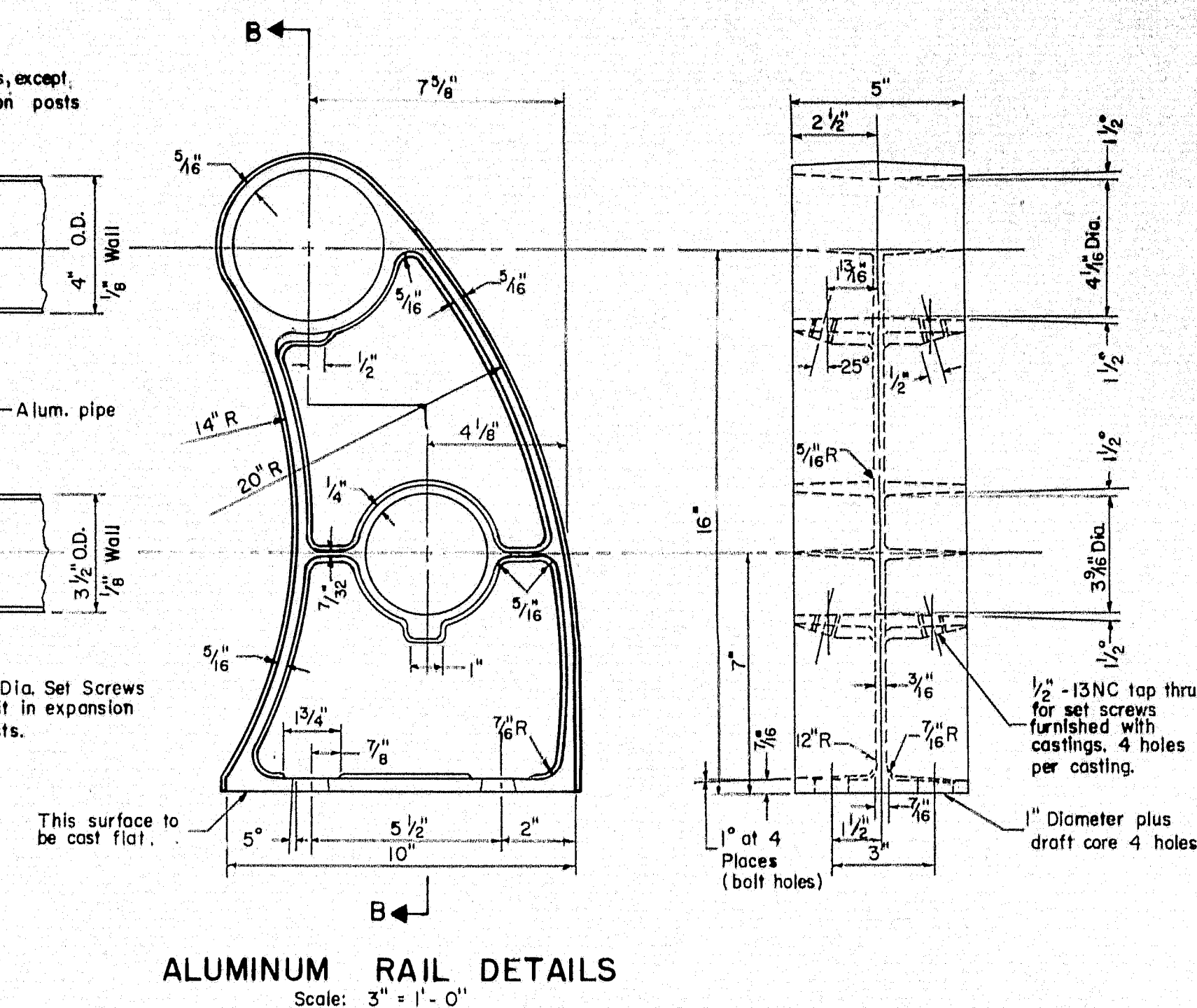
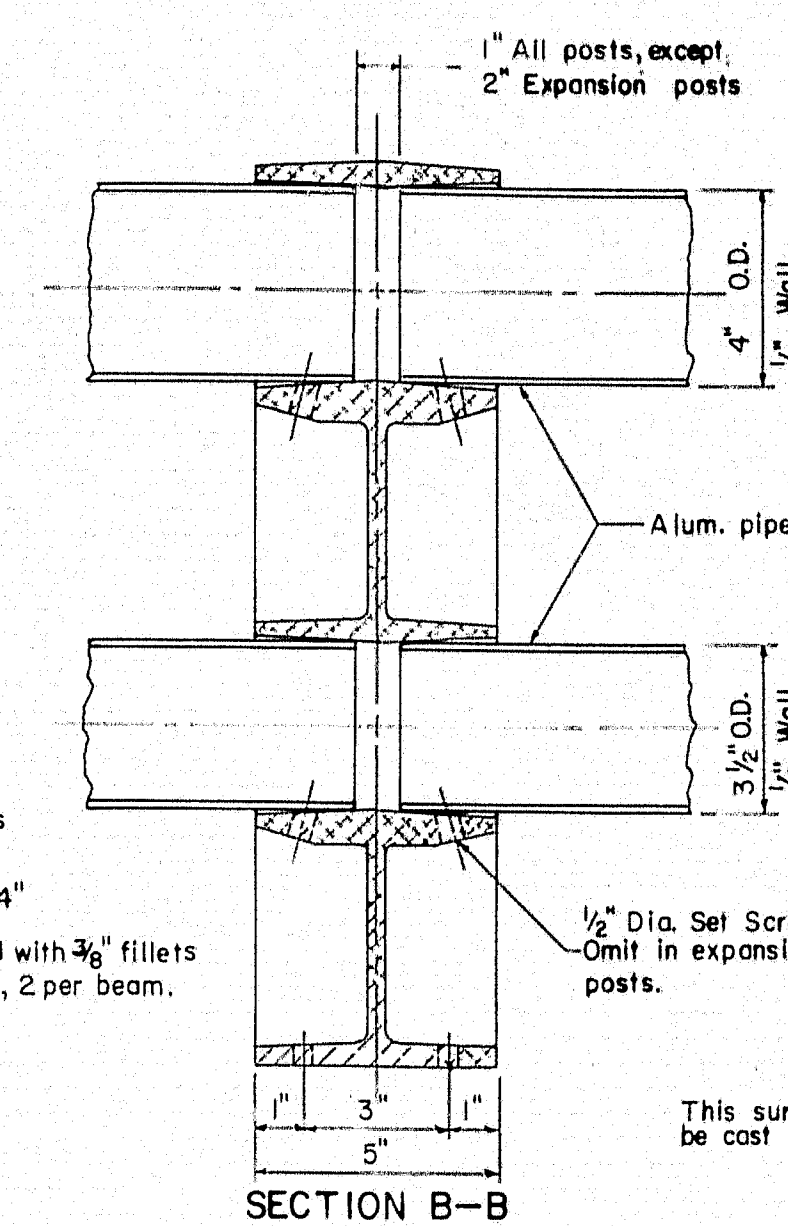
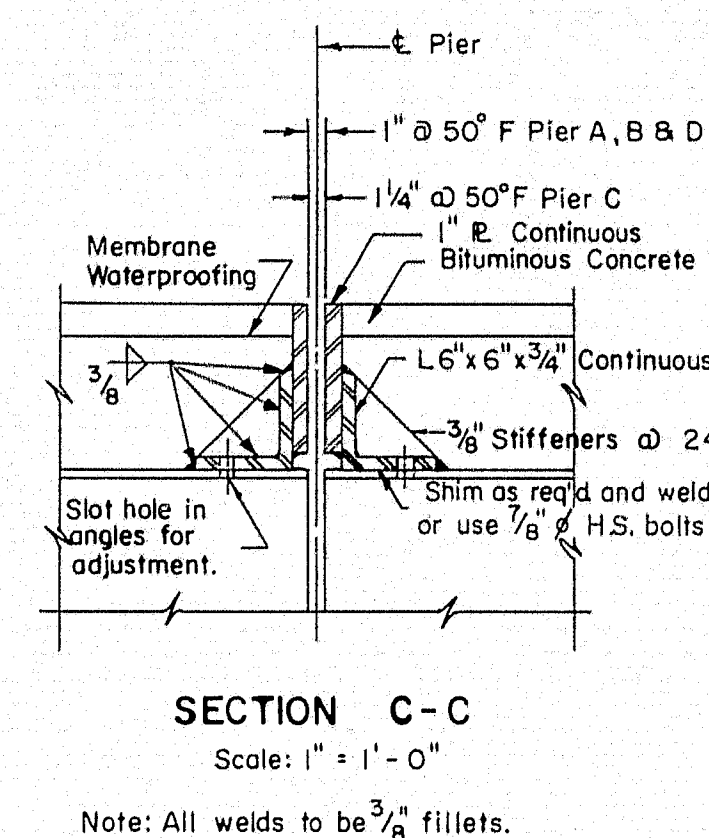
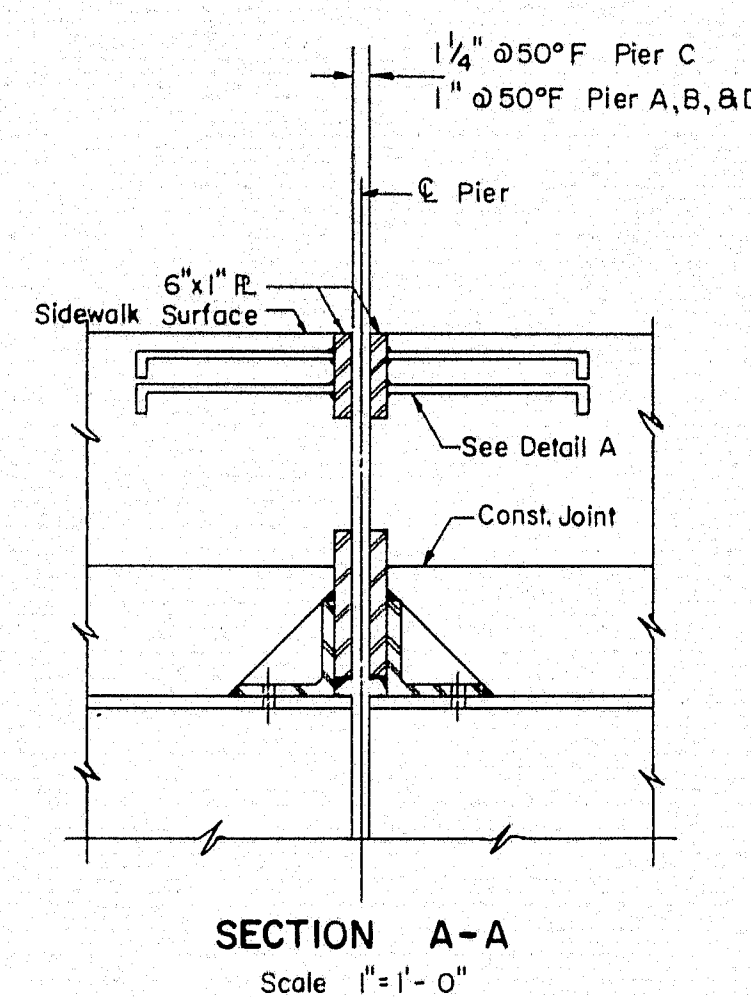
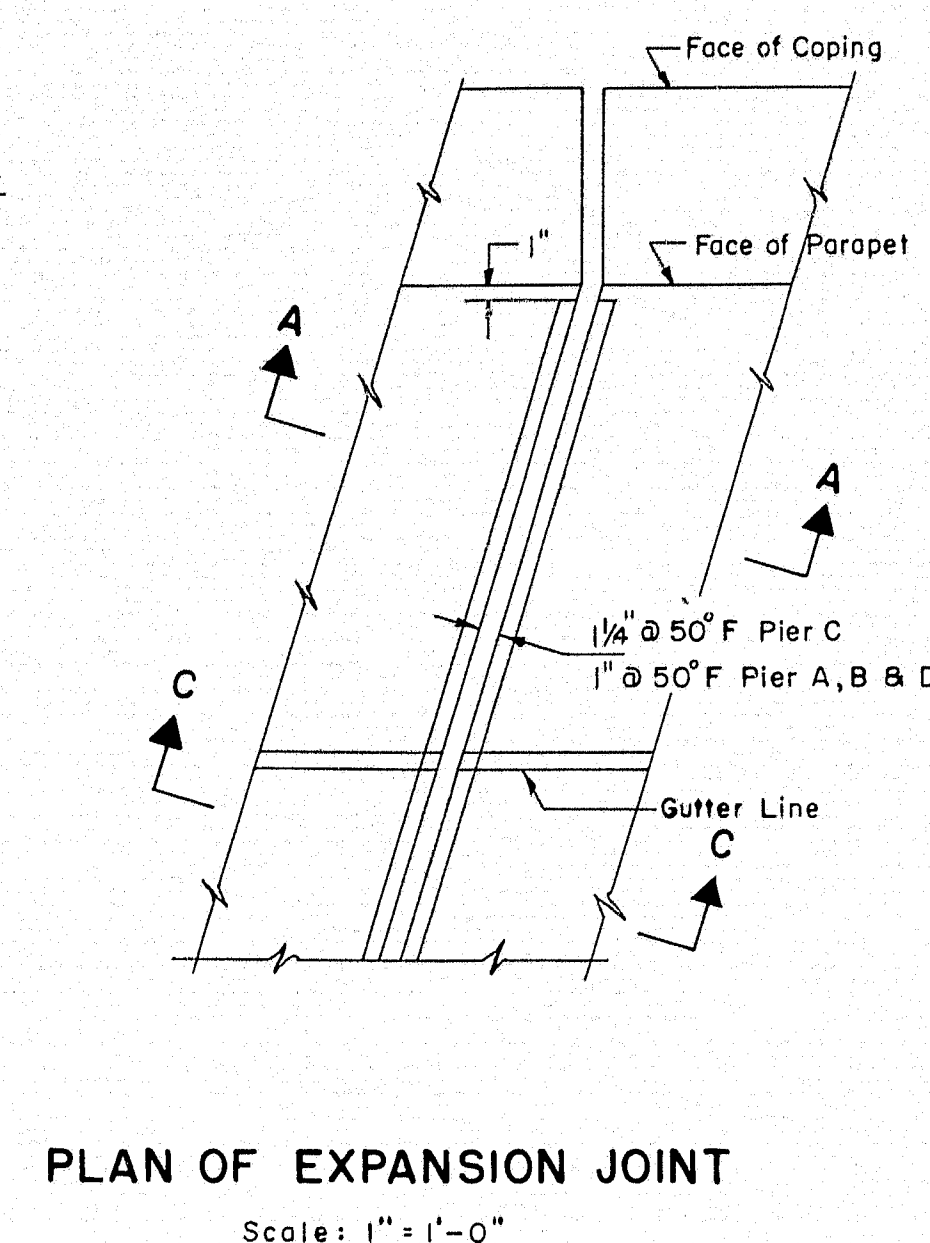
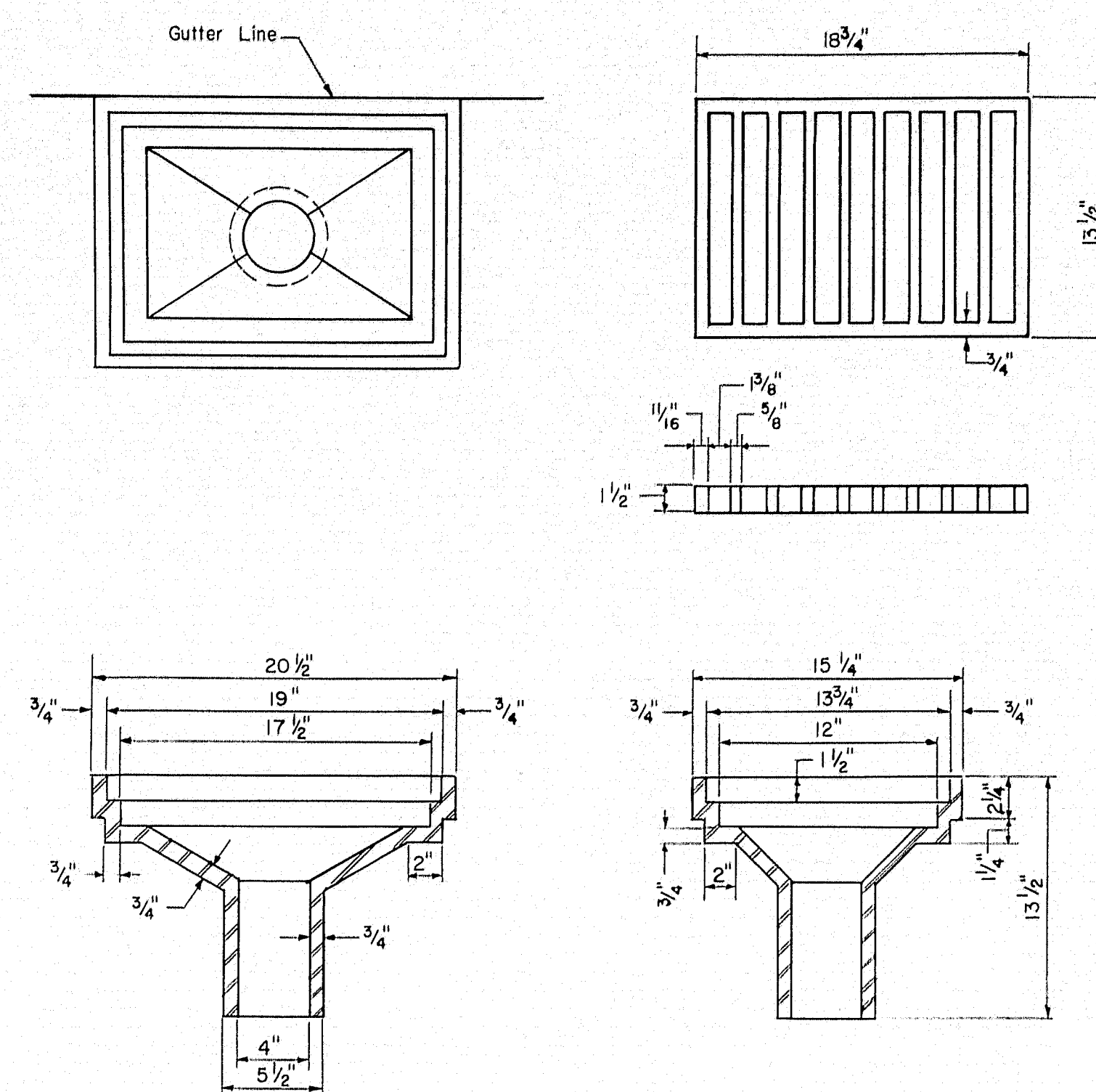
SUPERSTRUCTURE DETAILS

SHEET 7 OF 8 SHEETS

AUGUSTA, MAINE

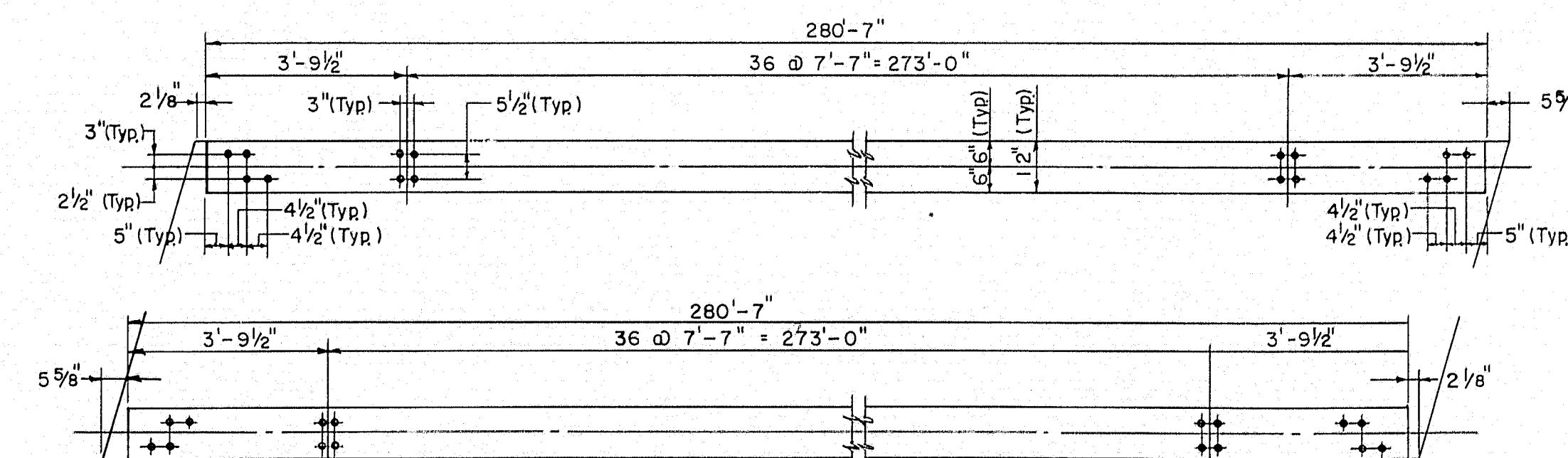
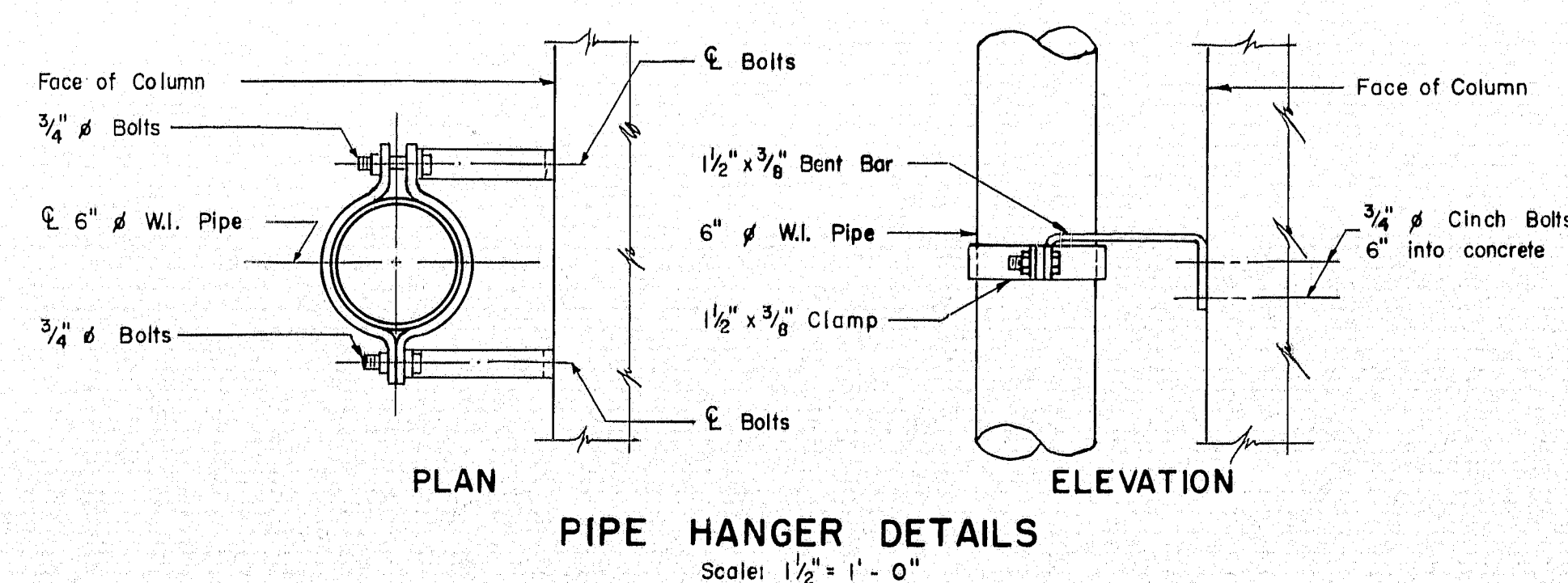
B.P.R. REG. NO.	STATE	PROJECT NO.	SHEET NO	TOTAL SHEETS
1	MAINE	I-95-6(25)	20	27

1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100
FAIRFIELD INTERSTATE																																																																																																			



Note: 1. Any variation in railing dimensions shall be submitted for approval.
2. All anchor bolts, nuts & washers to be aluminum alloy.
3. All anchor bolts to be $\frac{3}{4}$ " ϕ bolts (Set 11" into concrete).

Note: Membrane waterproofing and Bituminous surfacing
are not in this contract



THE CLARKESON ENGINEERING CO., INC.

DESIGN J.M.-J.T.	CHECK G.B.-H.P.	BRIDGE NO.
	W.A.H.	SURVEY
DRAWN REM.-DAI	APPROVED C.J.M.	PLOT

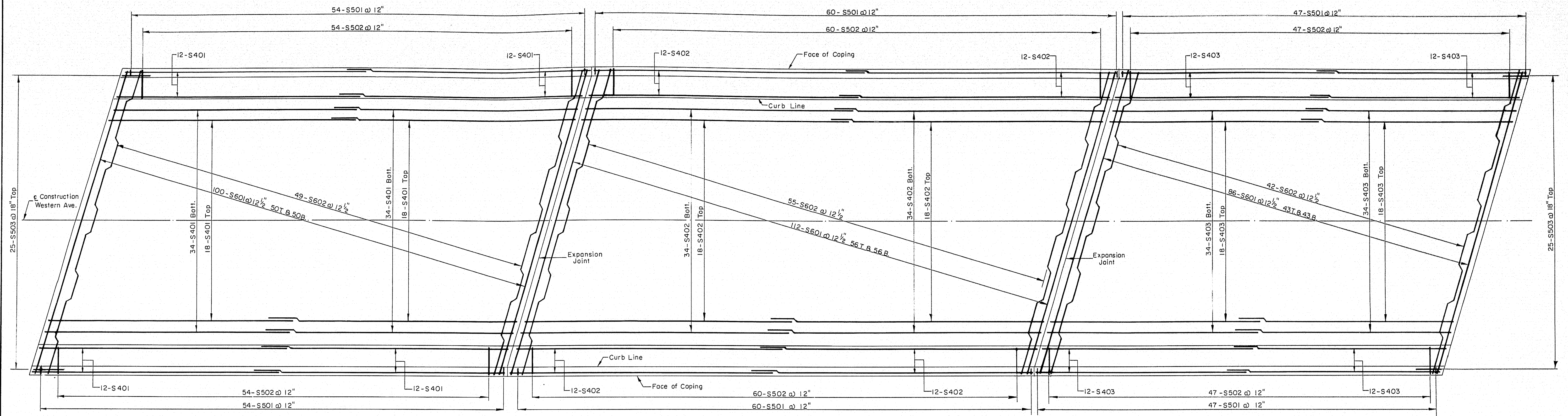
STATE HIGHWAY COMMISSION
INTERSTATE #95
UNDER
WESTERN AVENUE
IN THE CITY OF
FAIRFIELD
SOMERSET COUNTY
DETAILS

SHEET 8 OF 8 SHEETS

AUGUSTA, MAINE

B. P. R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-6(25)	204	27

FAIRFIELD INTERSTATE



SPAN 1

SPANS 2, 3 & 4

Note: For spacing of S401, S402 & S403 bars see Cross Section

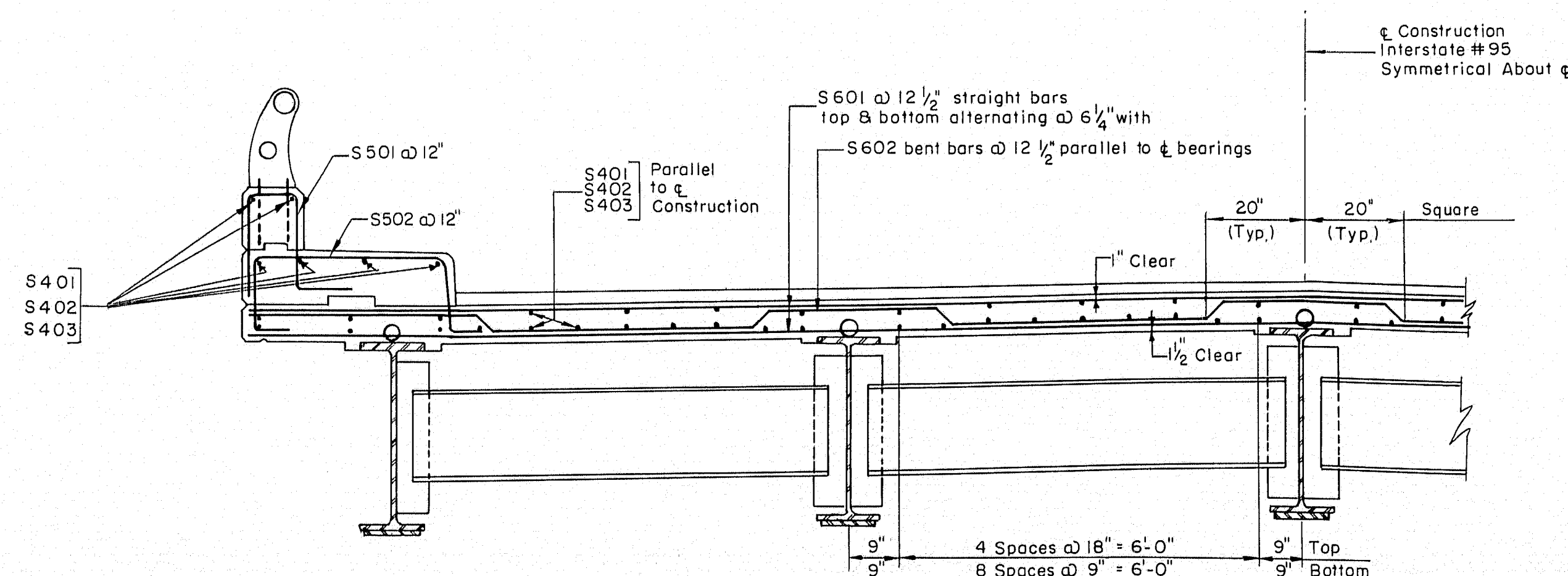
SPAN 5

DECK SLAB REINFORCEMENT

Scale: $\frac{3}{16}'' = 1'-0''$

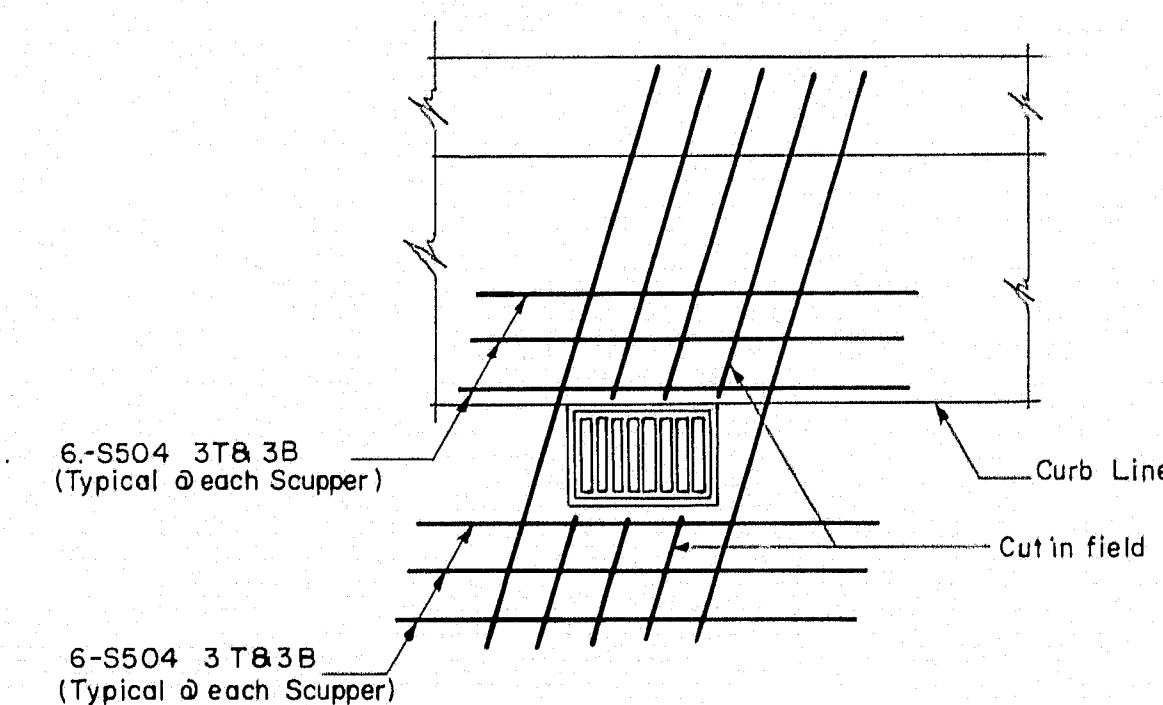
ESTIMATED QUANTITIES

DECK SLAB 65,154 LBS.



TYPICAL CROSS SECTION

Scale: $\frac{1}{2}'' = 1'-0''$



SCUPPER REINFORCEMENT

Scale: $\frac{1}{2}'' = 1'-0''$

THE CLARKESON ENGINEERING CO., INC.

DESIGN R.L.M.	CHECK P.W.G.	BRIDGE NO. SURVEY
DRAWN D.A.T.	APPROVED W.A.H.-C.J.M.	PLOT

STATE HIGHWAY COMMISSION

INTERSTATE #95

UNDER

WESTERN AVENUE

IN THE CITY OF

FAIRFIELD

SOMERSET COUNTY

DECK REINFORCEMENT

SHEET 1 OF 5 SHEETS

AUGUSTA, MAINE

74-21

FAIRFIELD INTERSTATE

ELEVATION WESTERLY ABUTMENT

PLAN EASTERLY ABUTMENT FOOTING

PLAN WESTERLY ABUTMENT FOOTING

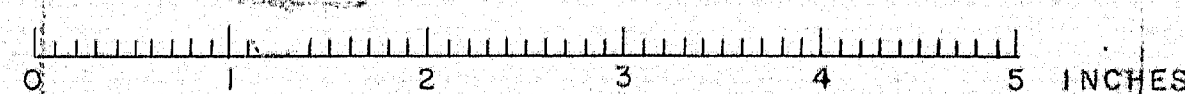
SECTION A-A

SECTION C-C

SECTION B-B

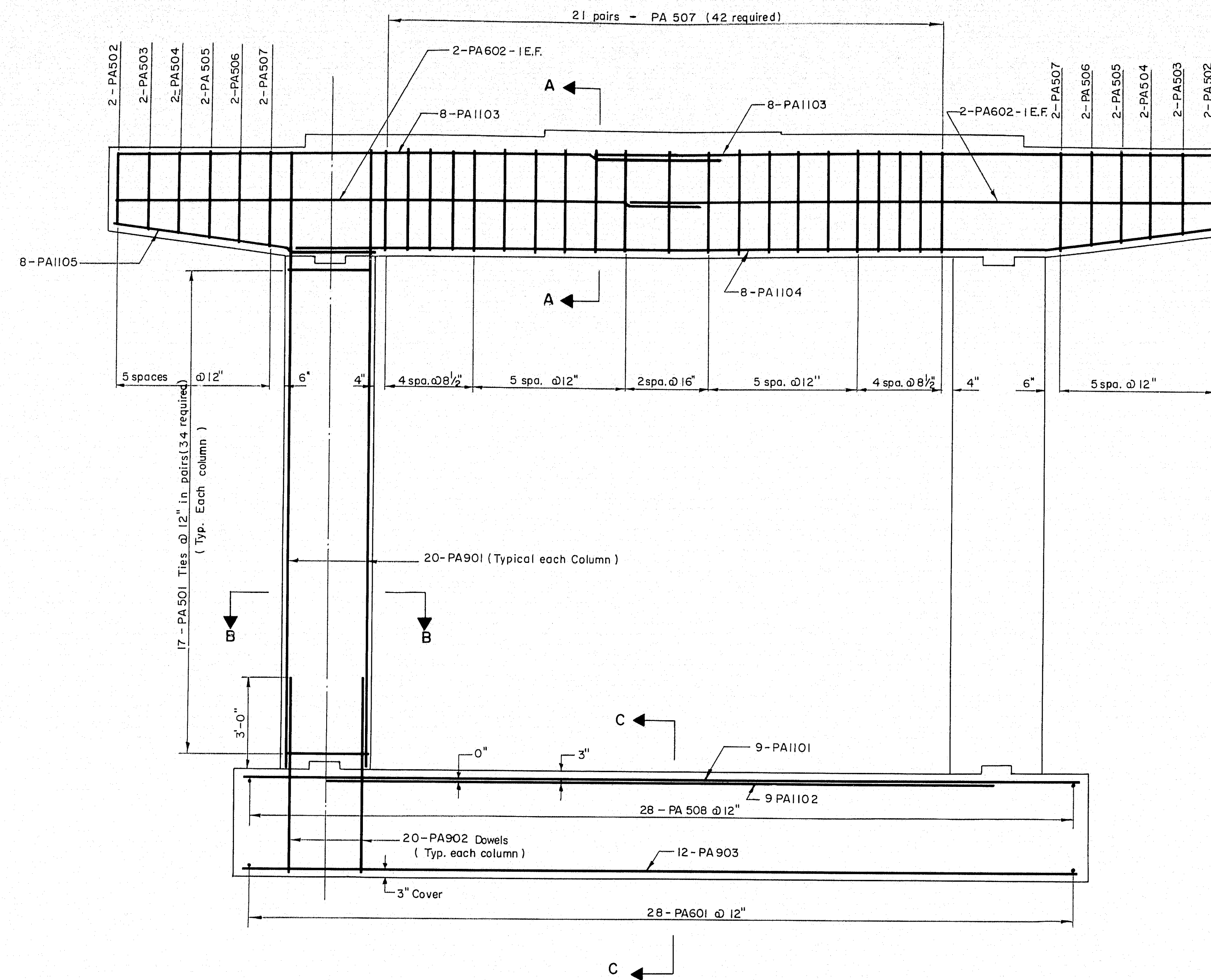
SECTION D-D

K-E PHOENIX BLUE 168 D. L. M. INC.
 ① TRACING CLOTH 19815



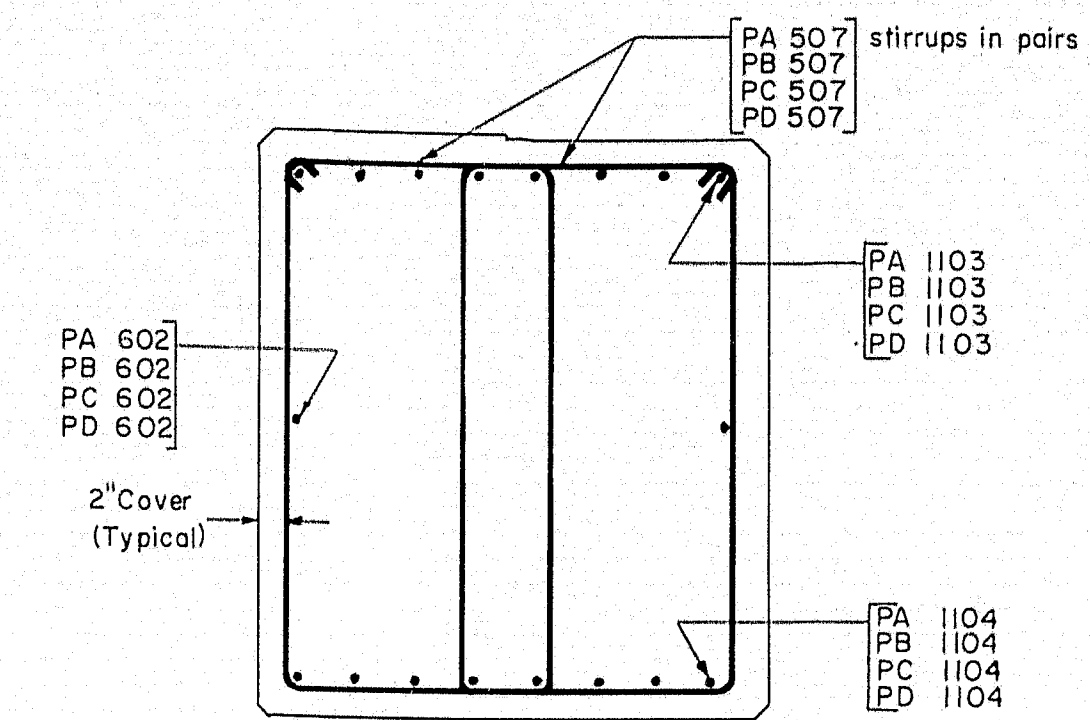
B.P.R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	L-95-6(25)	20 C	27

FAIRFIELD INTERSTATE

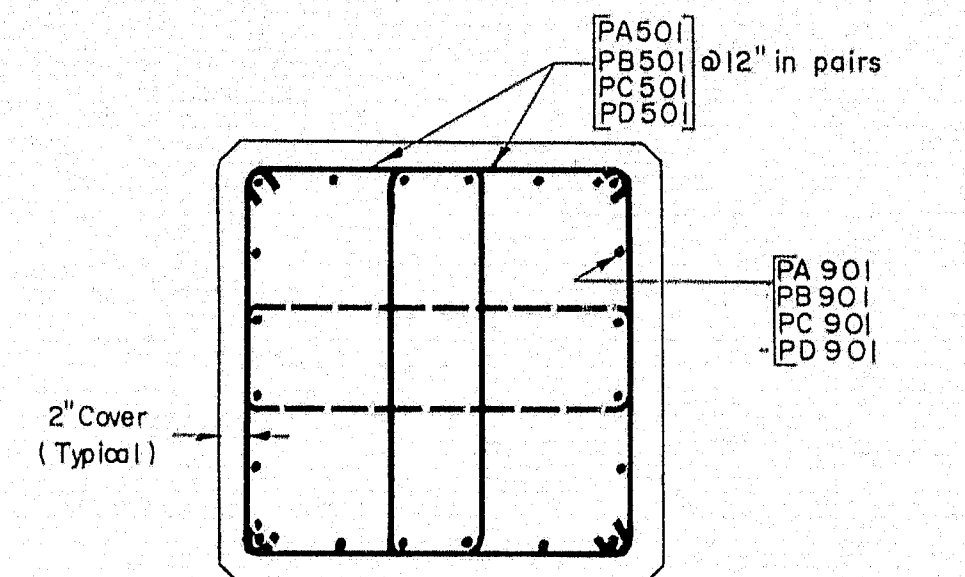


ELEVATION PIER A
ELEVATION PIERS B, C & D (SIMILAR)
Scale: 3/8" = 1'-0"

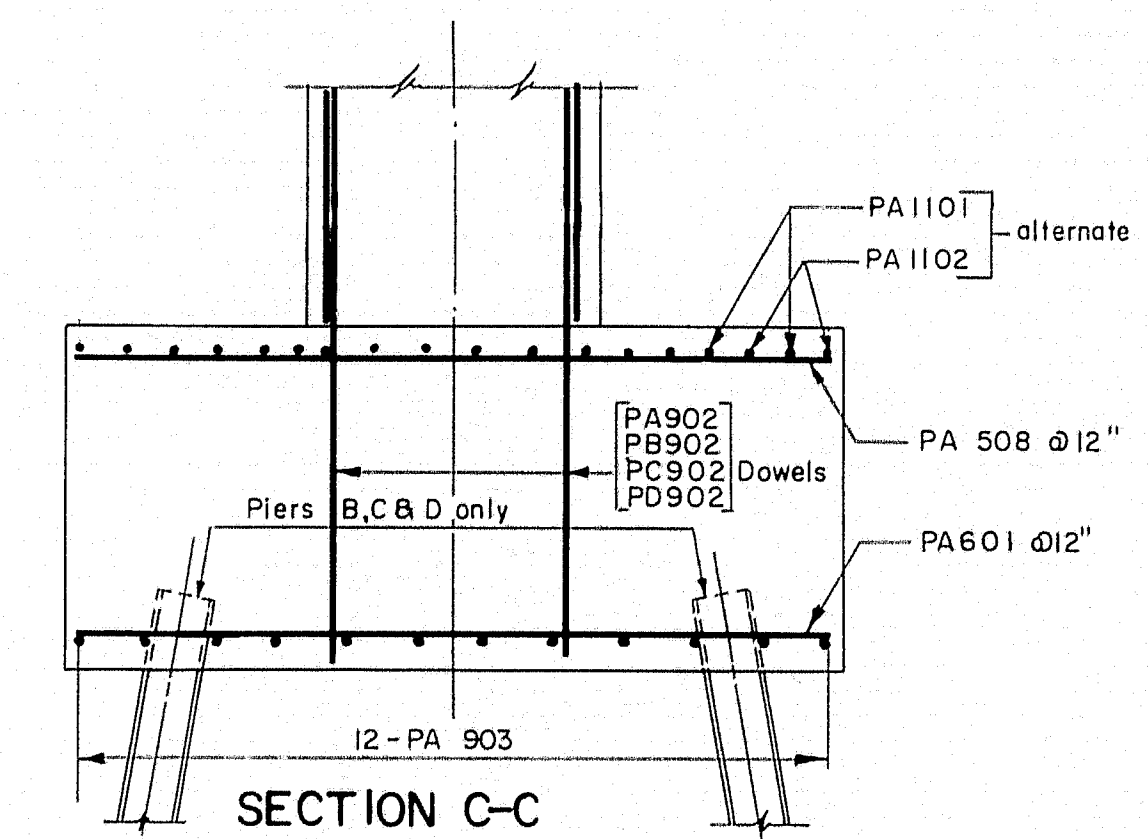
NOTE: Reinforcing in Piers B, C & D similar
except to be prefixed PB, PC & PD respectively



SECTION A-A
Scale: 3/4" = 1'-0"



SECTION B-B
Scale: 3/4" = 1'-0"



SECTION C-C
Scale: 3/8" = 1'-0"

ESTIMATED QUANTITIES

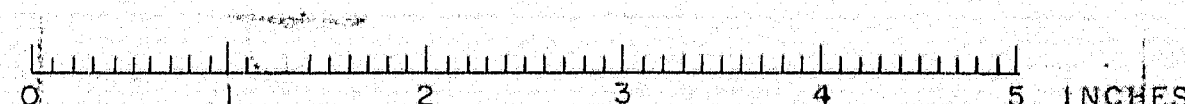
PIER A	12,632 LBS
PIER B	12,598 LBS
PIER C	12,751 LBS
PIER D	12,399 LBS

THE CLARKSON ENGINEERING CO., INC.

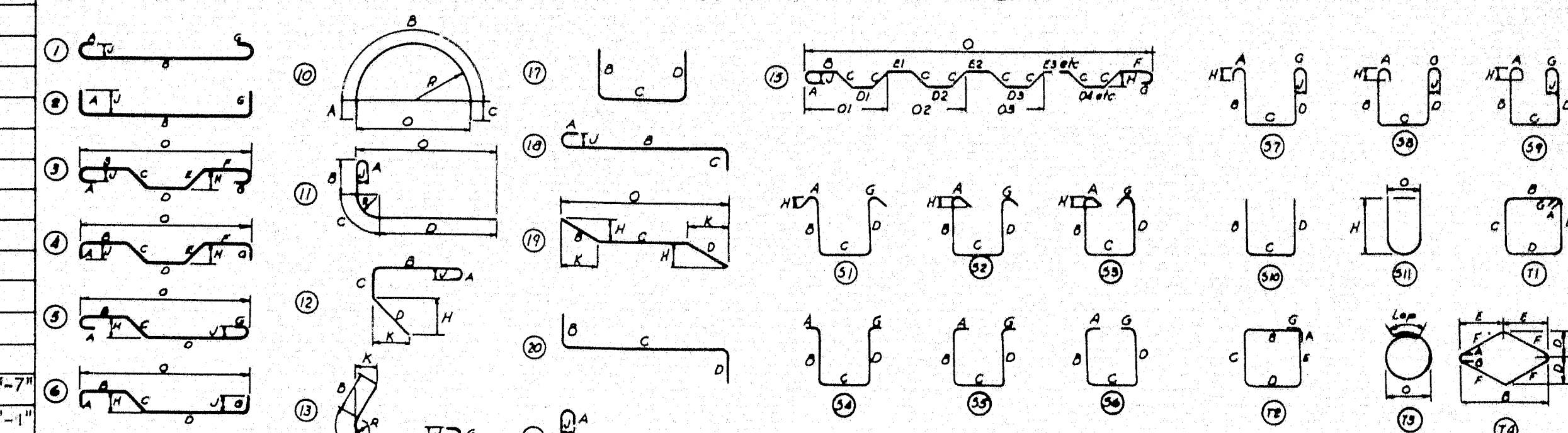
DESIGN R.L.M.	CHECK P.W.G.	BRIDGE NO.
DRAWN D.A.T.	APPROVED W.A.H.-C.J.M.	SURVEY PLOT

STATE HIGHWAY COMMISSION
INTERSTATE #95
UNDER
WESTERN AVENUE
IN THE CITY OF
FAIRFIELD
SOMERSET COUNTY
PIER A REINFORCEMENT

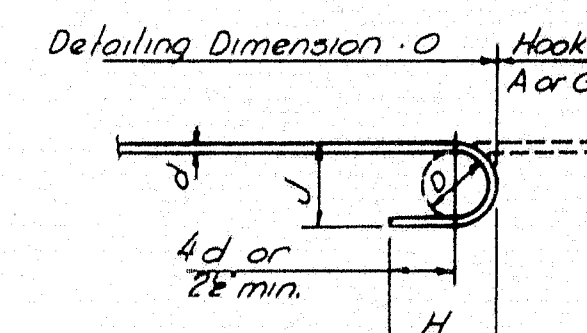
SHEET 3 OF 5 SHEETS AUGUSTA, MAINE



TYPICAL BAR BENDS



- NOTES:
- All dimensions are out to out of bar.
 - J' dimensions on 180° hooks to be shown only where necessary to restrict hook size, otherwise standard hooks are to be used.
 - Where J' is not shown, J' will be kept equal to or less than 'H'. Where J' can exceed 'H', it should be shown.
 - H' dimension on stirrups to be shown where necessary to restrict hooks.
 - Where bars are to be bent more accurately than standard bending tolerances, bending dimensions which require closer marking should have limits indicated.
 - Figures in circles show types.
 - No allowance for bend curvature is to be made except for standard hook & radii in excess of same.



STANDARD HOOK DETAIL

BAR SIZES

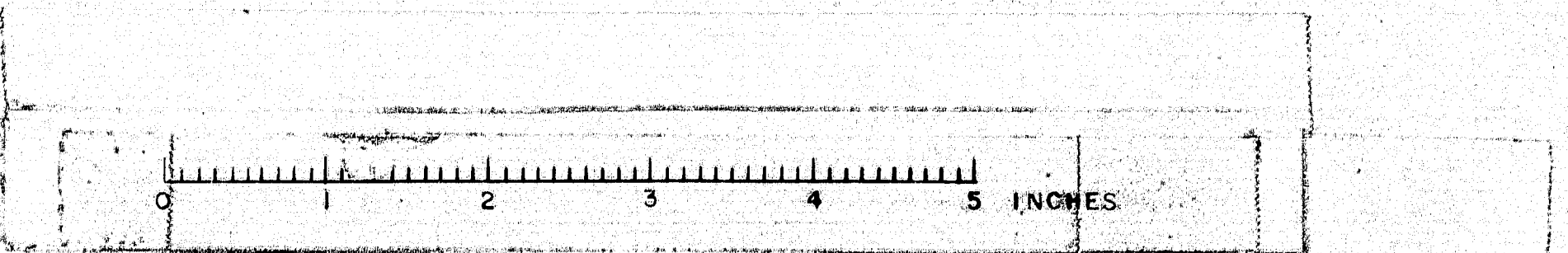
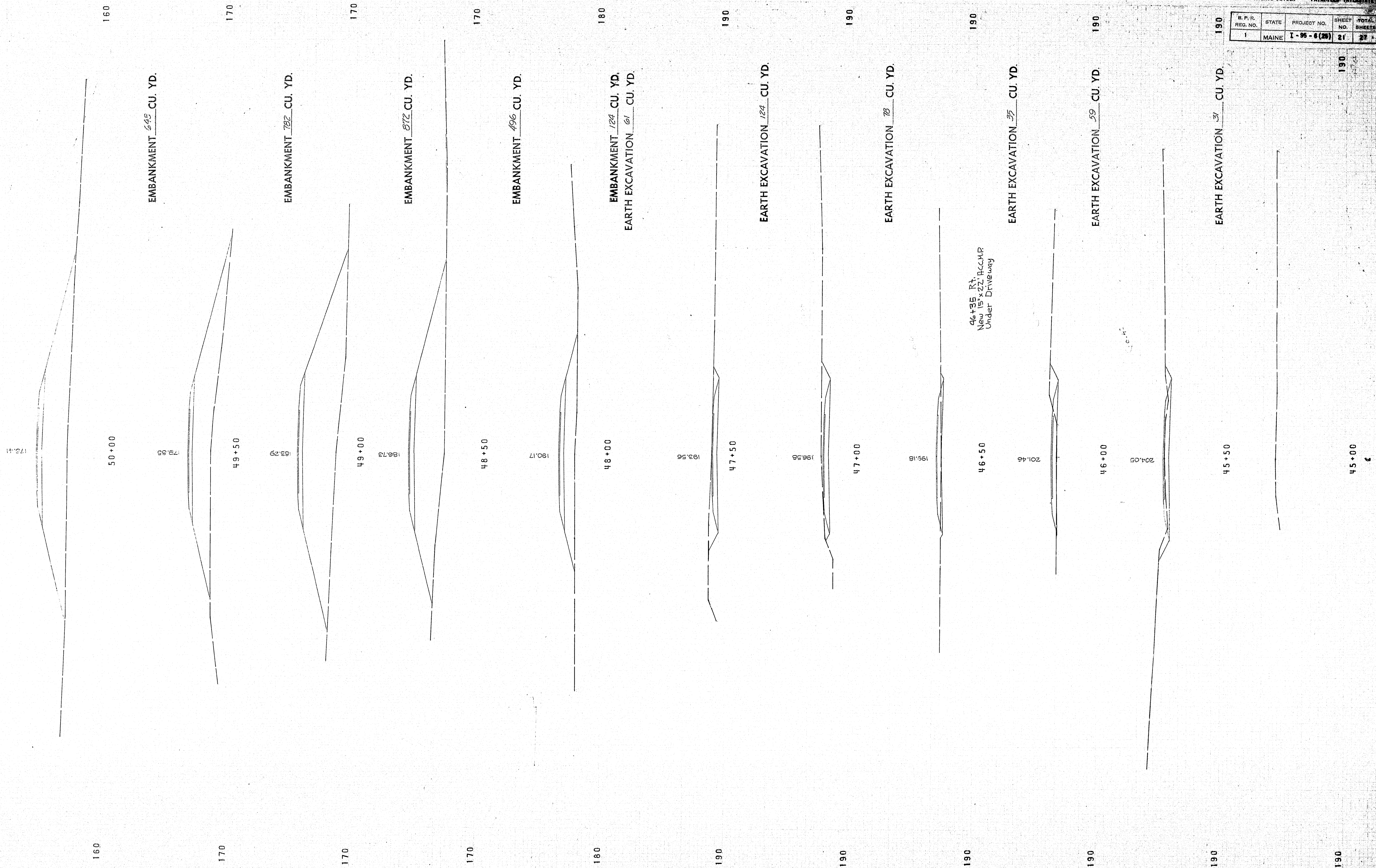
Equivalent Size	Present (Numbers)
1/4"	#2
3/8"	#3
1/2"	#4
5/8"	#5
3/4"	#6
7/8"	#7
1"	#8
1 1/8"	#9
1 1/4"	#10
1 1/2"	#11

8. Bar reinforcement metal shall conform to the requirements of the Standard Specifications for new Billet Steel Concrete Reinforcement Bars (Intermediate Grade) Serial Designation A.A.S.H.O. M 31-48 or its latest revision. All bars shall be deformed to conform with A.S.T.M. Specifications A 305-49.

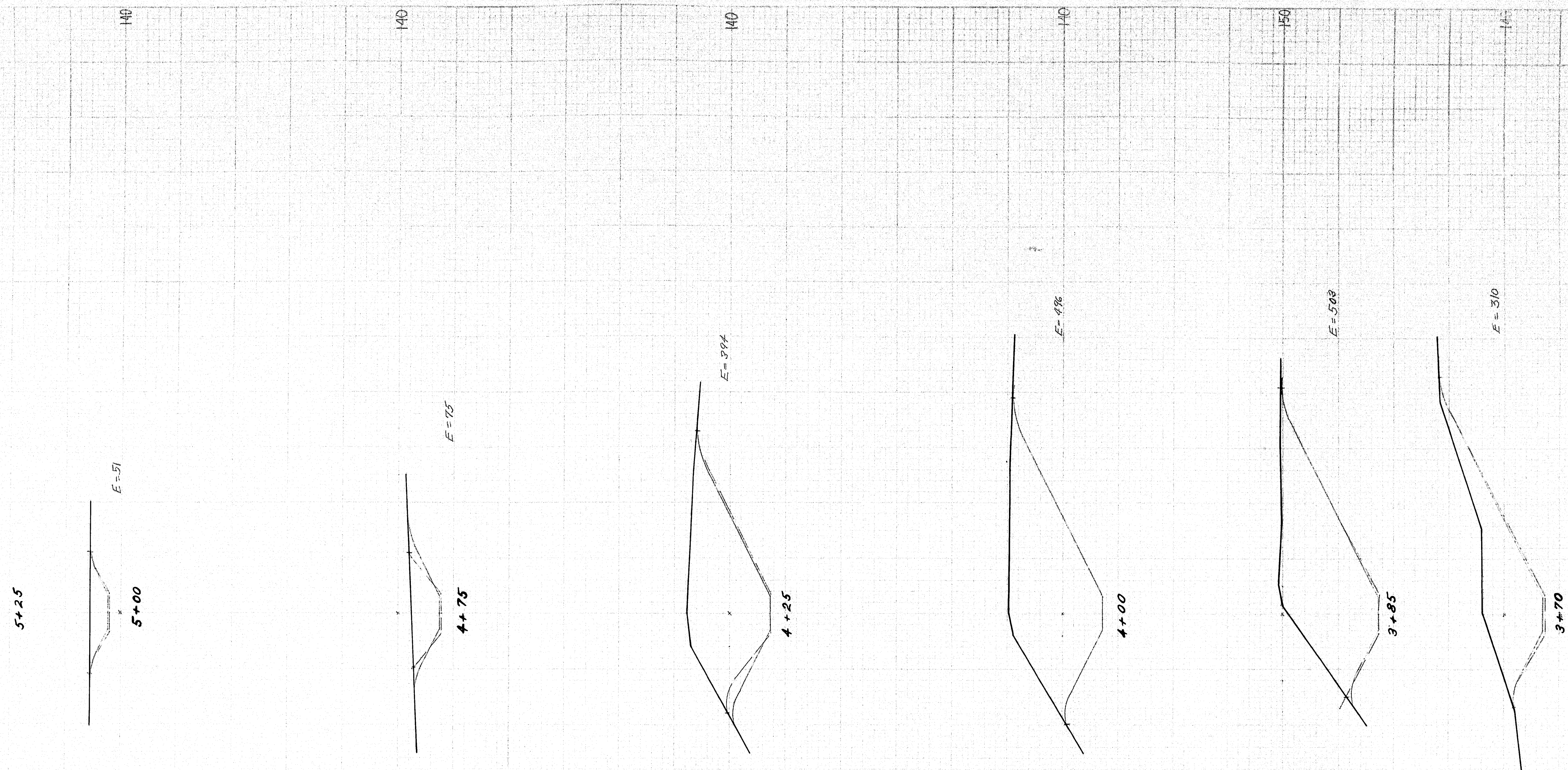
THE CLARKSON ENGINEERING CO., INC.		
DESIGN R.L.M.	CHECK P.W.G.	BRIDGE NO.
DRAWN J.S.	APPROVED W.A.H.C.J.M.	SURVEY PLOT
STATE HIGHWAY COMMISSION		
INTERSTATE #95		
UNDER		
WESTERN AVENUE		
IN THE CITY OF		
FAIRFIELD		
SOMERSET COUNTY		
PIER AND DECK		
REINFORCEMENT SCHEDULE		

Item	No. Pieces	Size	Length	Mark	Type	A	B	C	D	E	F	G	H	J	K	R	O
PIER A																	
1	68	#5	9'-8"	PA 501	TI	0'-6"	1'-8"	2'-8"	1'-8"	2'-8"							
2	4		9'-6"	PA 502			1'-11"	2'-4"	1'-11"	2'-4"							
3			9'-10"	PA 503				2'-6"		2'-6"							
4			10'-2"	PA 504				2'-8"		2'-8"							
5			10'-6"	PA 505				2'-10"		2'-10"							
6	4		10'-10"	PA 506				3'-0"		3'-0"							
7	46		11'-2"	PA 507	TI	0'-6"	1'-11"	3'-2"	1'-11"	3'-2"							
8	28	#5	7'-8"	PA 508	Str.												
9	28	#6	7'-8"	PA 601													
10	4	#6	19'-0"	PA 602													
11	40	#9	20'-3"	PA 901													
12	40	#9	6'-3"	PA 902													
13	12	#9	27'-8"	PA 903													
14	9	#11	27'-8"	PA 1101													
15	9		22'-0"	PA 1102													
16	16		20'-2"	PA 1103	Str.												
17	8		30'-7"	PA 1104	19		5'-7"	25'-0"					0'-9"	5'-7"		30'-7"	
18	8	#11	8'-1"	PA 1105	19		5'-7"	2'-6"					0'-5"	5'-7"		8'-1"	
PIER B																	
23	68	#5	9'-8"	PB 501	TI	0'-6"	1'-8"	2'-8"	1'-8"	2'-8"							
24	4		9'-6"	PB 502			1'-11"	2'-4"	1'-11"	2'-4"							
25			9'-10"	PB 503				2'-6"		2'-6"							
26			10'-2"	PB 504				2'-8"		2'-8"							
27			10'-6"	PB 505				2'-10"		2'-10"							
28	4		10'-10"	PB 506				3'-0"		3'-0"							
29	46		11'-2"	PB 507	TI	0'-6"	1'-11"	3'-2"	1'-11"	3'-2"							
30	28	#5	7'-8"	PB 508	Str.												
31	28	#6	7'-8"	PB 601													
32	4	#6	19'-0"	PB 602													
33	40	#9	20'-3"	PB 901													
34	40	#9	6'-3"	PB 902													
35	12	#9	27'-8"	PB 903													
36	9	#11	27'-8"	PB 1101													
37	9		22'-0"	PB 1102													
38	16		20'-2"	PB 1103	Str.												
39	8		30'-7"	PB 1104	19		5'-7"	25'-0"					0'-9"	5'-7"		30'-7"	
40	8	#11	8'-1"	PB 1105	19		5'-7"	2'-6"					0'-5"	5'-7"		8'-1"	
PIER C																	
45	72	#5	9'-8"	PC 501	TI	0'-6"	1'-8"	2'-8"	1'-8"	2'-8"							
46	4		9'-6"	PC 502			1'-11"	2'-4"	1'-11"	2'-4"							
47			9'-10"	PC 503				2'-6"		2'-6"							
48			10'-2"	PC 504				2'-8"		2'-8"							
49			10'-6"	PC 505				2'-10"		2'-10"							
50	4		10'-10"	PC 506				3'-0"		3'-0"							
51	46		11'-2"	PC 507	TI	0'-6"	1'-11"	3'-2"	1'-11"	3'-2"							
52	28	#5	7'-8"	PC 508	Str.												
53	28	#6	7'-8"	PC 601													
54	4	#6	19'-0"	PC 602													
55	40	#9	20'-3"	PC 901													
56	40	#9	6'-3"	PC 902													
57	12	#9	27'-8"	PC 903													
58	9	#11	27'-8"	PC 1101													
59	9		22'-0"	PC 1102													
60	16		20'-2"	PC 1103	Str.												
61	8		30'-7"	PC 1104	19		5'-7"	25'-0"					0'-9"	5'-7"		30'-7"	
62	8	#11	8'-1"	PC 1105	19		5'-7"	2'-6"					0'-5"	5'-7"		8'-1"	
DECK																	
106	152	4	27'-10"	S 401	Str.												
107	456	4	30'-9"	S 402	Str.												
108	152	4	24'-1"	S 403	Str.												
109	562	5	5'-1"	S 501	S5		1'-10"	0'-8"	1'-7"				1'-0"				
110	562	5	6'-10"	S 502	S5		0'-7"	1'-3"	3'-2"	1'-3"			0'-7"				
111	50	5	3'-0"	S 503	Str.												
112	522	6	36'-2"	S 601	Str.												
113	256	6	37'-6"	S 602	15		3'-9"	0'-7"	4'-4"	2'-8"	3'-0"					36'-2"	
114	96	5	6'-0"	S 504	Str.												

B. P. R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-6(20)	21	27

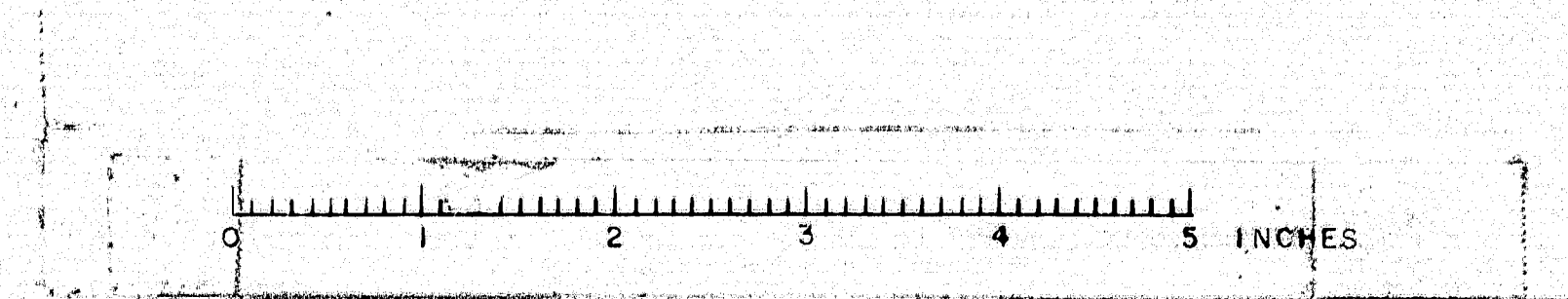


21 AS POINT



EMERY BROOK CHANNEL DIVERSION
3+70 TO 5+25

FAIRFIELD WESTERN AVE.



EMBANKMENT 1472 CU. YD.
CHANNEL EXCAVATION 458 CU. YD.
EARTH EXCAVATION 6 CU. YD.

EARTH EXCAVATION 19 CU. YD.
EMBANKMENT 226 CU. YD.

EMBANKMENT 161 CU. YD.
EARTH EXCAVATION 56 CU. YD.

EMBANKMENT 98 CU. YD.
EARTH EXCAVATION 80 CU. YD.

EMBANKMENT 37 CU. YD.
EARTH EXCAVATION 252 CU. YD.

EMBANKMENT 761 CU. YD.

EMBANKMENT 1069 CU. YD.

EMBANKMENT 1061 CU. YD.

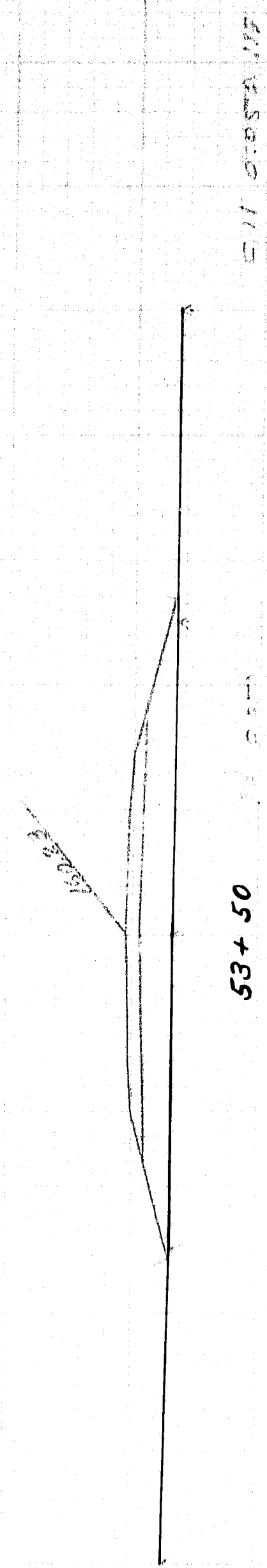
EMBANKMENT 1043 CU. YD.

EMBANKMENT 996 CU. YD.

EMBANKMENT 952 CU. YD.

EMBANKMENT 815 CU. YD.

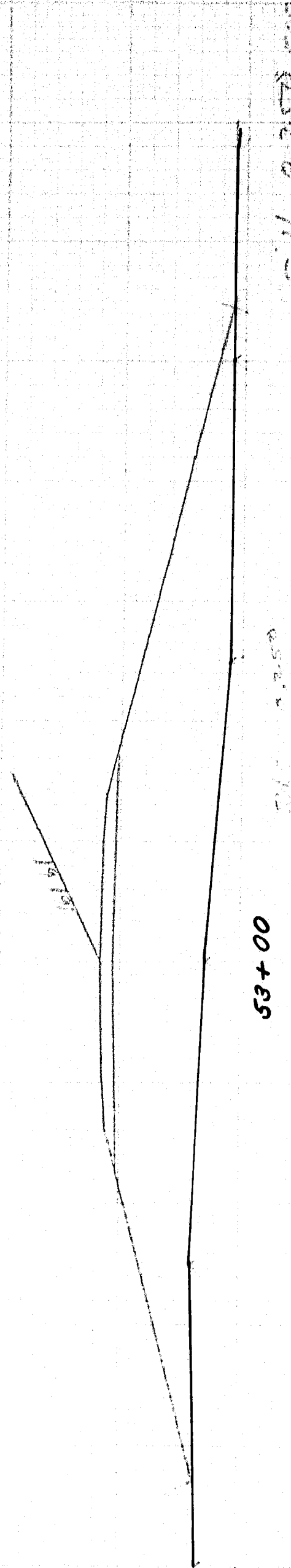
WESTERN AVENUE DETOUR - FAIRFIELD INTERCHANGE				
B. P. R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-6(26)	22	27



53+50

1:1

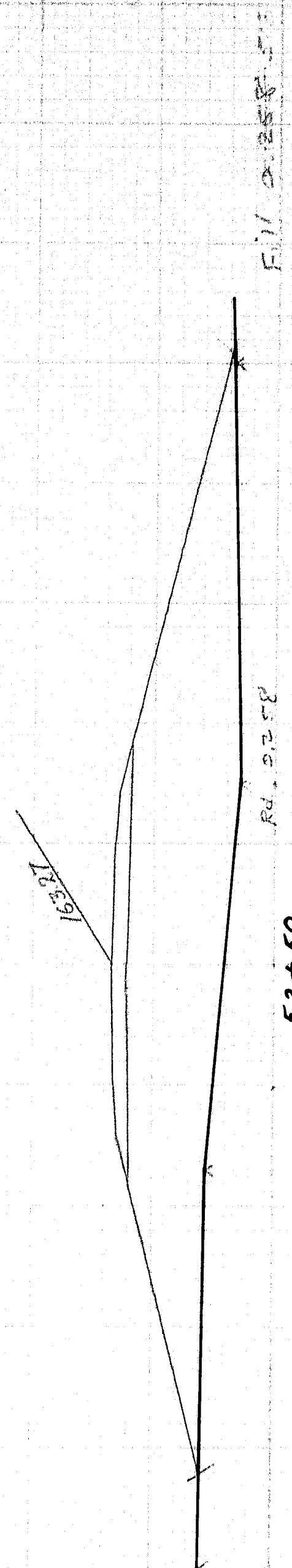
1:1.5



53+00

1:1

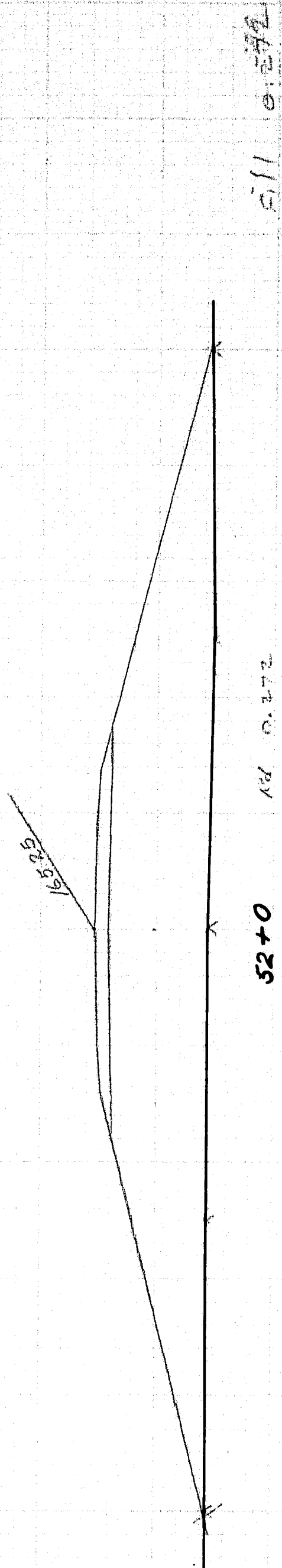
1:1.5



52+50

1:1

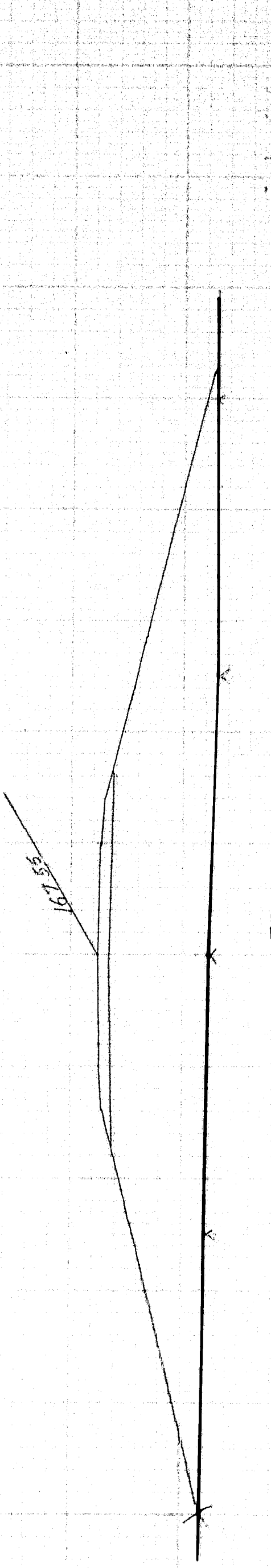
1:1.5



52+00

1:1

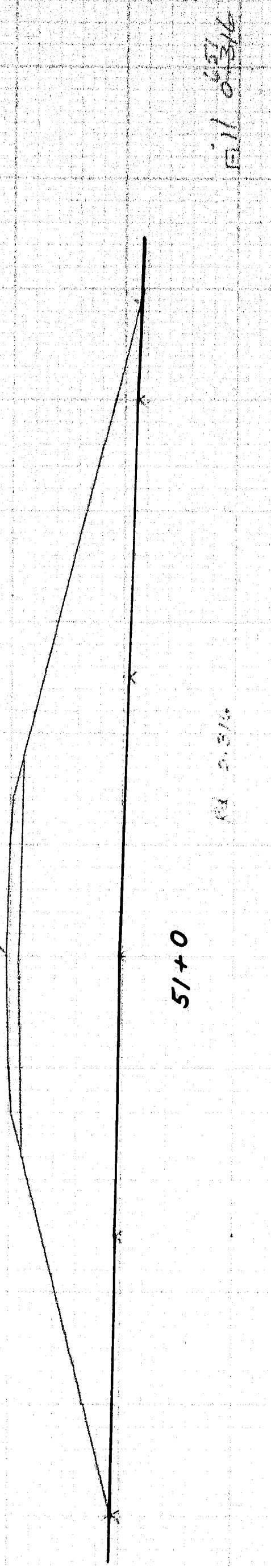
1:1.5



51+50

1:1

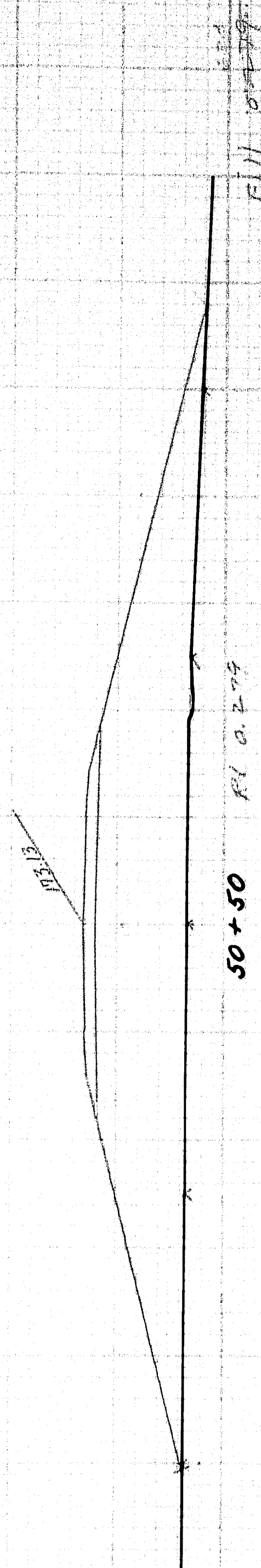
1:1.5



51+00

1:1

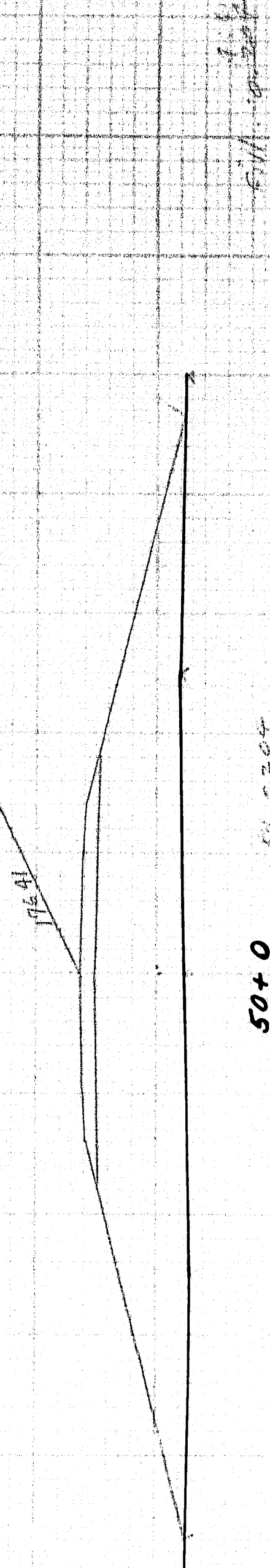
1:1.5



50+50

1:1

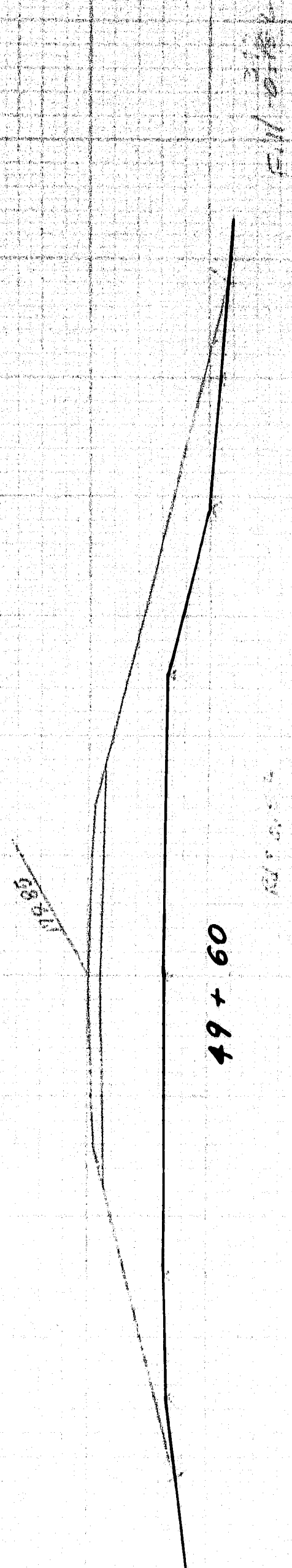
1:1.5



50+00

1:1

1:1.5



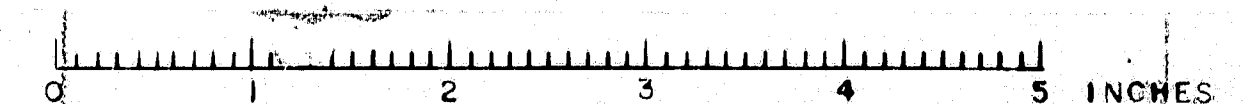
49+60

1:1

1:1.5

WESTERN AVE. DETOUR
49+60 TO 53+50

FAIRFIELD WESTERN AVE.



Notes
D.C. 155
H.E.

150

60+87

150

60+50

EARTH EXCAVATION 48 CU. YD.

150

60+00

EMBANKMENT 11 CU. YD.
EARTH EXCAVATION 100 CU. YD.

150

59+50

EMBANKMENT 94 CU. YD.
EARTH EXCAVATION 52 CU. YD.

150

59+00

EMBANKMENT 556 CU. YD.

140

58+50

EMBANKMENT 1395 CU. YD.

140

58+00

EMBANKMENT 2248 CU. YD.

130

57+50

EMBANKMENT 2782 CU. YD.

130

57+00

EMBANKMENT 3129 CU. YD.

130

56+50

EMBANKMENT 2682 CU. YD.

140

56+00

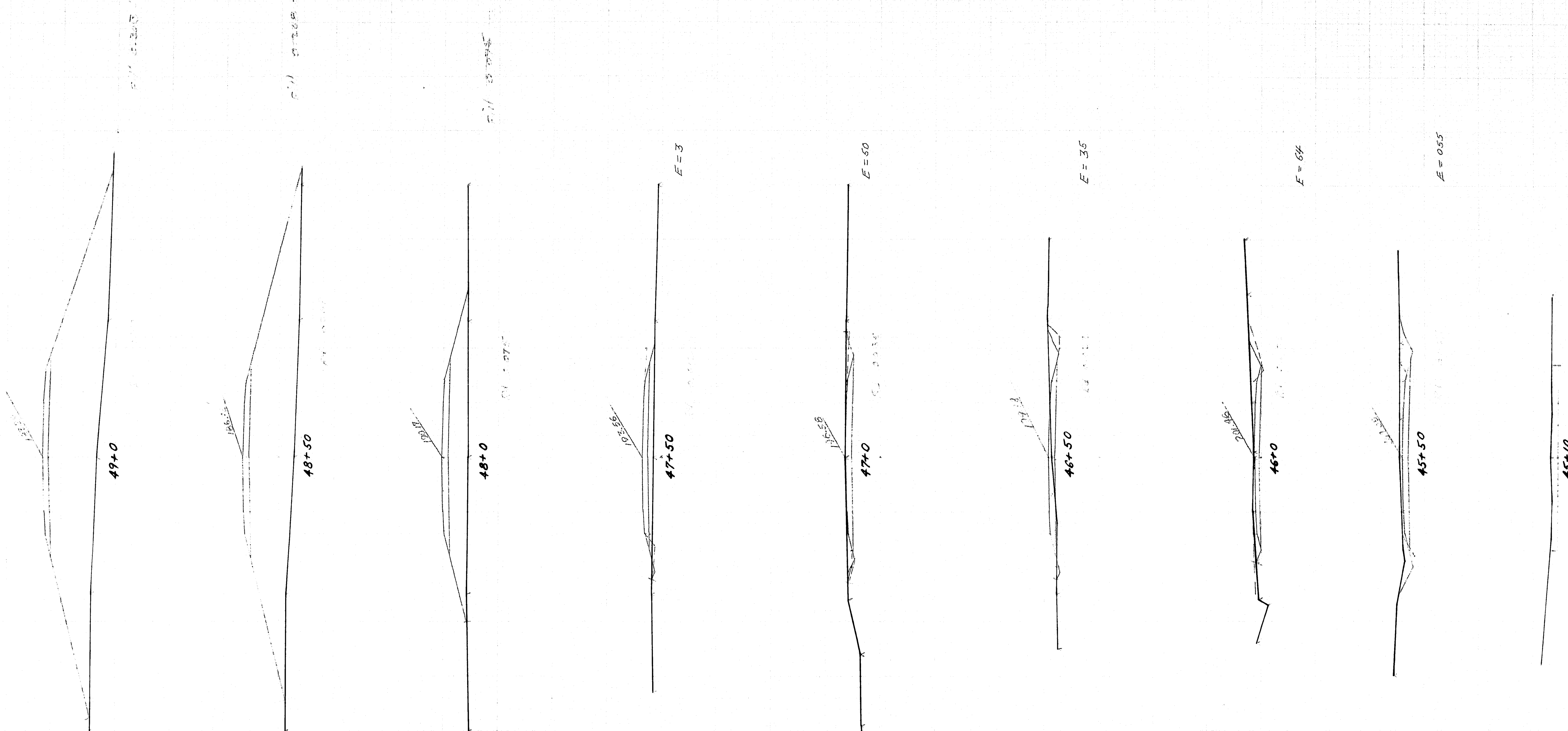
56+15 West Ave.
208x96 Structural Plate Pipe Culvert Constructed
of 7' 9" A.C.M. Plates with 5% Elliptical Fabrication
Hand Laid Riprap at Outlet, 2' Thick, to 4' Above Invert
and 32' from toe of Pipe.
Skew 15° Ahead Rt.
Pipe shall be placed with a camber of 3" from each
end towards the center.

56+00
15' x 62' ACCMP
Hand Laid Riprap at Outlet
18" Thick

140

56+00 to 56+15 Rt.
70' x 2' Ditch

B. P. R.	STATE	PROJECT NO.
REG. NO.		
1	MAINE	1-49-4(2)



WESTERN AVE. DETOUR
45+10 TO 49+00

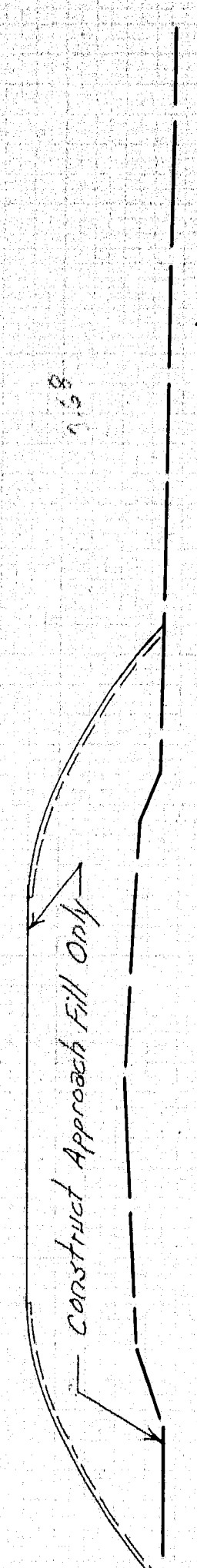
FAIRFIELD WESTERN AVE.

74-32

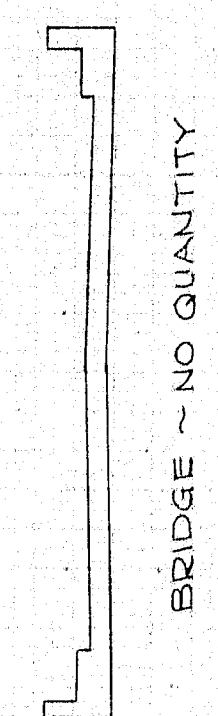
0 1 2 3 4 5 INCHES

BRIDGE

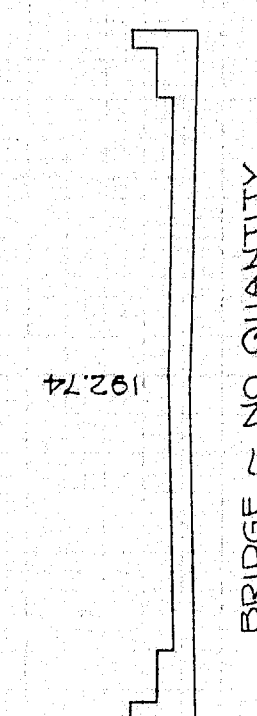
EMBANKMENT CU. YD.



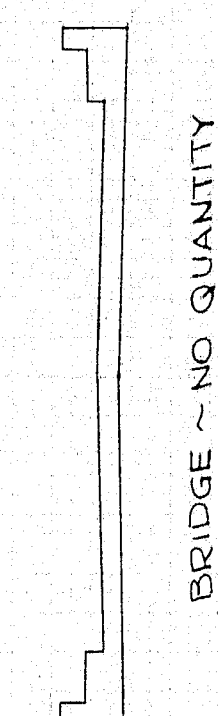
EMBANKMENT CU. YD.



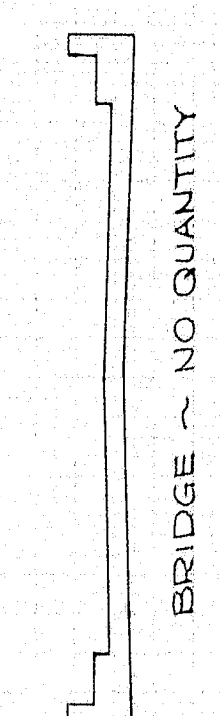
50+50



49+50

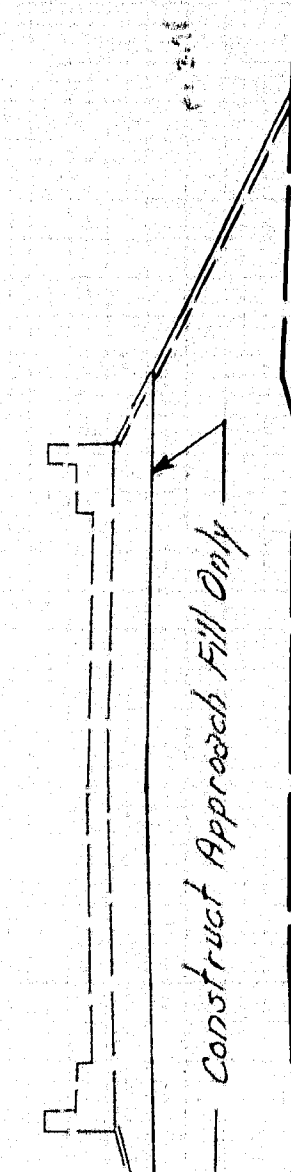


49+00



48+50

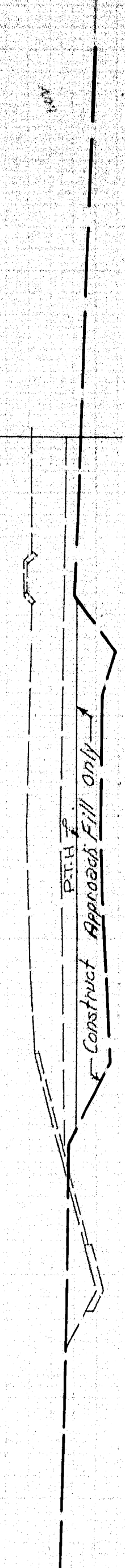
48+47 to 55+82 Lt.
150' Ditch (Temporary)



48+00 to 48+45
50' x 2' Ditch at
Hand Laid Riprap
18" Thick

48+00

EMBANKMENT CU. YD.



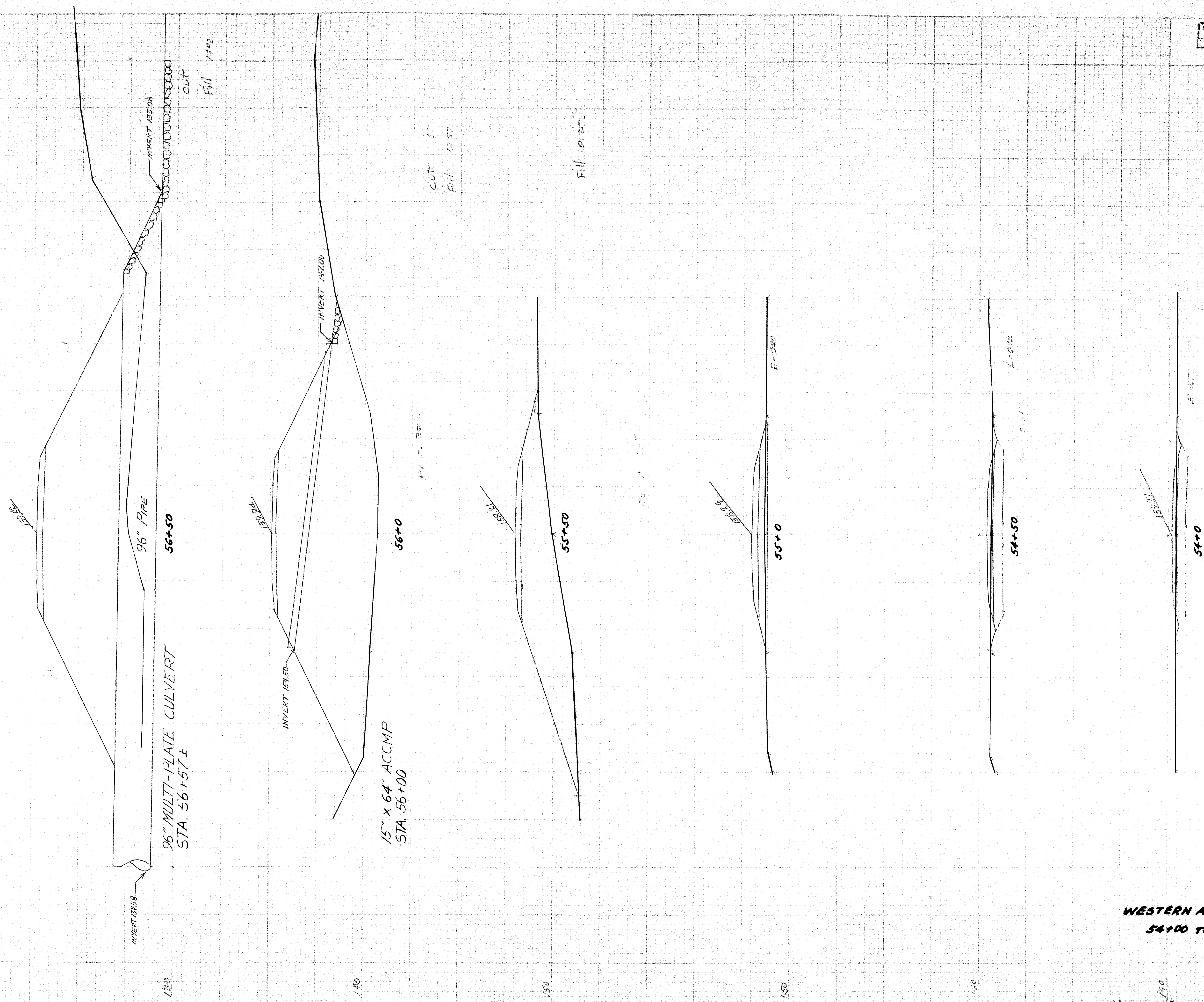
47+50

47+02 to 48+00 Lt.
105' Ditch (Temporary)

EMBANKMENT CU. YD.

STATE	FED. AID	SHEET	TOTAL
NO.	NO.	NO.	SHEETS
1	2006(07)	24A	47

24AS BUILT



WESTERN AVE. DETOUR
54+00 TO 56+50

FAIRFIELD WESTERN AVE.

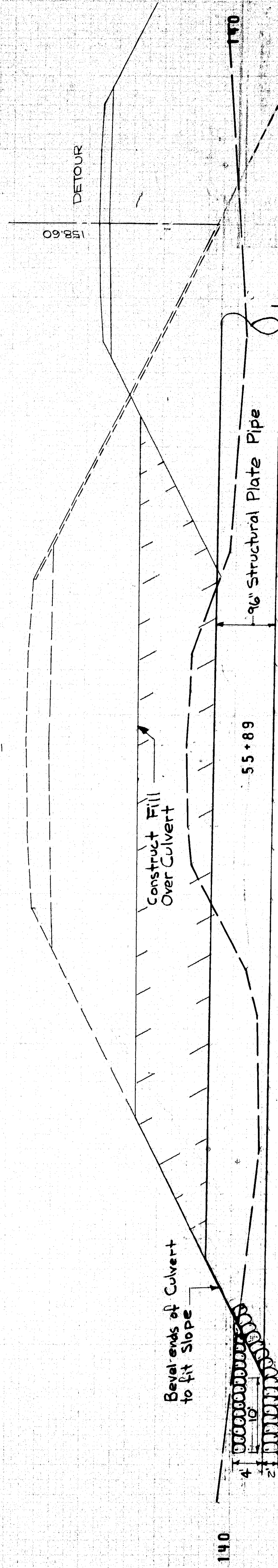
74-34

0 1 2 3 4 5 INCHES

11' x 10' 5/8" x 1/8"

56x18 West Ave
 208x96" Structural Plate Pipe Culvert Constructed of
 7 ga. ACCM. Plates with 5% Elliptical Fabrication,
 Hand Laid Riprap at Inlet, 2' thick, to 4' Above Invert
 and 10' from Tip of Pipe.
 Skew = 15° ahead of Rt.
 Pipe shall be placed with a camber of 3" from each end
 towards the center.

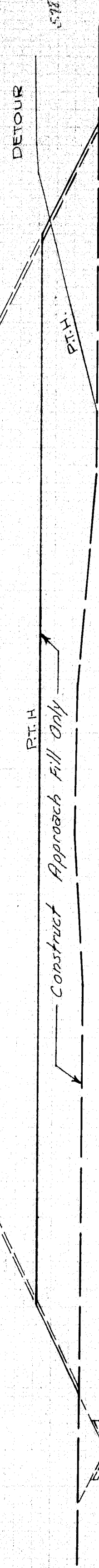
157.76



Bevel ends of Culvert to fit Slope

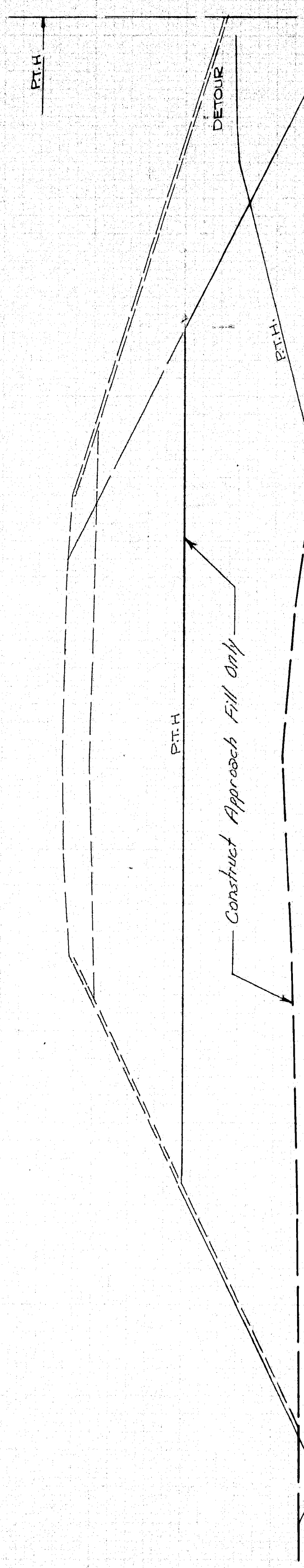
Hand Laid Riprap

EMBANKMENT CU. YD.



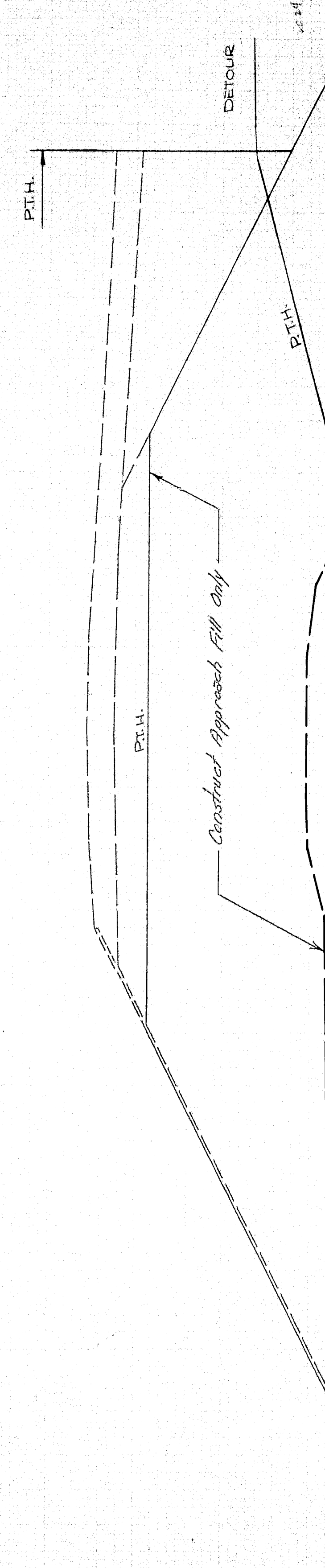
52+50

EMBANKMENT CU. YD.



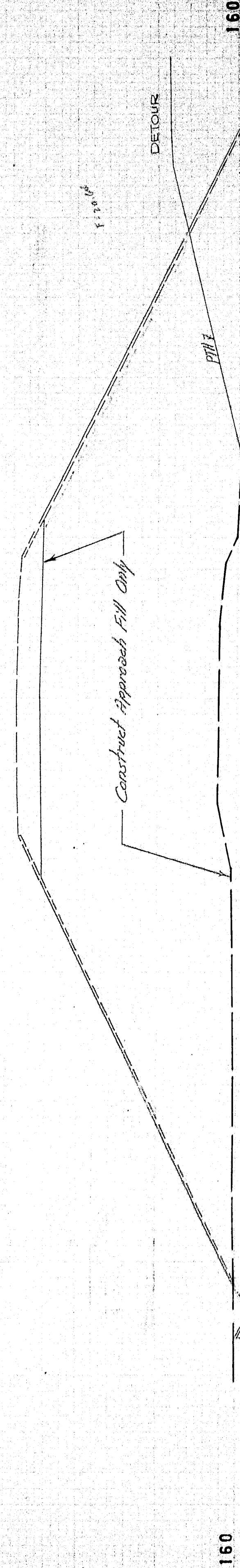
52+00

EMBANKMENT CU. YD.



51+50

EMBANKMENT CU. YD.



51+00

EMBANKMENT CU. YD.

S. P. R.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	3-16-00	26	27

25 AS BUILT

E = 0.4

60+50

E = 0.70

60+0

E = 1.00

59+50

E = 0.04

59+0

E = 0.12

58+50

58+0

57+50

57+0

WESTERN AVE. DETOUR
57+00 TO 60+00

FAIRFIELD WESTERN AVE.

74-36



S.P.R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	1-88-4 (28)	26	27

130 130
4 + 92

CHANNEL EXCAVATION 117 C.Y.

130 130
4 + 50

CHANNEL EXCAVATION 372 C.Y.

130 130
4 + 00

CHANNEL EXCAVATION 223 C.Y.

130 130
3 + 65

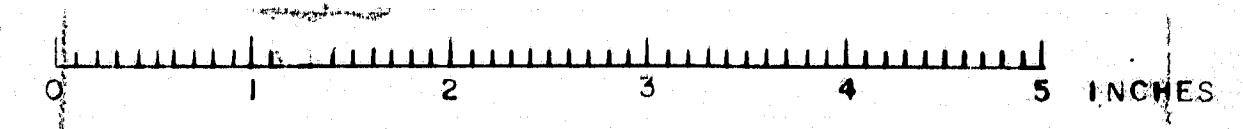
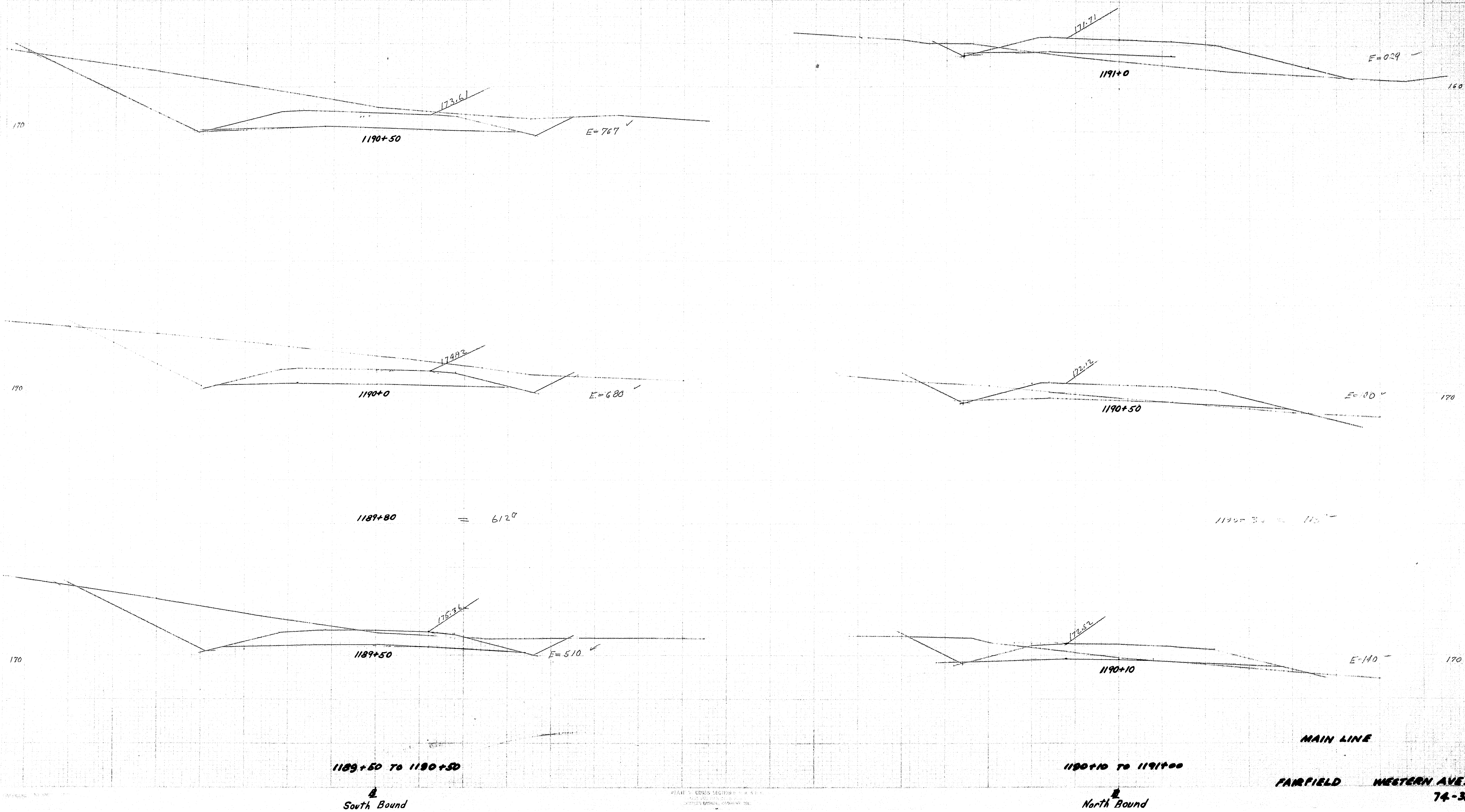
CHANNEL EXCAVATION 0

130 130
1 + 40

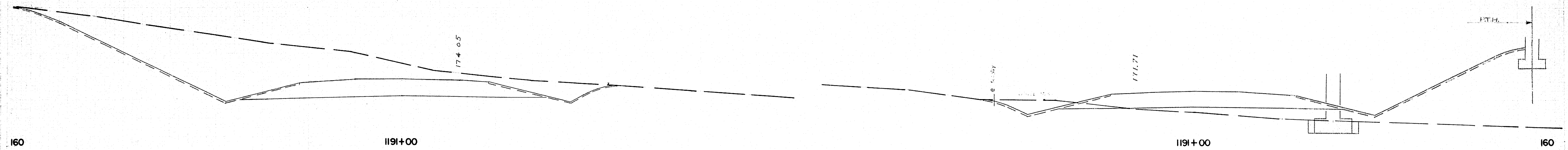
CHANNEL EXCAVATION 16 C.Y.

130 130
1 + 00

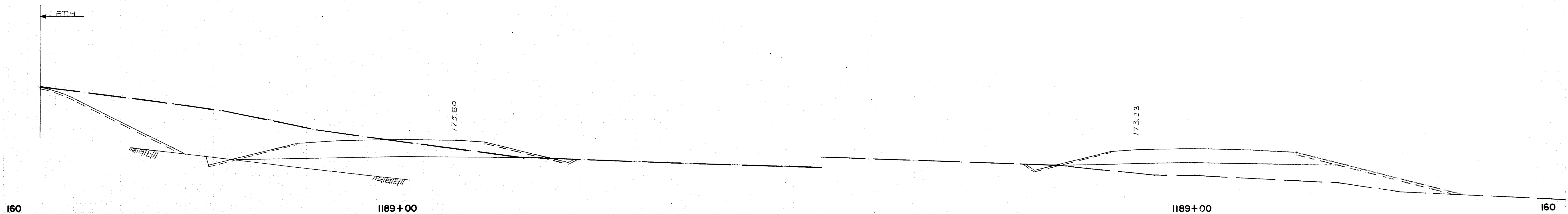
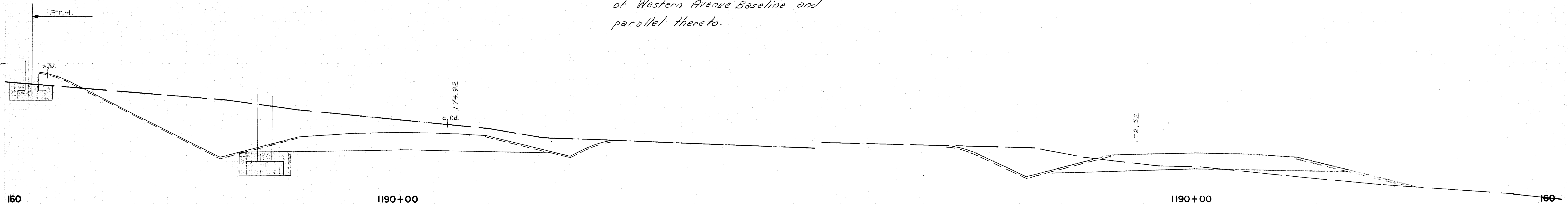
Emery Brook Relocation



B. P. R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-8(29)	27	27

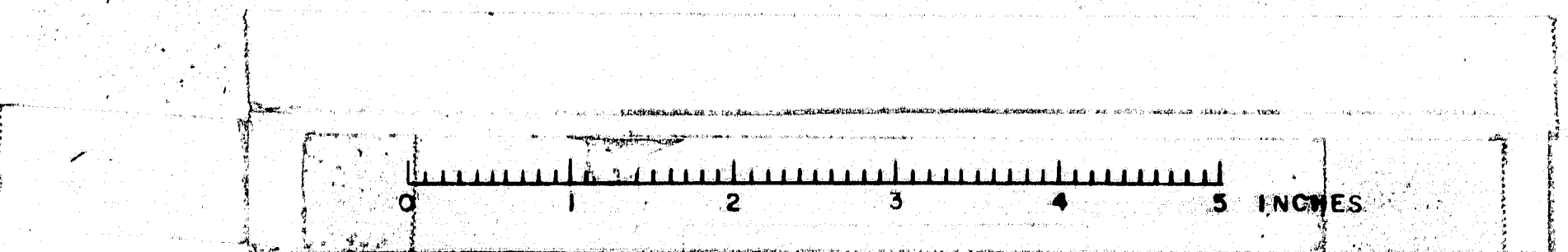


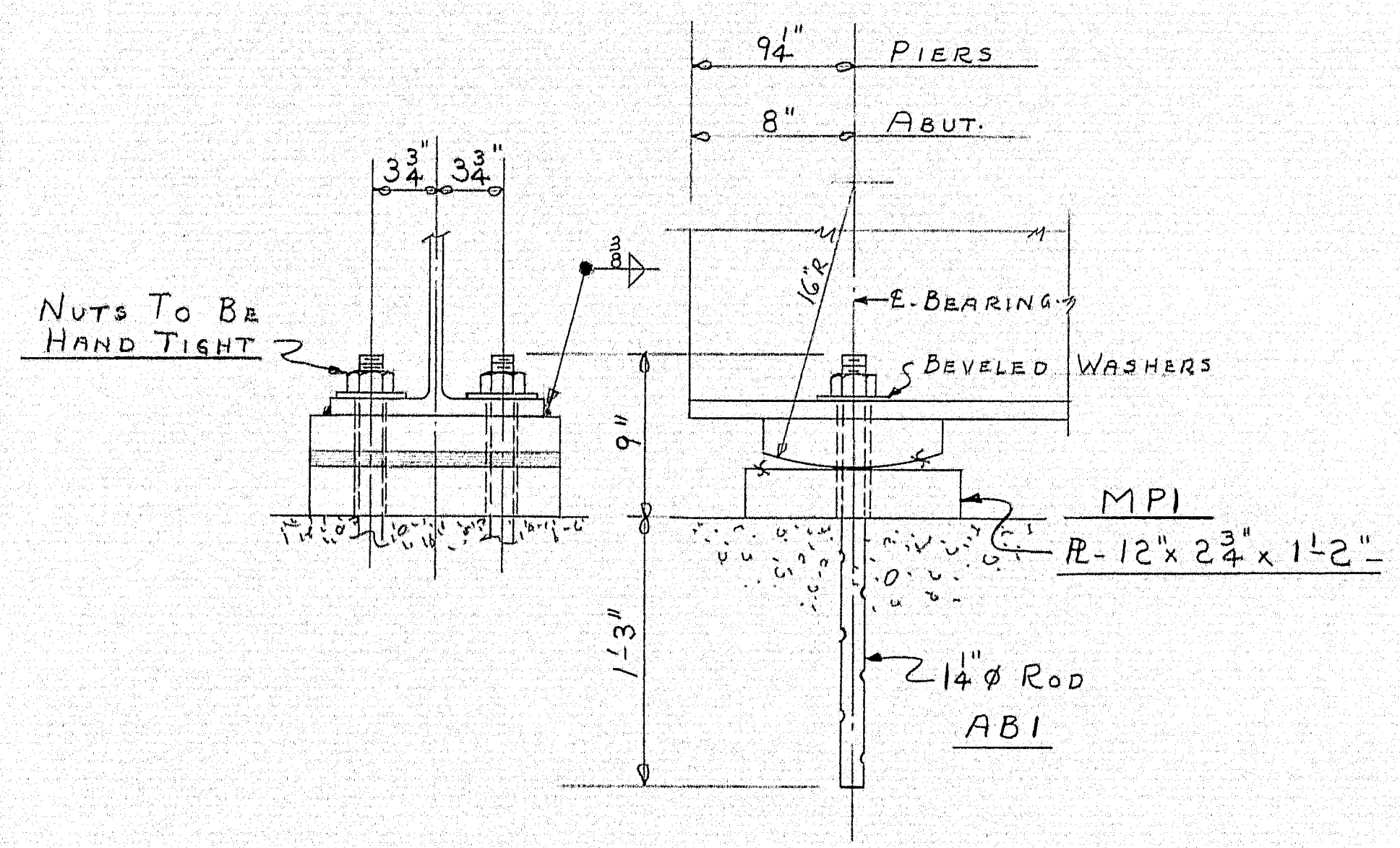
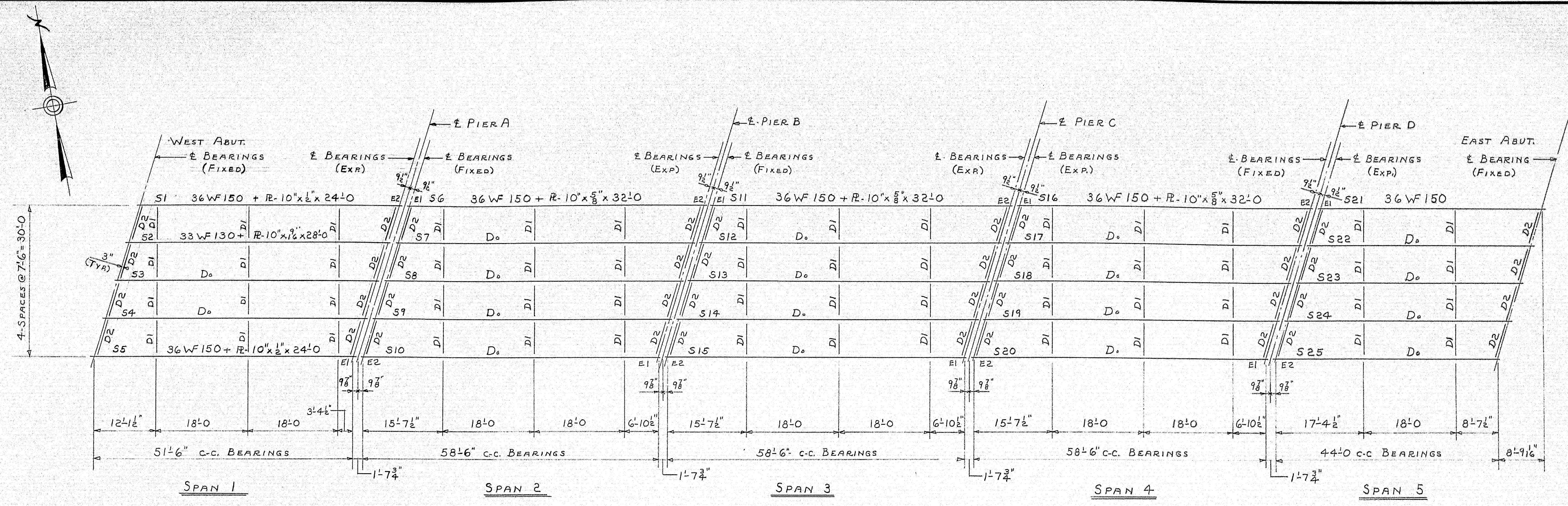
Note: Excavate Interstate 95 to subgrade for a distance of 95 feet either side of Western Avenue Baseline and parallel thereto.



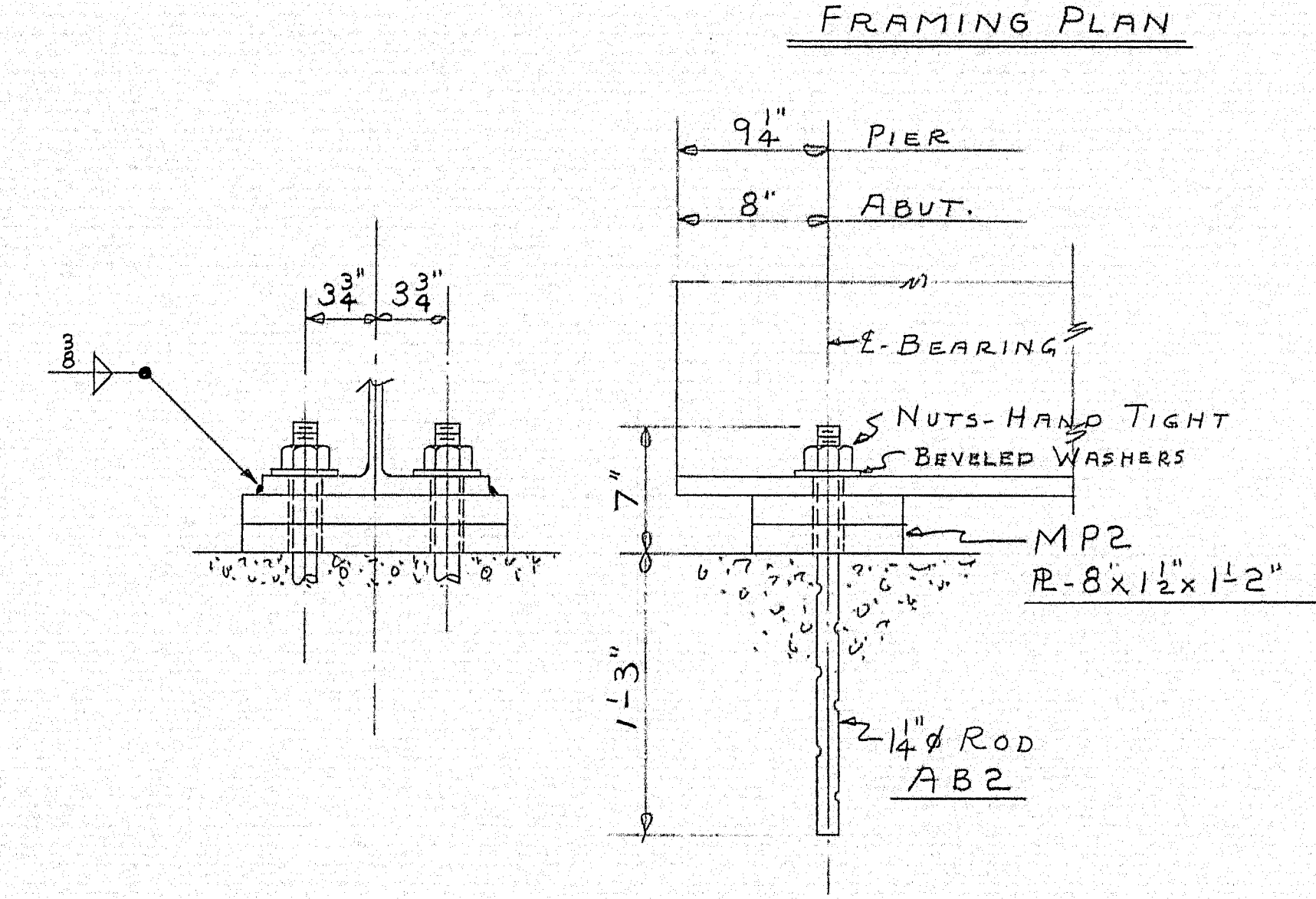
Southbound

Northbound

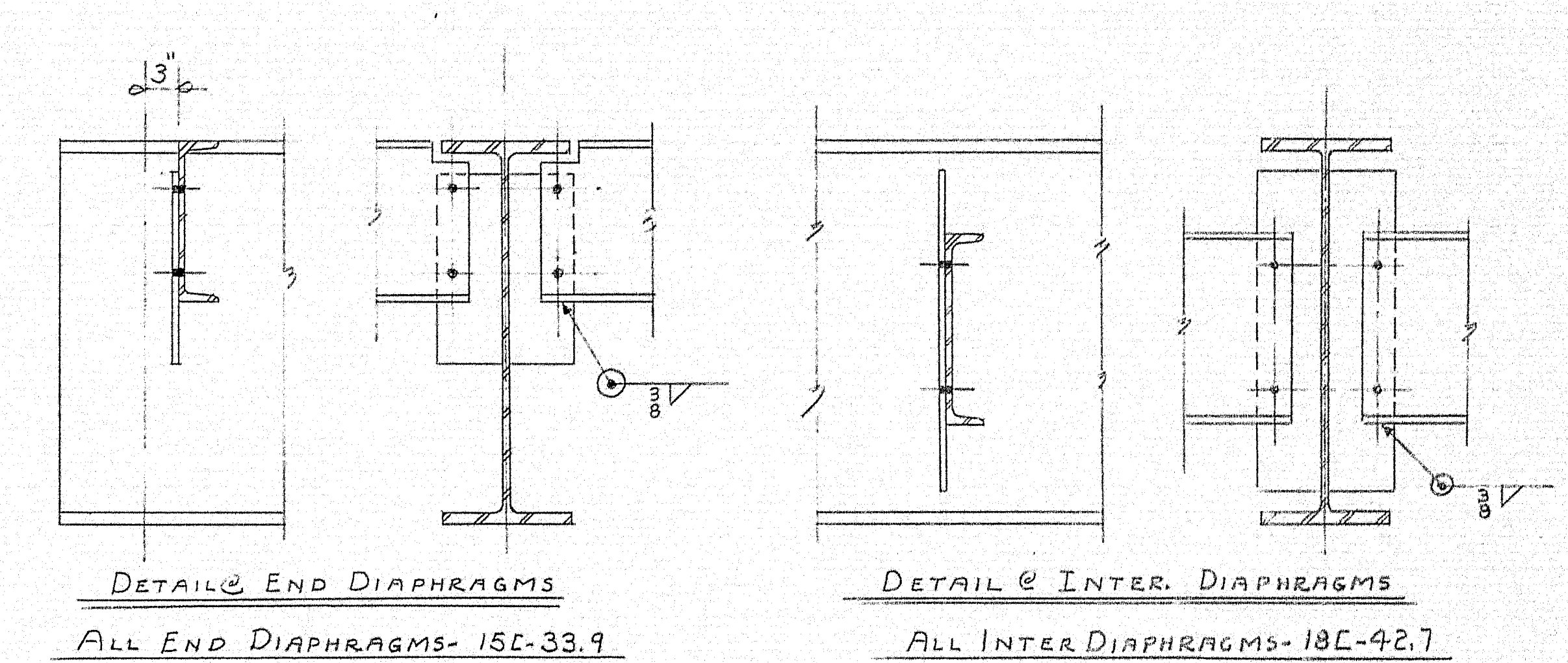




ANCHOR BOLT SETTING
FIXED & EXP. END OF
SPANS 1-2-3 & 4



ANCHOR BOLT SETTING
SPAN 5- FIXED & EXP. ENDS

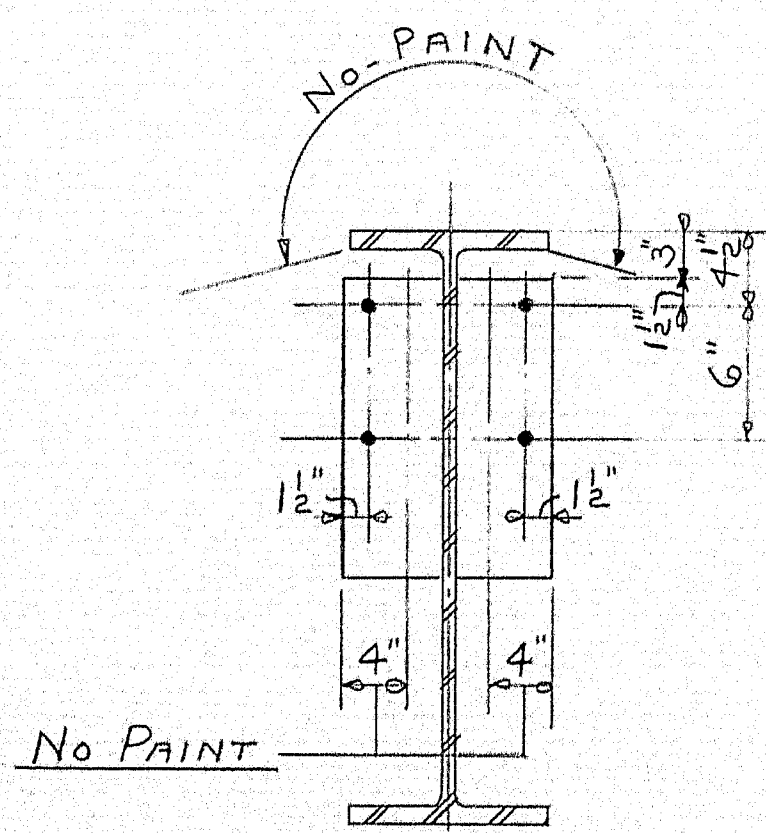
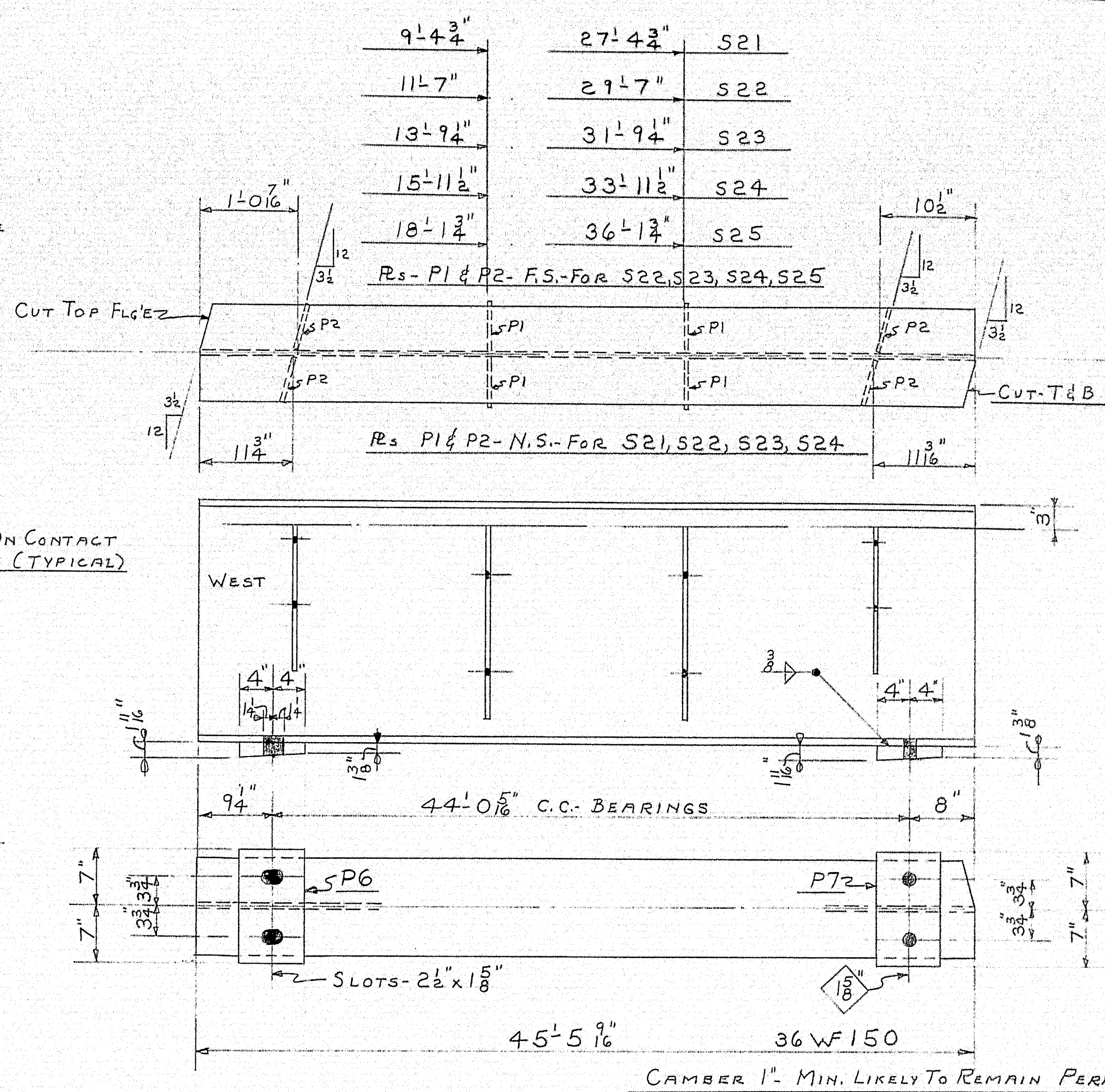
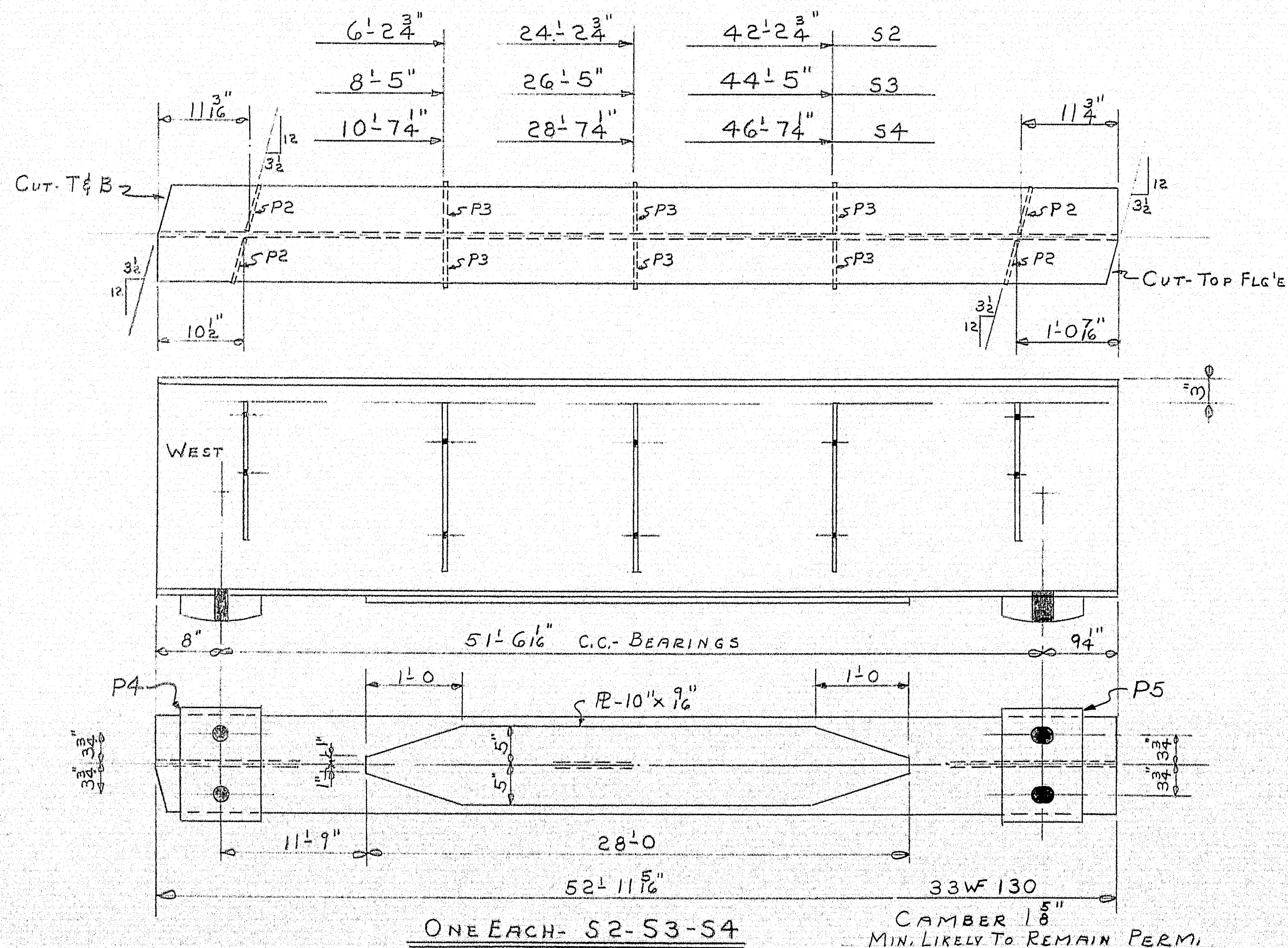
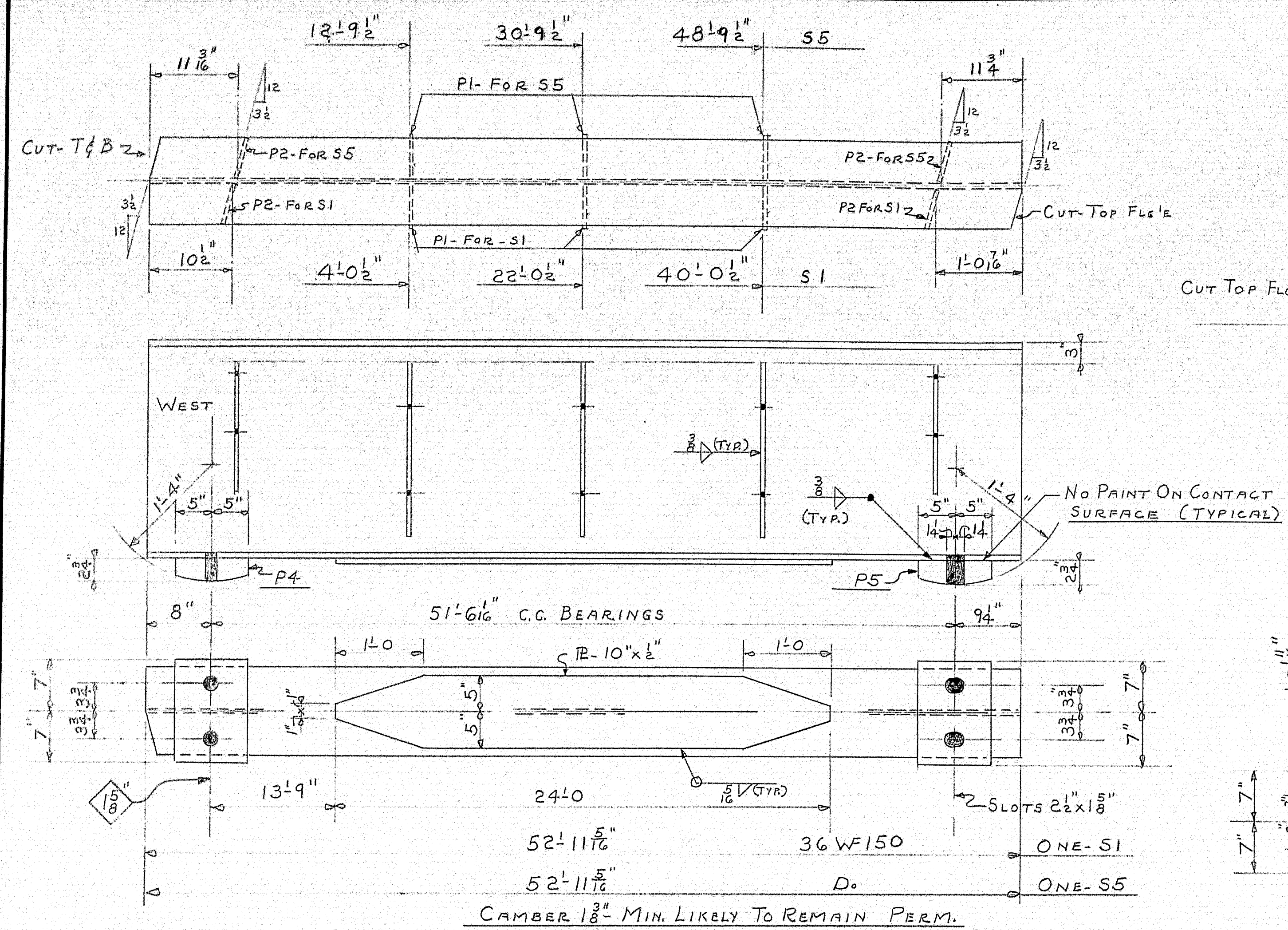


NOTES:
CAMBER SHOWN IS MINIMUM
LIKELY TO REMAIN PERMANENT

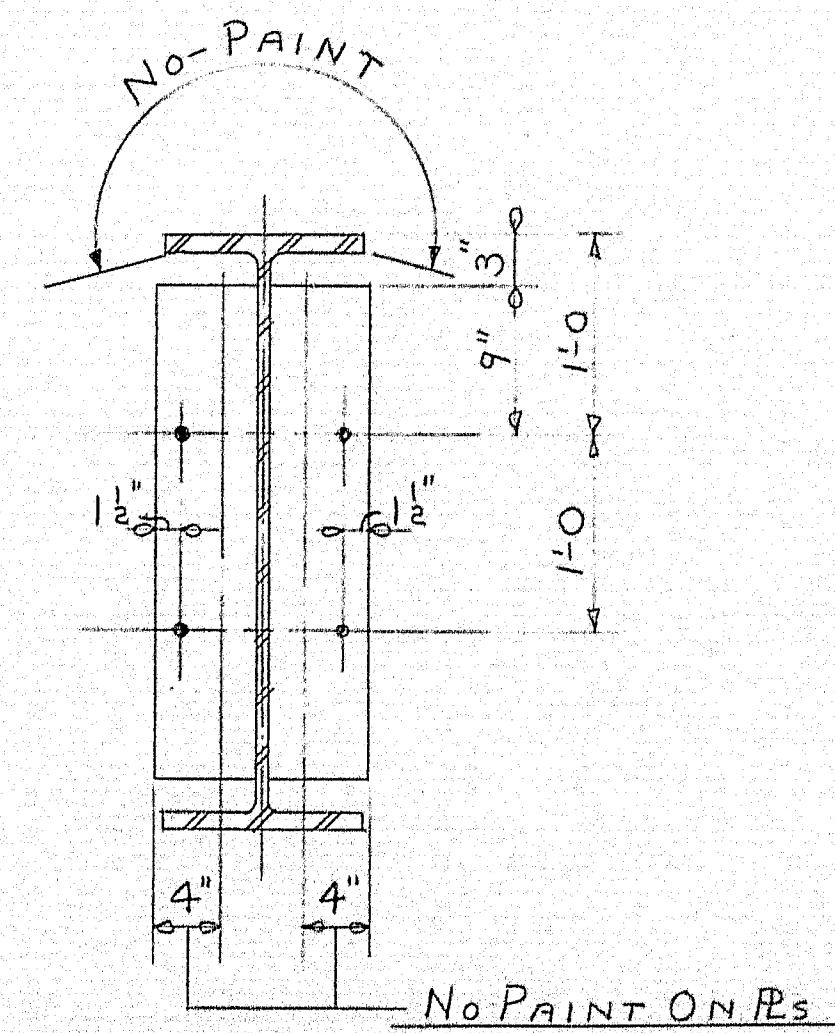
APPR. AS NOTED 3-25-59

FRAMING PLAN	
Bancroft & Martin Rolling Mills Company Brewer, Maine	
WESTERN AVE. BRIDGE INTERSTATE No 15 (I-95-G(25)126) FAIRFIELD MAINE	
CUSTOMER CIANCHETTE BROS.	
DESIGNER CLARKESON ENG. CO. INC.	
ORDER VERBAL	DWG. 89-36-E1

2-SHOP-4-3	
4-DIST. 4-3	
2-F/A-3-19-59	
DRAWN 2-28-59 D.C.	
REVISION 4-3-59 D.C.	
REVISION	
REVISION	



DETAIL @ R-P2



DETAIL @ R-P1 & P3

SHIP		BILL OF MATERIAL					DWG. 89-36-S1
MARK	NO.	MARK	SHAPE	LENGTH	WT.	WT. DEDUCT.	REMARKS
S1	1		36 W F 150	52 11 5/8		7 1/2	1 3/8" CAMBER
S5	1		D.	52 11 5/8		7 1/2	D.
S21	1		D.	45 5 1/8		7 1/2	1" CAMBER
S22	1		D.	45 5 1/8		7 1/2	D.
S23	1		D.	45 5 1/8		7 1/2	D.
S24	1		D.	45 5 1/8		7 1/2	D.
S25	1		D.	45 5 1/8		7 1/2	D.
S2	1		33 W F 130	52 11 5/8		6 1/2	1 3/8" CAMBER
S3	1		D.	52 11 5/8		6 1/2	D.
S4	1		D.	52 11 5/8		6 1/2	D.
	2		R-10" x 1/2	24 0		27 1/2	
	3		R-10" x 1/8	28 0		46	
	22	P1	R-6 x 1/2	2 6		4 1/2	
	32	P2	D.	1 6		6 1/2	
	18	P3	D.	2 3		3 1/2	
	P4	5	R-10" x 2 3/4	1 2		8 1/2	REQ. # 777
	P5	5	D.	1 2		9 1/2	D.
	P6	5	R 8" x 1 1/8	1 2		45	D.
	P7	5	D.	1 2		39	D.
	1		1/8" WELD	266 0			@ 166# PER. FT.
	1		3/8" WELD	327 0			@ 124# PER. FT.

SHOP CONNECTIONS: WELD
FIELD CONNECTIONS: DO
HOLES: 1/8" UNLESS NOTED
PAINT: STATE OF MAINE SPEC'S.

APPR. AS NOTED- 3-25-59

STRINGERS- SPANS 1 1/2

Bancroft & Martin Rolling Mills Company
Brewer, Maine

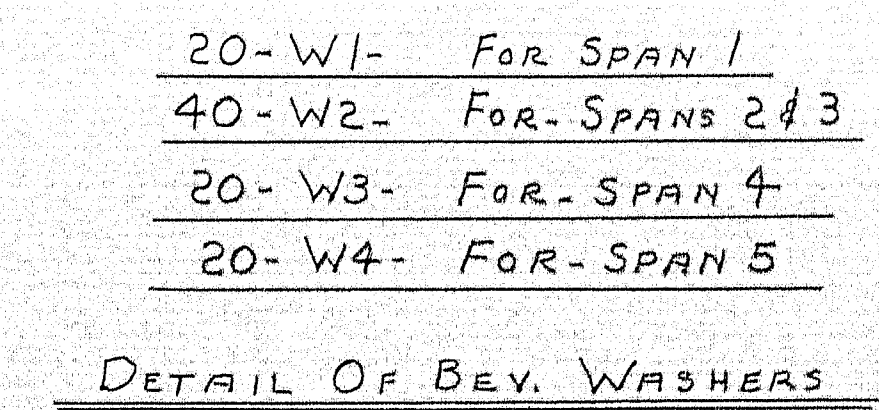
WESTERN AVE. BRIDGE
INTERSTATE No 95 [I-95-G(25)12C]
FAIRFIELD MAINE

CUSTOMER CIANCHETTE BROS.

DESIGNER CLARKSON ENG. CO. INC.

4-DIST-4-G	2-F/A-3-19-59
4-SHOP-4-3	DRAWN 3-3-59 D.C.
4-DIST-4-3	REVISION 4-3-59 D.C.
	REVISION 4-6-59 D.C.

ORDER VERBAL DWG. 89-36-S1



4-SHOP 4-3
4-DIST-4-3
2-F/A- 3-19-5

DRAWN	3-4-59
REVISION	
REVISION	
REVISION	

